



Ballpark NEXT Community Design Plan

Appendix
August 2025



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SLCRDA



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Regulatory and Site Analysis Summary

Perkins&Will

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01 — Context

Ballpark Neighborhood

Assets

1. Smith's Ballpark

Smith's Ballpark is an iconic stadium and the Ballpark neighborhood's central feature. It currently serves as the home of the Salt Lake Bees, who are scheduled to relocate after the 2024 season.

2. Jefferson Park

Located within the neighborhood, Jefferson Park offers green space for recreation and community activities.

3. Liberty Park

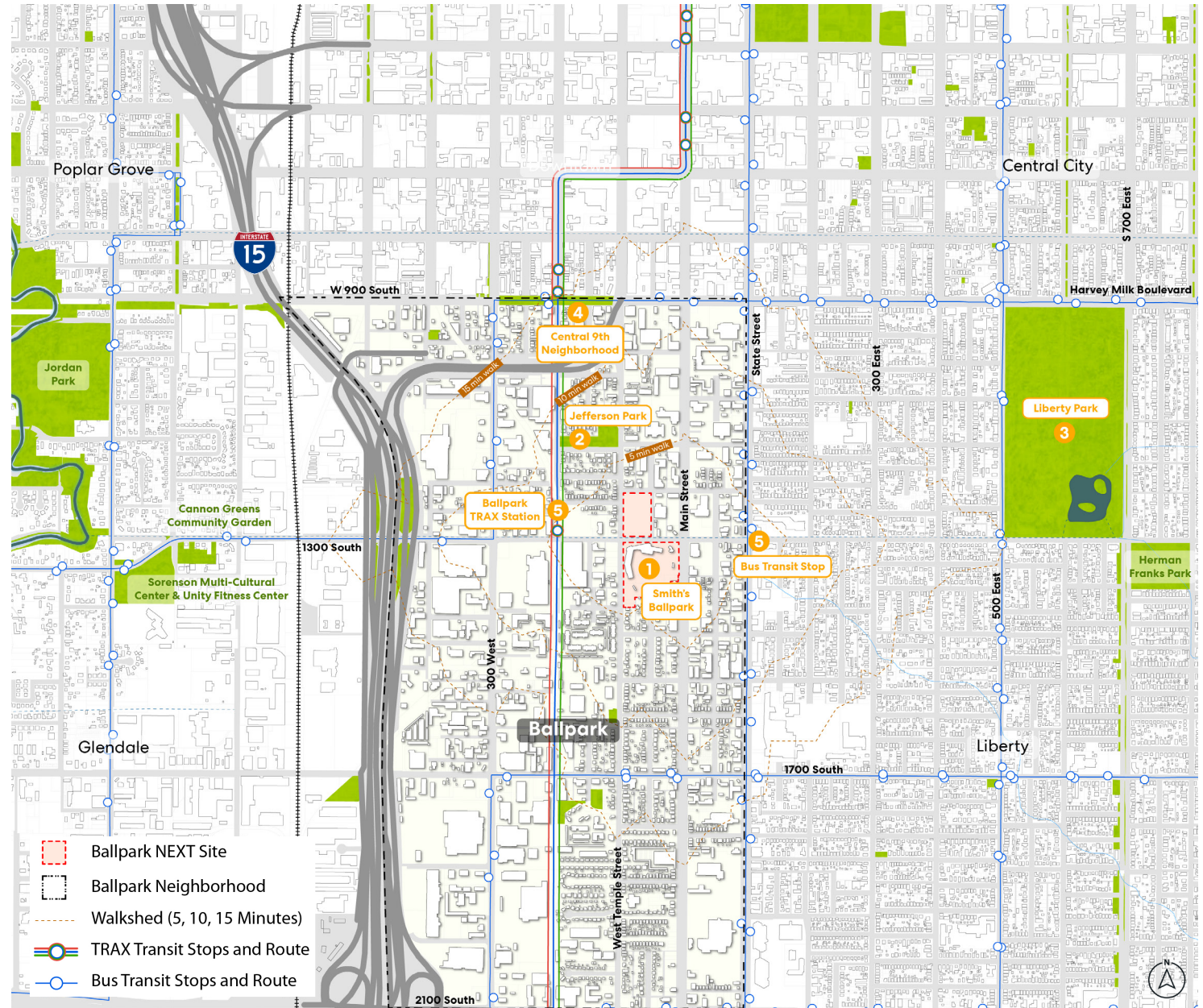
Within a 15 minute walk of the Ballpark NEXT Site, Liberty Park is one of Salt Lake City's largest and most popular parks, offering a wide range of recreational facilities.

4. Central 9th Neighborhood

Just north of the Ballpark neighborhood, this neighborhood is known for its revitalized streets, local businesses, and eateries, contributing to the area's growing reputation as a hub for dining and culture.

5. Easy Access to Public Transportation

The Site is well-served by TRAX light rail, particularly the Ballpark TRAX Station, which connects residents to Downtown and other parts of the city.



01 — Context

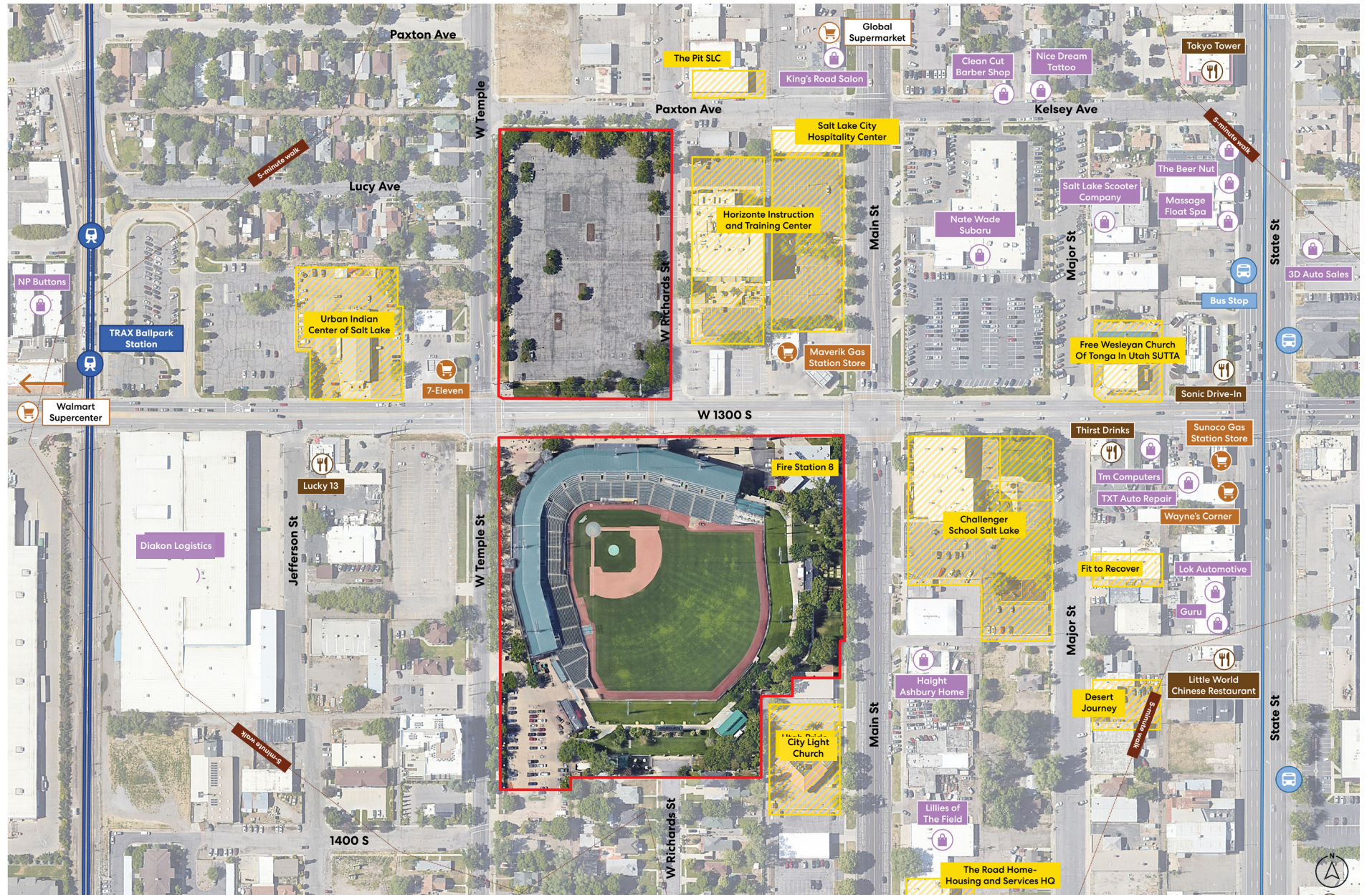
Neighborhood Amenities

The Ballpark NEXT Site is located at the intersection of 1300 South and West Temple Street. The Site comprises of the Smith's Ballpark south of 1300 South and the parking lot north of 1300 South, with a total area of 14.82 acres.

The Site is surrounded by a rich ecosystem of local businesses and community services.

Legend

- Ballpark NEXT Site Boundary
- Existing Community Oriented Uses
- 5-Minute Walkshed
- TRAX Transit Stop and Line
- Bus Transit Stop and Route
- Existing Amenities**
- Grocery Store/Supermarket
- Convenience Store
- Businesses/Retail Establishments
- Restaurant



01 — Context

Site History



Community Park Baseball Game, June 28, 1938,
Source: Utah State Historical Society



Franklin Quest Field



Smith's Ballpark (2023)

Community Park
(1915-1947)

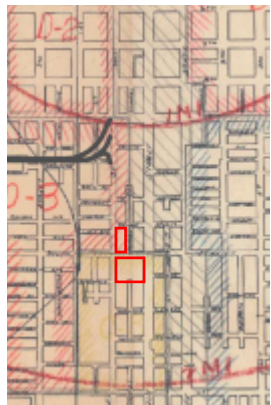
Derks Field
(1947-1993)

Franklin Quest Field
(1994-1997)

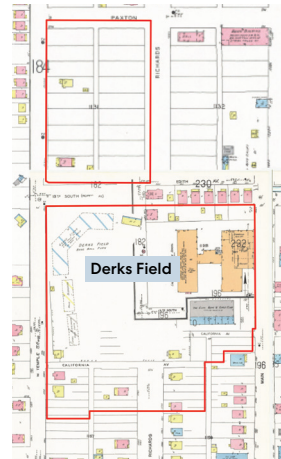
Franklin Covey Field
(1997-2009)

Spring Mobile Park
(2009-2014)

Smith's Ballpark
(2014-Present)



Redlining Map of Salt Lake
City, Utah; ca. 1933 - 1939
Source: National Archives at
College Park



Derks Field Historic Map
Source: University of Utah
Digital Library



Derks Field Aerial Shot, April 30, 1952
Source: Utah State Historical Society



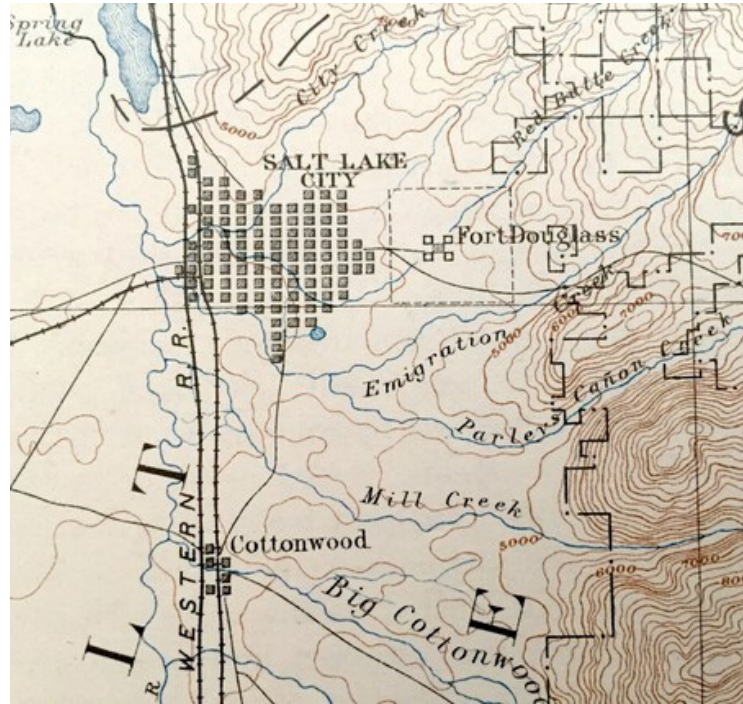
Franklin Covey Field (2002)

01 — Context

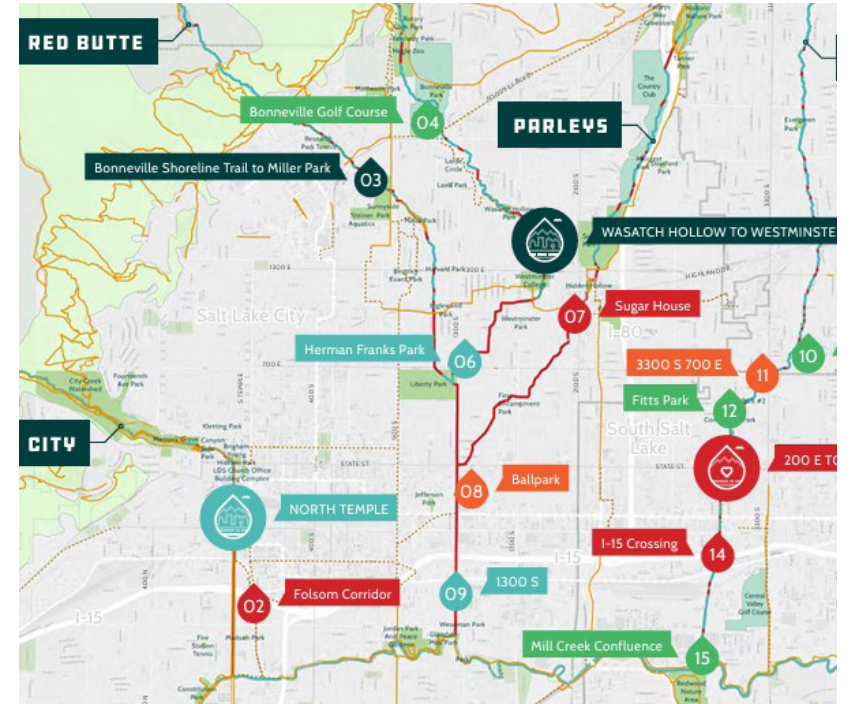
Hydrology

Background

- Historically, Red Butte, Emigration and Parleys Creeks were channelized to converge east of the the Site and then flow west to the Jordan river along present-day 1300 South.
- The “Three Creeks Confluence Park,” located west of the site near 1300 South and 900 West, was created by daylighting the underground channels where the three creeks meet the Jordan River. This project spurred the formation of the Seven Canyons Trust.
- The Site is marked as a civic activation opportunity in the Seven Greenways Vision Plan developed by local nonprofit Seven Canyons Trust. The plan recommends daylighting Red Butte, Emigration, and Parleys creeks and increase the urban forest as Salt Lake City’s Ballpark neighborhood experiences growth and redevelopment.



Salt Lake City, Utah 1885 US Geological Survey Topographic Map



Opportunity areas along seven greenways, Source: Seven Greenways Vision Plan



01 — Context

Challenges

Mobility & Connectivity

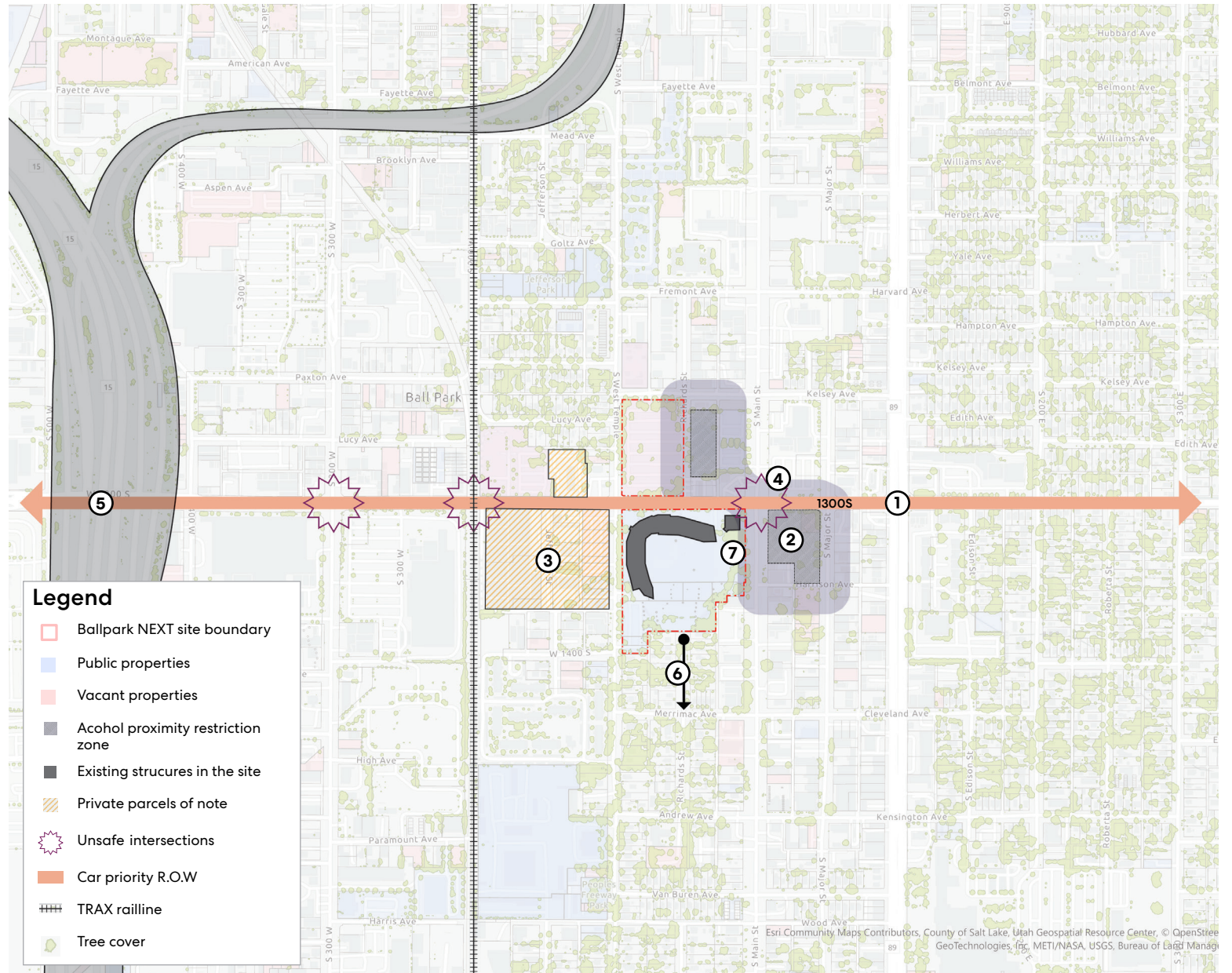
- 1. Narrow sidewalks and fast traffic along 1300 South create a uncomfortable environment for both pedestrians and vehicles.
- 2. Several intersections along 1300 South have seen collisions and present undesirable crossing conditions.
- 3. West Richards Street concludes at the the south end of the Ballpark, decreasing network connectivity.

Ownership & Zoning

- 4. City parcel ownership is fragmented, with certain key parcels held under private ownership.
- 5. There are strict alcohol proximity restrictions in effect around the Site.
- 6. Fire Station 8's current location on 1300 South hinders efficient emergency response times.

Air Pollution

- 7. Interstate-15 is a huge source of pollution that contributes to negative health impacts. The underpass also presents an unsafe pedestrian environment.



The Site

02 — The Site

Site Ownership and Acreage

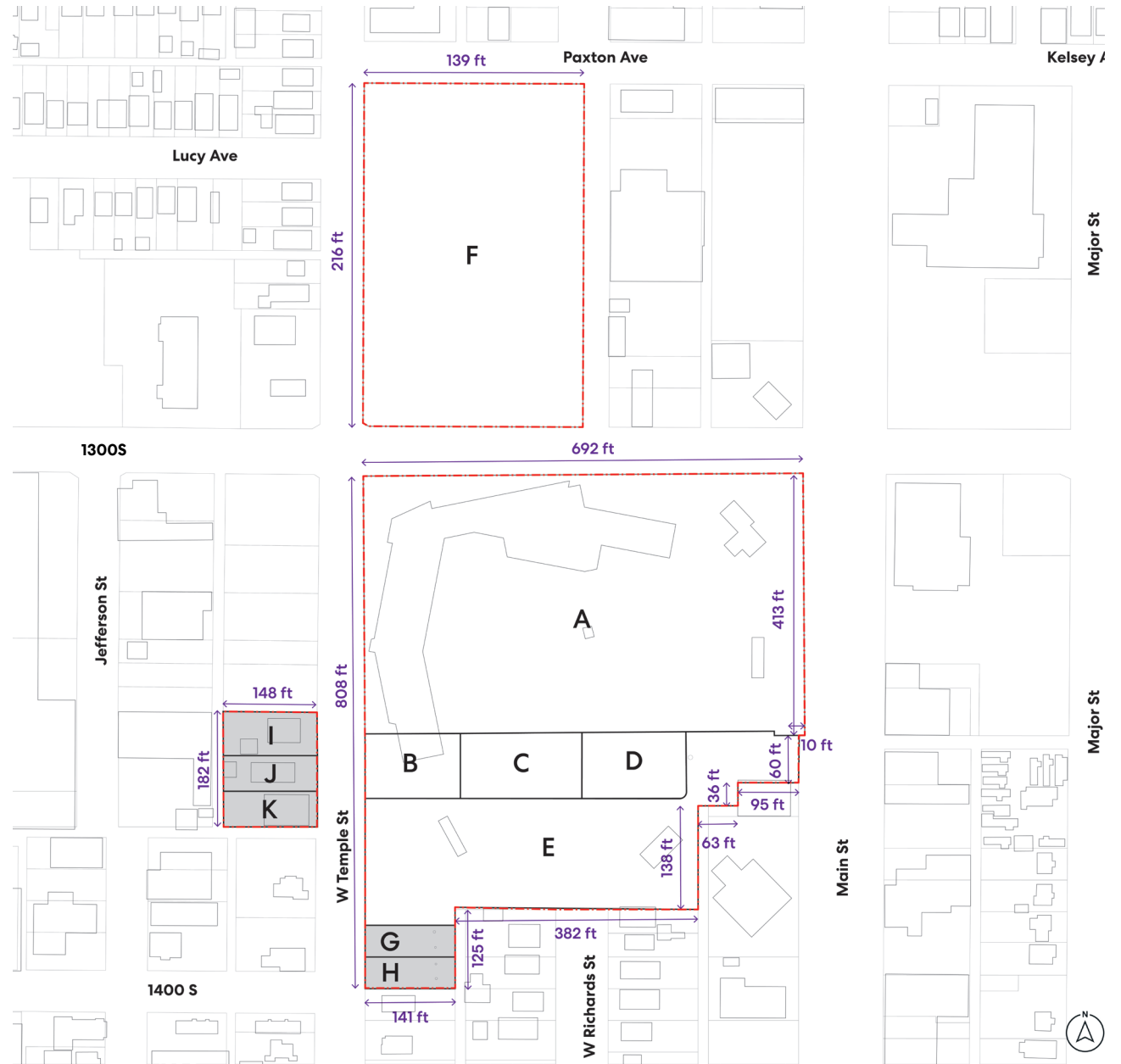
Parcel	Ownership	Acreage	
A	15-12-482-001-2000	SALT LAKE CITY CORP *	6.47
B	15-13-230-001-0000	SALT LAKE CITY CORP	0.35
C	15-13-230-033-2000	SALT LAKE CITY CORP	0.45
D	15-13-231-031-2000	SALT LAKE CITY CORP	0.39
E	15-13-230-034-2000	MUNICIPAL BUILDING AUTHORITY OF SLC	2.54
F	15-12-478-019-0000	S L C CORPORATION	4.30

Total 14.5 Acres

G	15-13-230-031-2000	SALT LAKE CITY	0.16
H	15-13-230-032-2000	SALT LAKE CITY CORP	0.16
I	15-13-226-004-0000	SALT LAKE CITY CORPORATION	0.23
J	15-13-226-005-0000	SALT LAKE CITY CORPORATION	0.19
K	15-13-226-006-0000	SALT LAKE CITY CORPORATION	0.16

Total 0.9 Acres

*Includes Fire Station Parcel



02 — The Site

Site Zoning

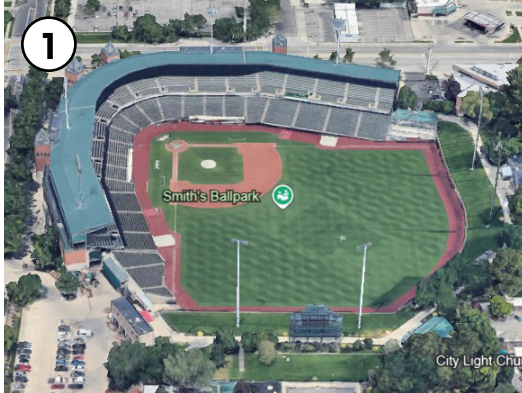
	Parcel	Zone	Zone Name	Description	Max. Height
A	15-12-482-001-2000	PL	Public Land	PL - The purpose of the PL public lands district is to specifically delineate areas of public use and to control the potential redevelopment of public uses, lands and facilities.	N/A
B	15-13-230-001-0000	PL	Public Land		
C	15-13-230-033-2000	PL	Public Land		
D	15-13-231-031-2000	PL	Public Land		
E	15-13-230-034-2000	PL	Public Land	The purpose of the RMF-35 moderate density multi-family residential district is to provide an environment suitable for a variety of moderate density housing types, including single-family, two-family, and multi-family dwellings with a maximum height of thirty five feet (35').	35'
F		RMF-35	Moderate Density Multifamily Residential		
G		RMF-35	Moderate Density Multifamily Residential		
H		RMF-35	Moderate Density Multifamily Residential		
I	15-13-226-004-0000	MU-8	Form Based Mixed-Use *Proposed	The purpose of the MU-8 Form Based Mixed Use 8 zoning subdistrict is to implement the city's general plan in areas that identify mid-rise buildings, generally eight stories or less in height, that contain a mix of land uses that support people who choose to live in or near the subdistrict.	90'
J	15-13-226-005-0000	MU-8	Form Based Mixed-Use *Proposed		
K	15-13-226-006-0000	MU-8	Form Based Mixed-Use *Proposed		

- PL- Public Lands
- R-1-5000
- RMF-35 Multifamily Residential
- MU-8-Form Based Mixed Use *Proposed
- FB-UN1-Form Based Urban Neighborhood 1
- FB-UN2-Form Based Urban Neighborhood 2



02 — The Site

Existing Structures



Smith's Ballpark



Fire Station 8



Canteen and Restrooms



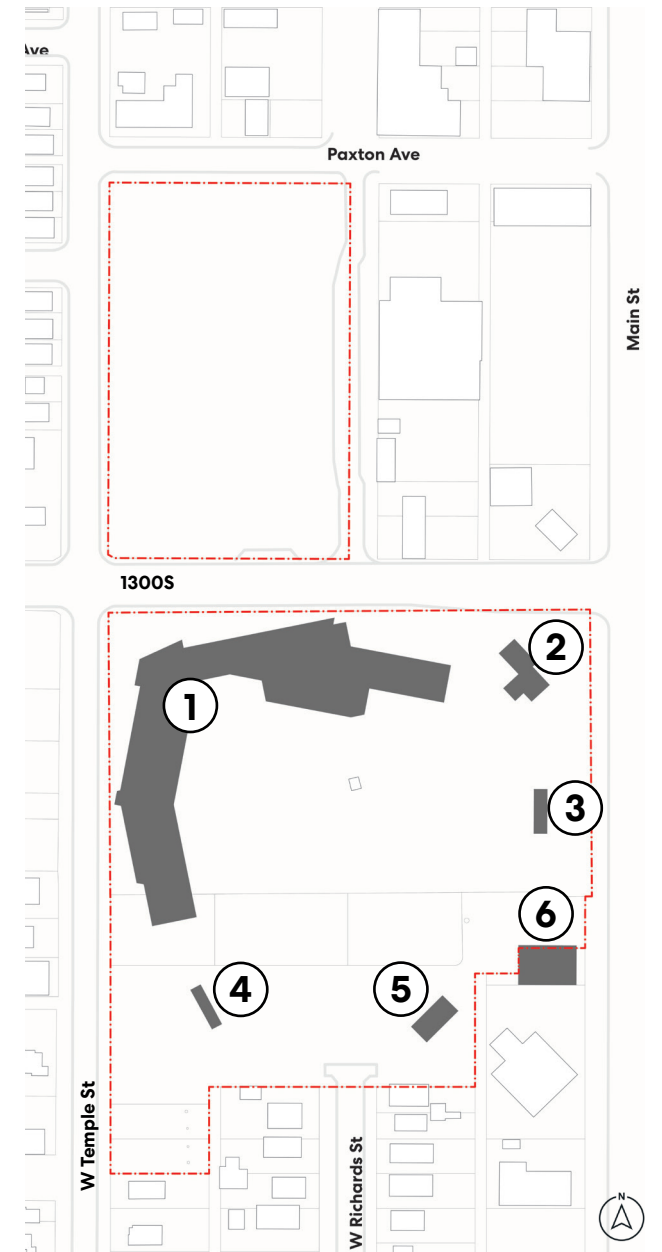
Canteen and Restrooms



Pavilion



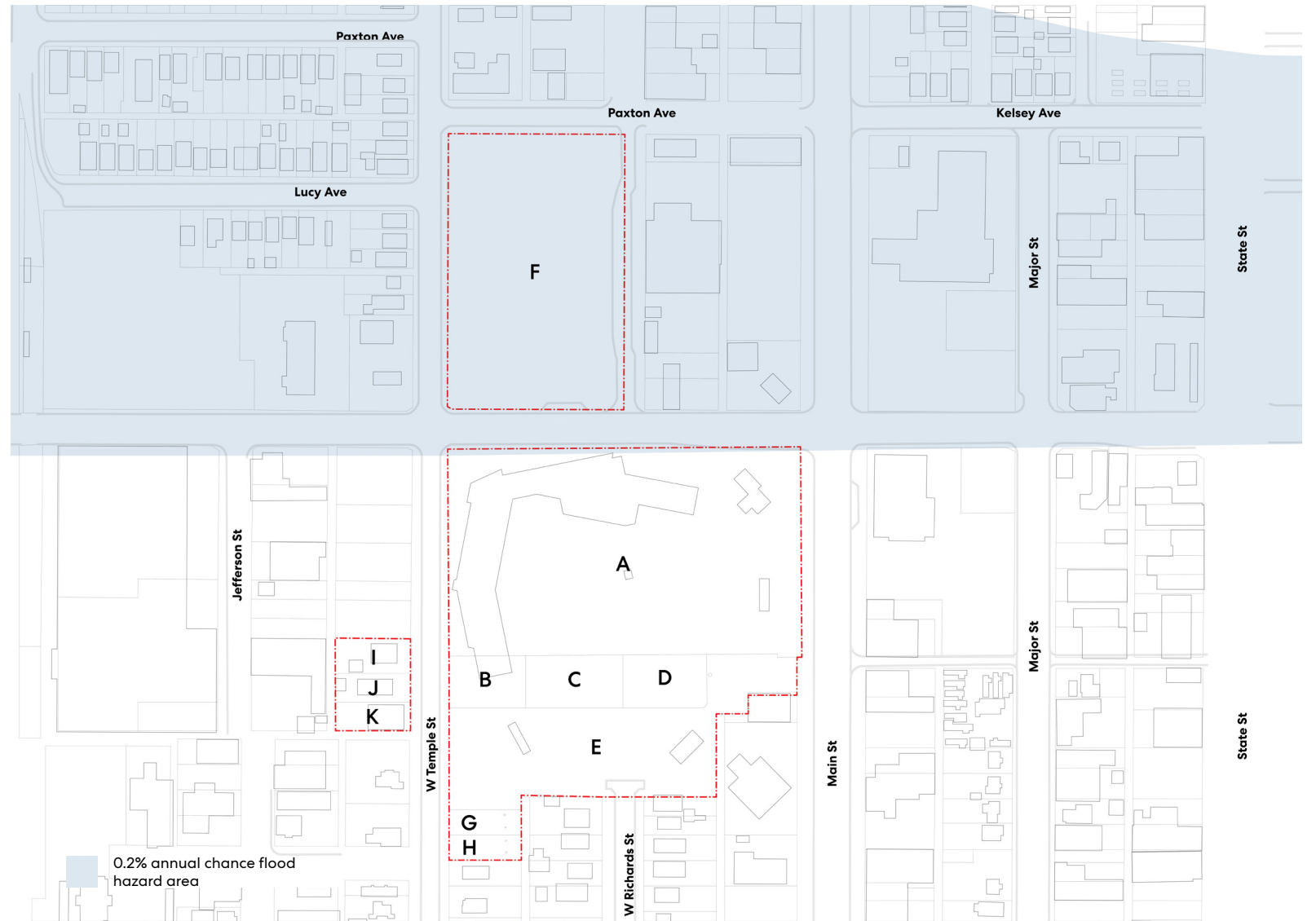
Utah R&I Properties, Contact Combat SLC, Advanced Solutions Property Management



02 — The Site

Climate Change: Flooding Floodplain

- Although no creeks have historically flowed through the Site, gradual transformation of the natural landform and underground channelization of the creeks has resulted in flood prone zones.
- Parcel F (see Floodplain Map) is located in an area with a 0.2% annual chance of flooding, also known as a “500-year flood zone.” The flood risk here is relatively low, and any potential floodwaters would likely be less than 1 foot deep or have a drainage area smaller than 1 square mile. Design exploration is required to develop strategies that mitigate the floodplain.



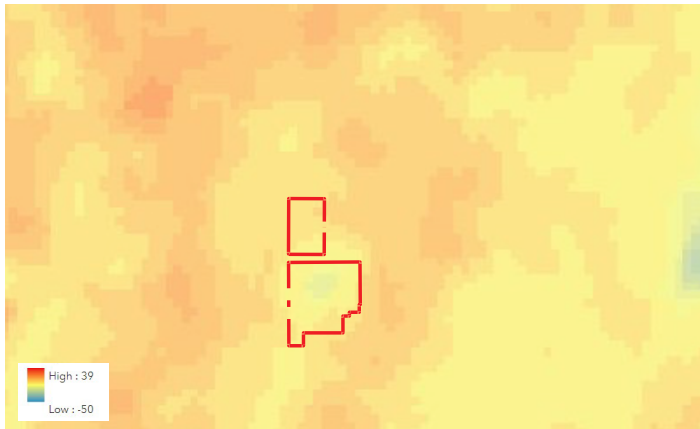
Floodplain Map, Source: FEMA

02 — The Site

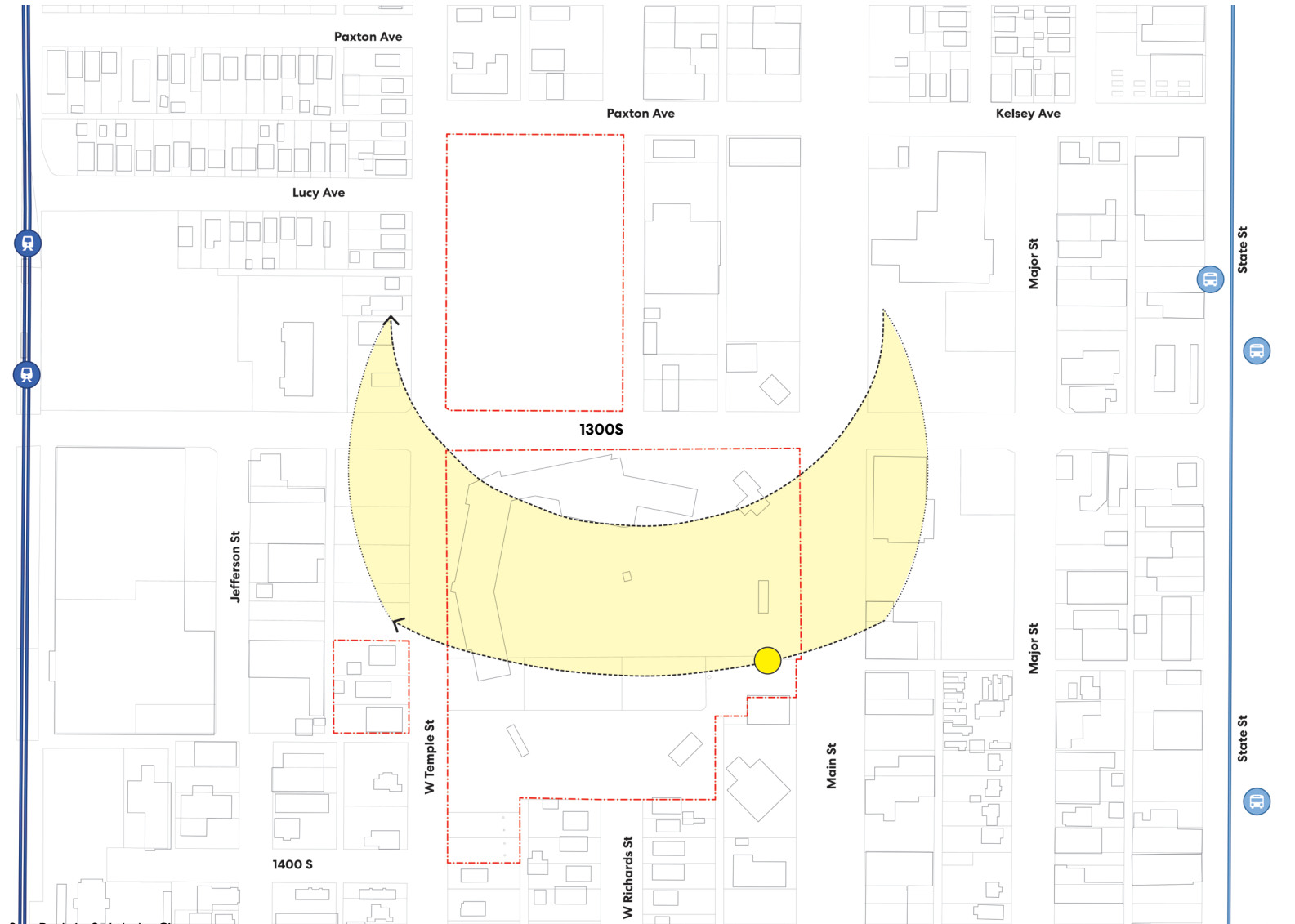
Climate Change: Heat

Urban Heat Island

- The Site experiences relatively lower urban heat island anomalies as compared to its surroundings, thanks to the Ballpark providing relief in the hardscape.
- However, majority of the Site still sits in the yellow zone on the heat island map, indicating moderate heat levels, due to a lack of vegetated green open spaces in the area. The Site receives strongest winds from South from June to April, and from the West from April to June.



Urban Heat Island Anomalies 2021



Sun Path in Salt Lake City

Weather Data Sources: Center for High Performance Computing University of Utah , Gaisma, World weather info

02 — The Site

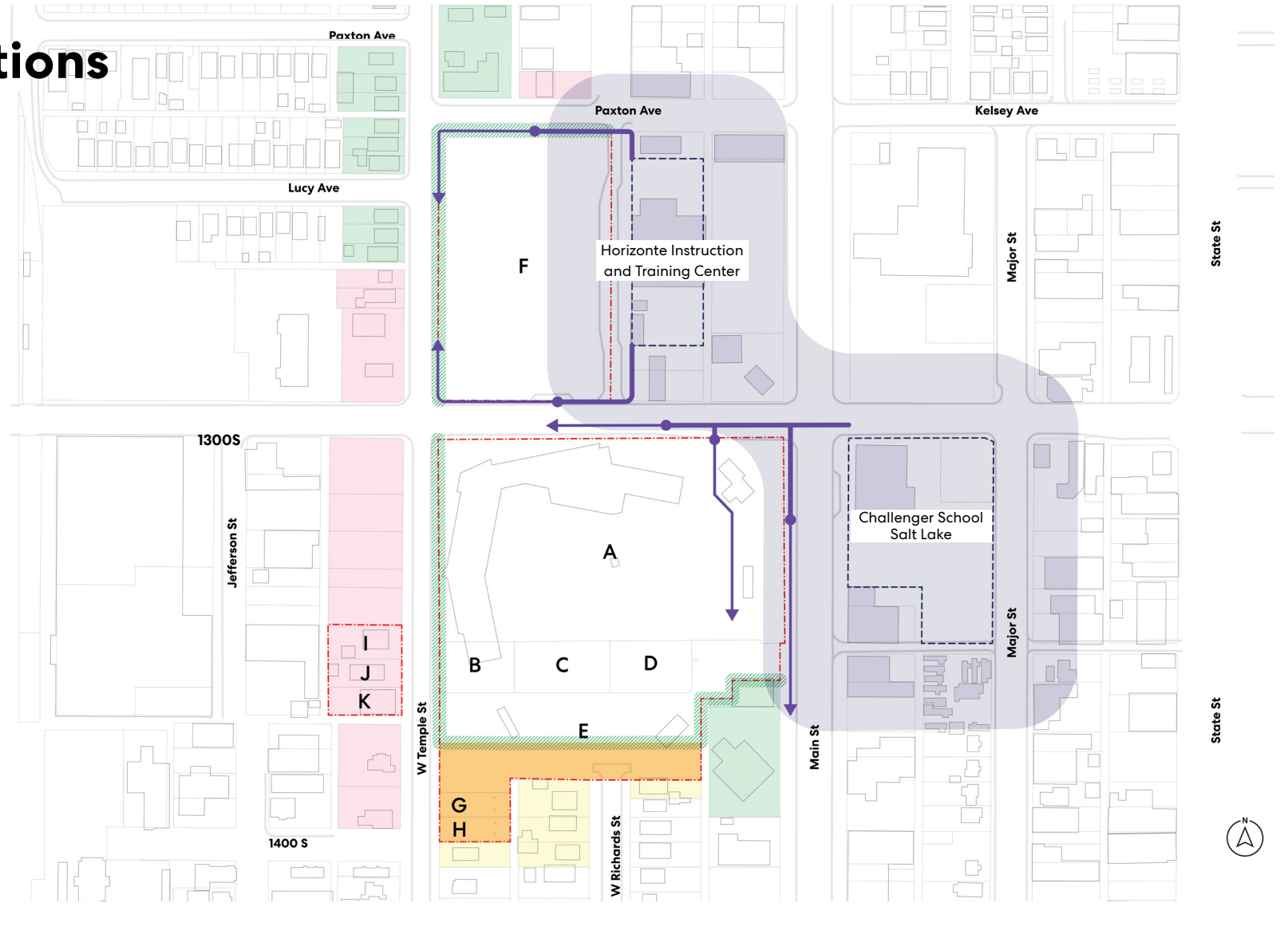
Alcohol Proximity Restrictions

As per the Department of Alcoholic Beverage Services, Pursuant to 32B-1-202, alcoholic beverage licenses are not granted within specific proximity measurements to a community location. A “community location” is defined as a church, public or private school, a public park, public playground or a library. Trade or technical schools, infant daycare centers and nursery schools are not considered community locations.

- Entrance to restaurants may not be located within 300 feet measured by ordinary pedestrian travel, or 200 feet measured in a straight line.
- Entrance to all other outlets may not be located within 600 feet measured by ordinary pedestrian travel, or 200 feet measured in a straight line.
- Buffering is required where a tavern, bar establishment, or brewpub abuts a residentially zoned parcel. Said buffering shall include vegetative landscaping or walls along any property line or within any required yard area on the lot where the premises are located.

Legend

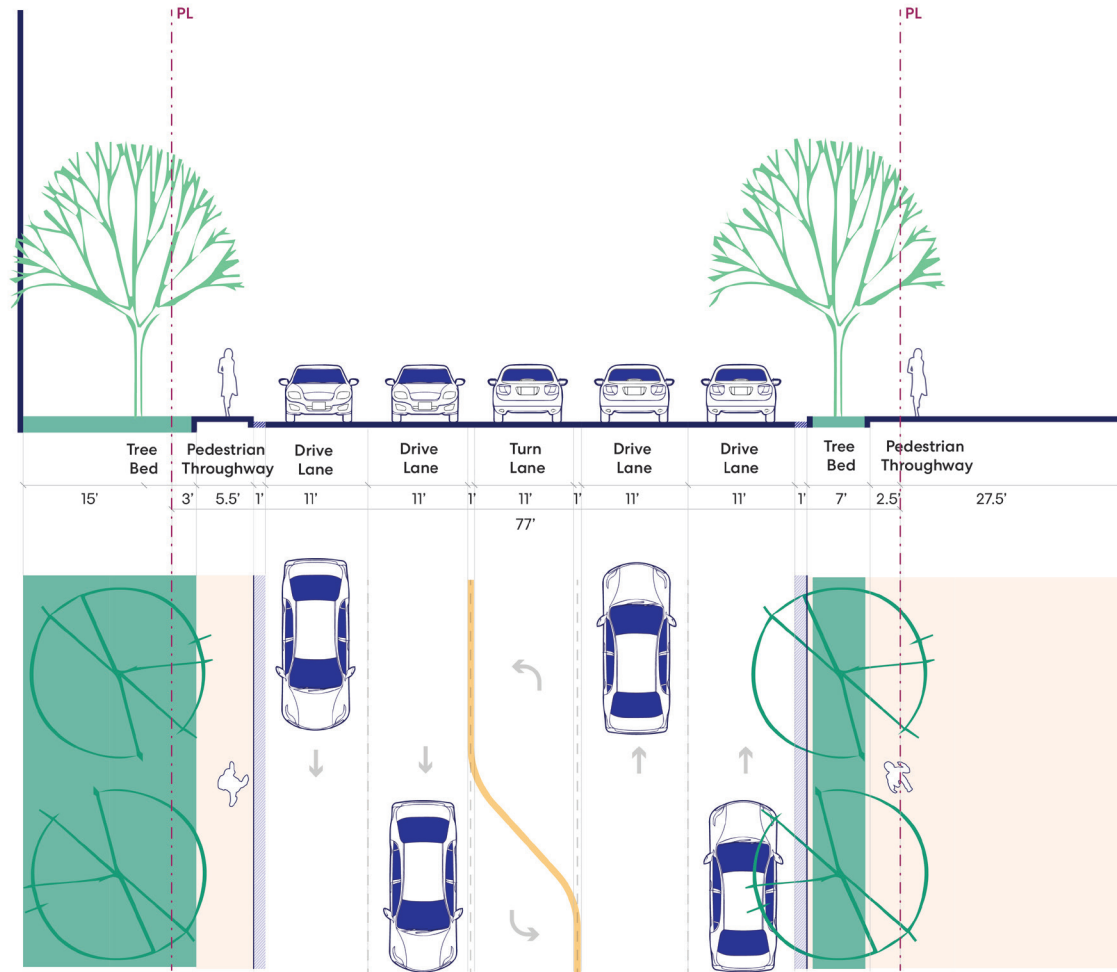
- | | |
|---|--|
| Ballpark NEXT Site Boundary | TRAX Transit Stop and Line |
| Ordinary pedestrian travel (300-600 feet from school) | Bus Transit Stop and Route |
| Ordinary pedestrian travel (0-300 feet from school) | R-1-5000 |
| 200 feet measured in a straight line | RMF-35 Multifamily Residential |
| School Property Boundary | MU-8-Form Based Mixed Use *Proposed |
| Landscaped Buffer from Residentially zoned parcels | FB-UN2-Form Based Urban Neighborhood 2 |



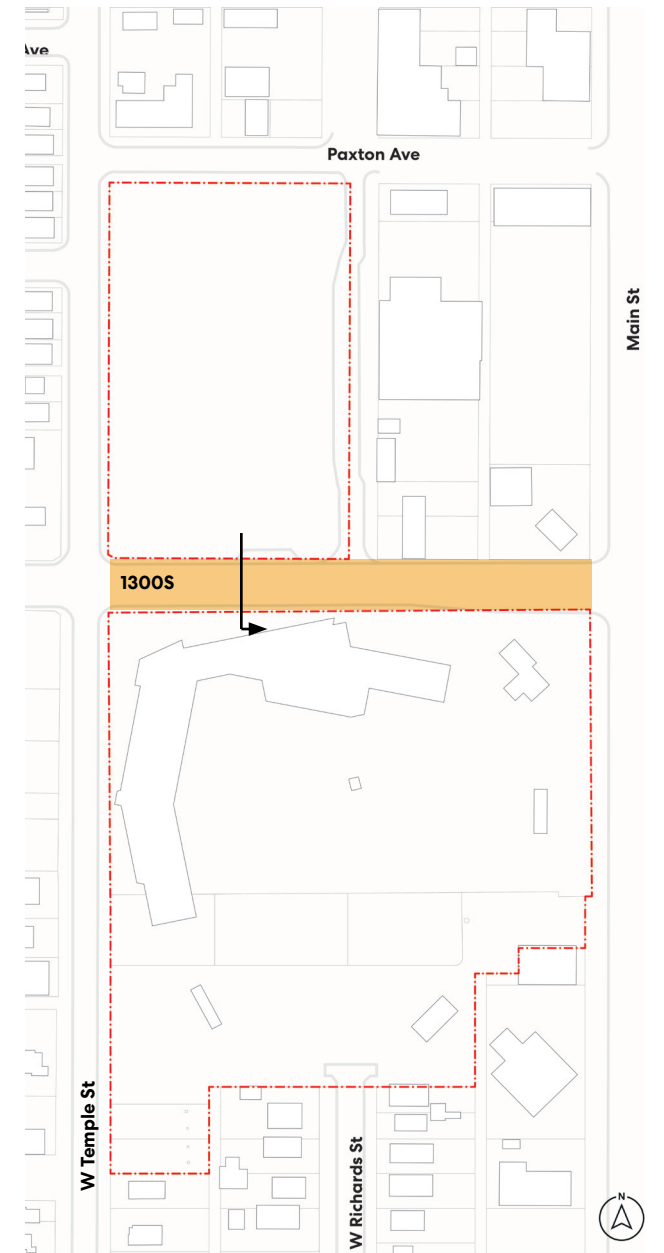
02 — The Site

Existing Street Sections

1300 South

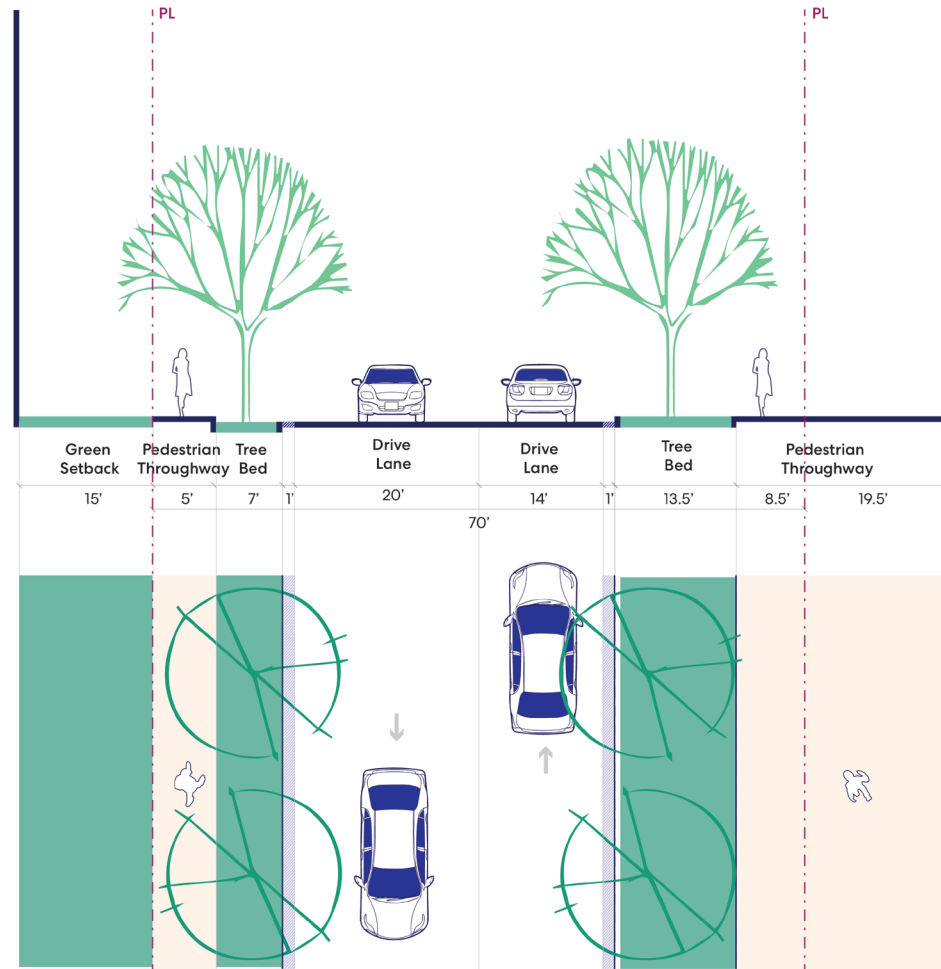


1300 South Cross Section

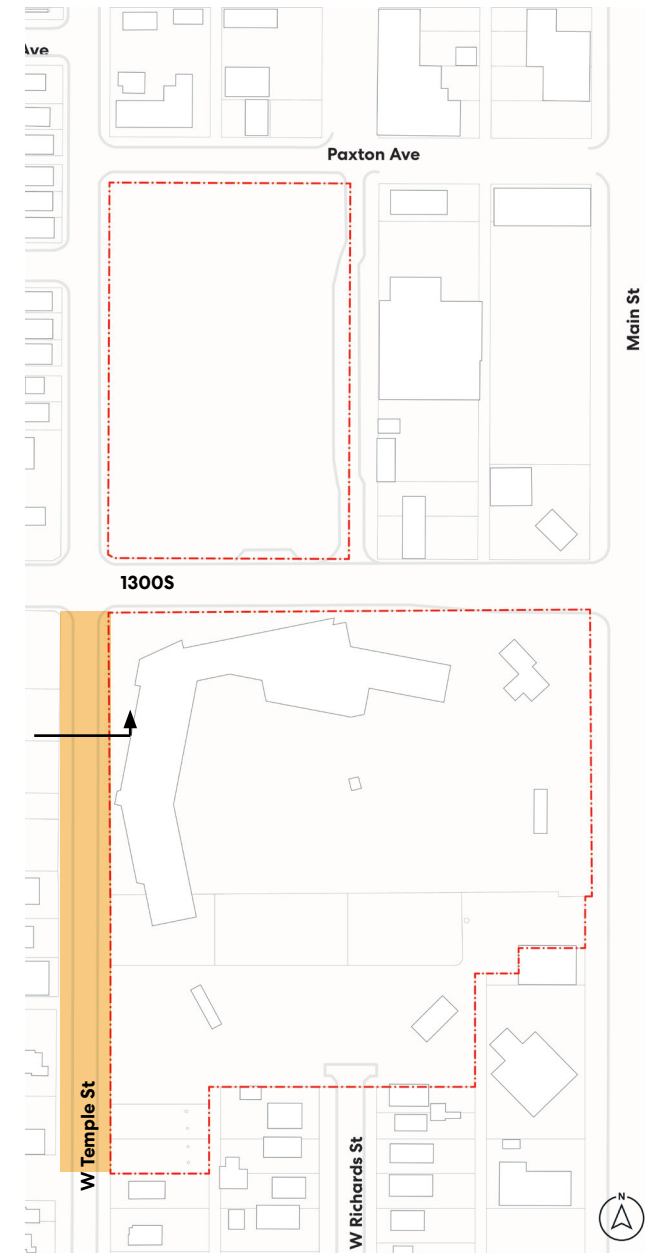


Existing Street Sections

SW Temple Street



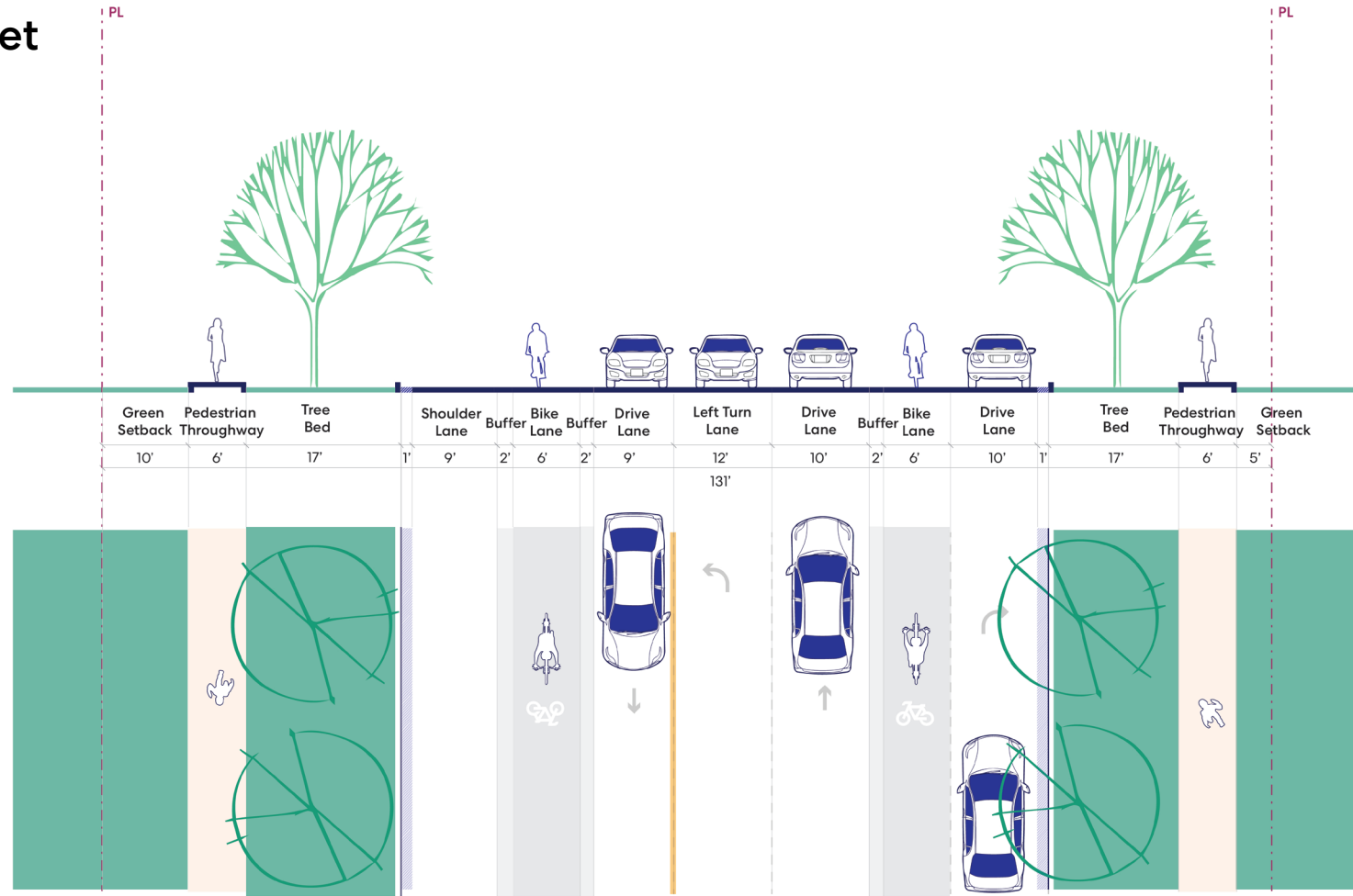
South West Temple Street Cross Section



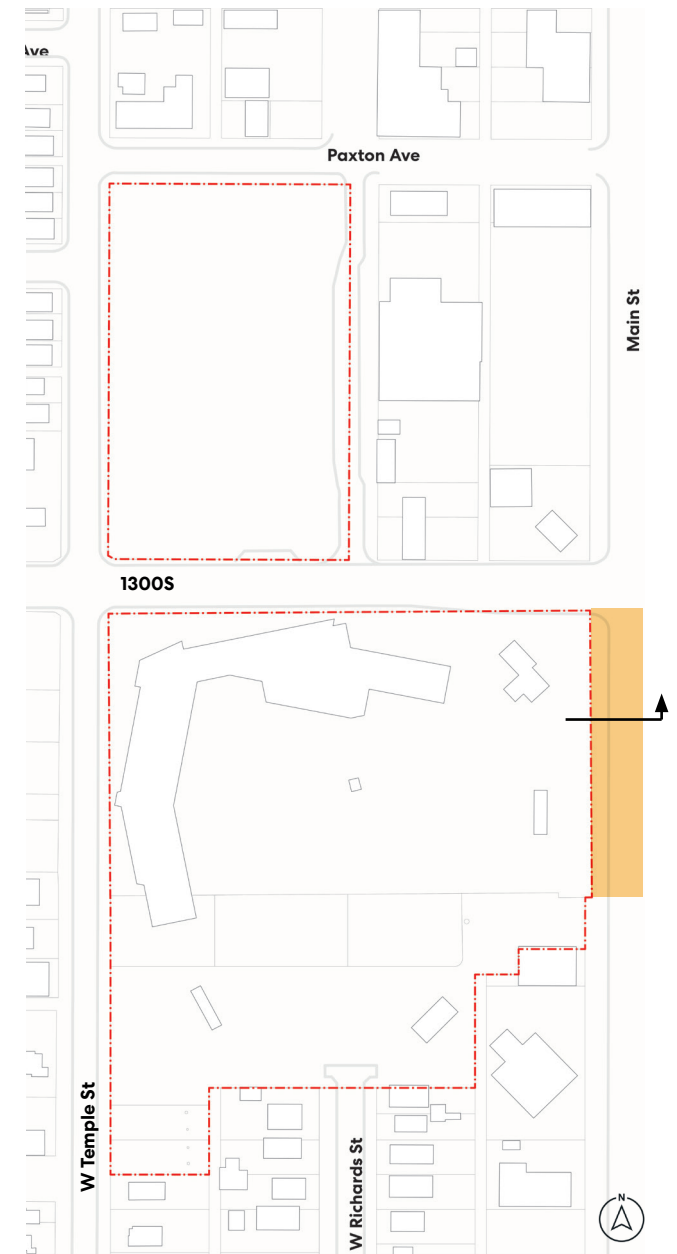
02 — The Site

Existing Street Sections

Main Street



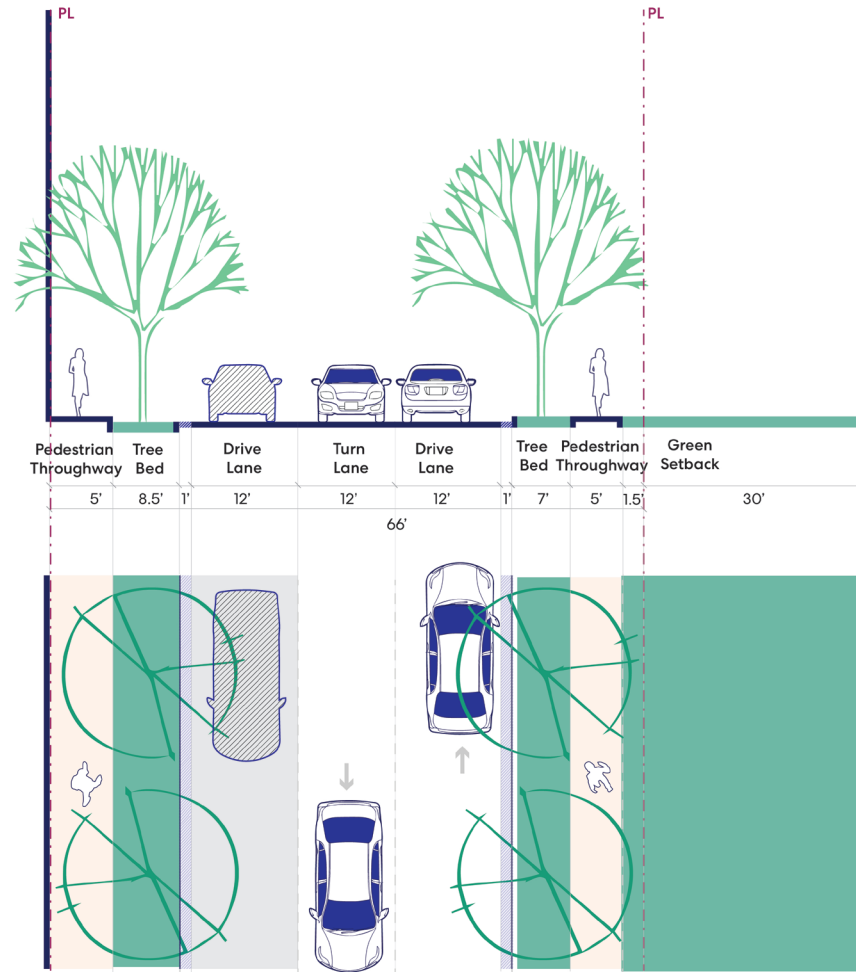
Main Street Cross Section



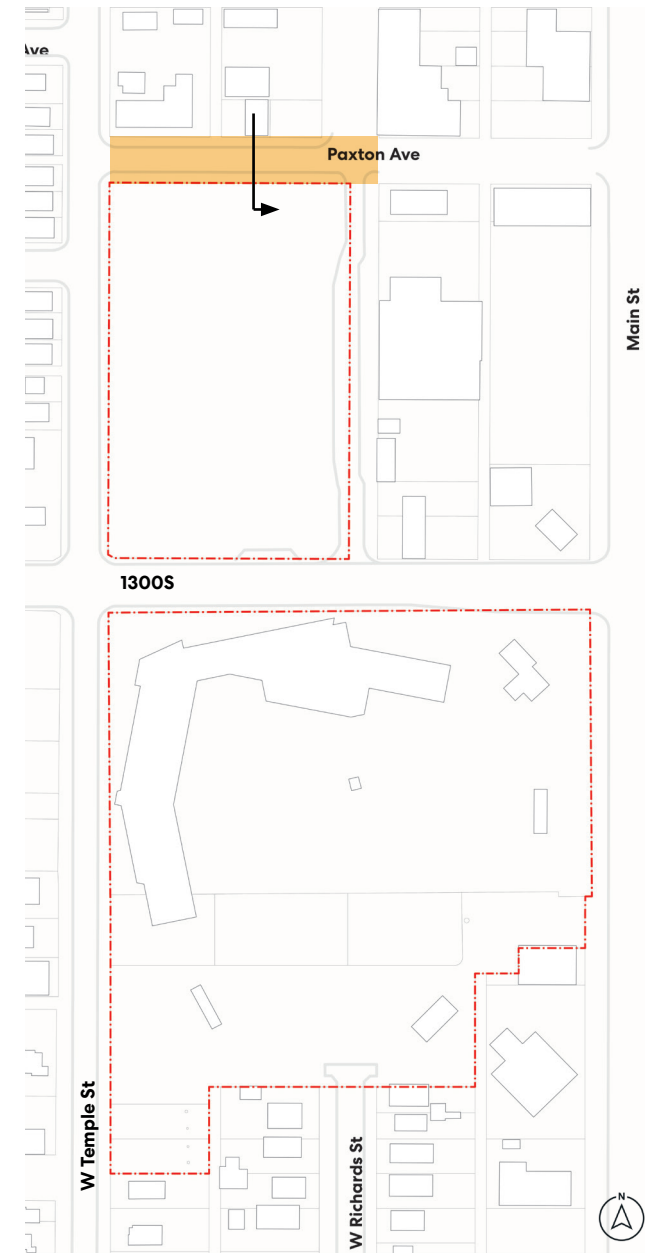
02 — The Site

Existing Street Sections

Paxton Avenue



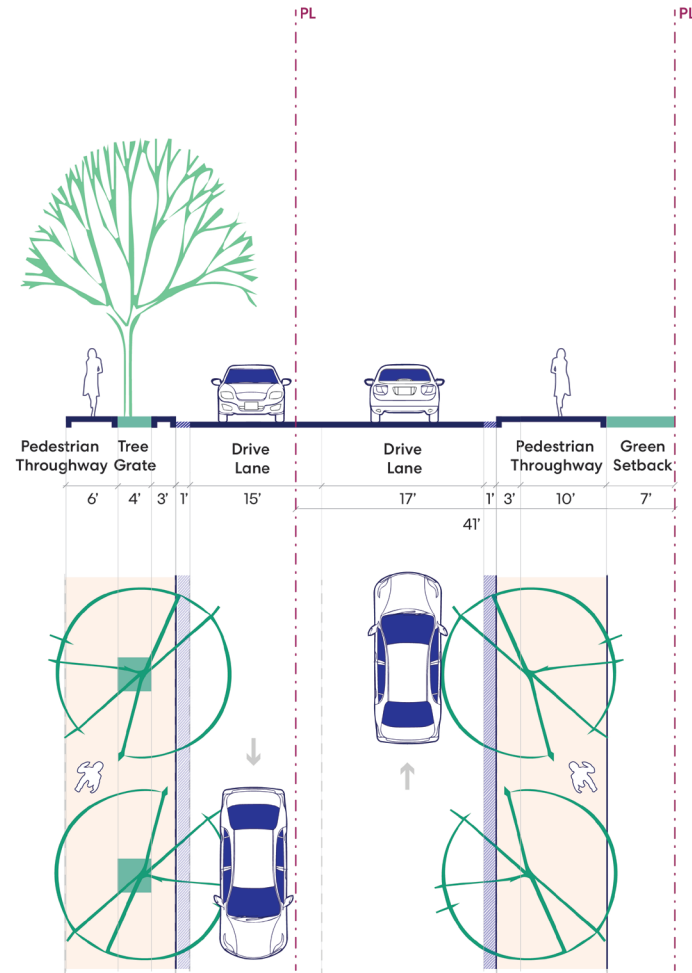
Paxton Avenue Cross Section



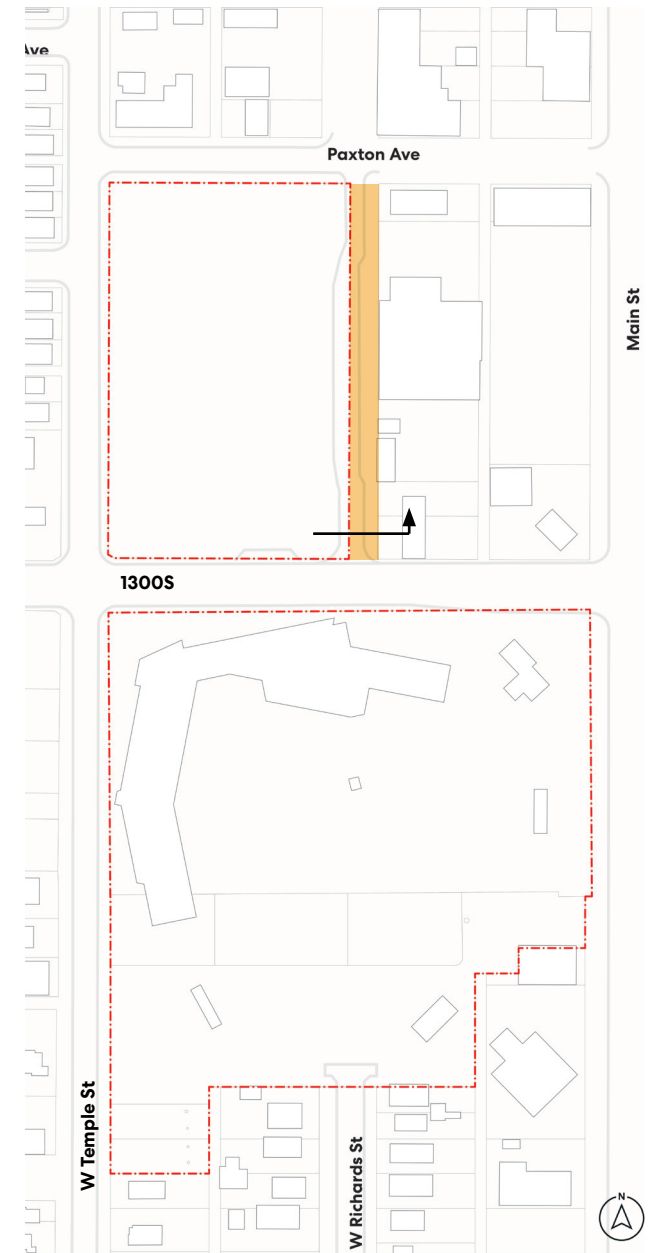
02 — The Site

Existing Street Sections

West Richards Street



West Richards Street Cross Section



Existing Plans and Stakeholder Input

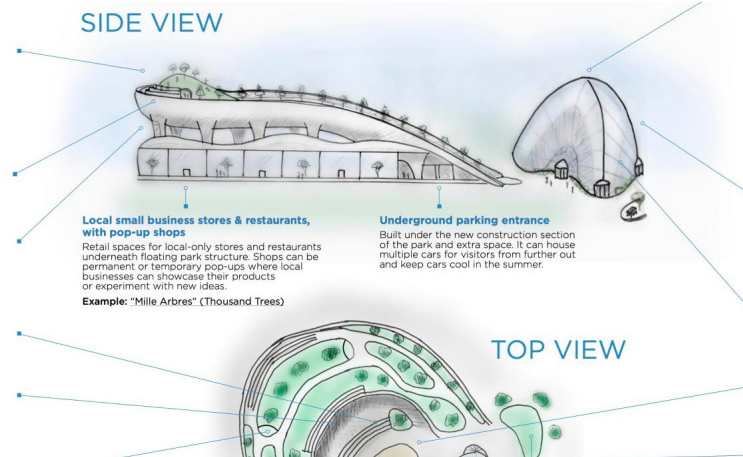
03 — Existing Plans and Stakeholder Input

Ballpark NEXT Design Competition (2023)

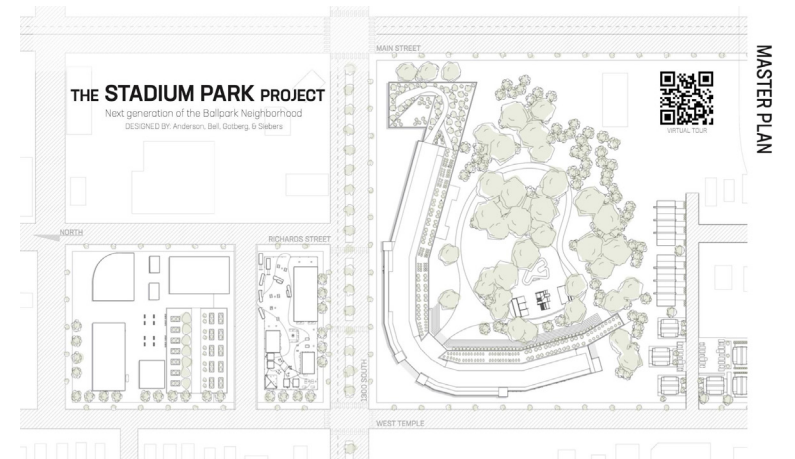
Professional Finalists



Resident Finalists



Student Finalists



Ballpark NEXT Vision Statement (2023)

The future Ballpark Site is an iconic, exciting destination for the neighborhood, city, and region. It is a catalyst for the transformation of our neighborhood, attracting people and development that uplifts and celebrates the area and its residents.

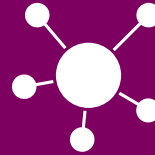
Salt Lake City Ballpark NEXT

03 — Existing Plans and Stakeholder Input

Ballpark NEXT Guiding Principles (2023)



**Neighborhood Safety and
Activation/ Safety in Every
Stride**



**Connectivity/
Interwoven**



**Natural Geography/
Be Green**



**Wellness/ Culture
of Health**



**Community-Centered/ A Space
for Every Face**



**Acknowledge the Past/ Honor
the Neighborhood's History**

03 — Existing Plans and Stakeholder Input

Ballpark Station Area Plan (2021)

Outlined below are the key actions proposed around the Ballpark in the Station Area Plan:

- Festival Street**
 A segment of West Temple between 1300 South and 1400 South has been identified as a future Festival Street.
- Proposed Bike Routes**
 - 300 West: Reconstruction, done by 2022
 - Main Street: Bikeway under review
 - Paxton: New bikeway to 300 West
- Multimodal Access**
 Links Main Street bikeway to TRAX
- Pedestrian Connection**
 - 1300 South to Ballpark: Links station to Festival Street via private development
 - West Temple to 300 West: Future connection, pending UTA agreement
- Potential Future Ballpark Public Space**
 Recommended: Trail, plazas, public areas
- Proposed Crossings**
 - 1300 South: Additional UTA crossings
 - Paxton Avenue: Crossing at TRAX line
 - 1300 South/West Temple: Enhanced crossing
- TRAX and Trail**
 UTA is evaluating potential TRAX service with a possible adjacent trail and green spaces as this area gets developed.

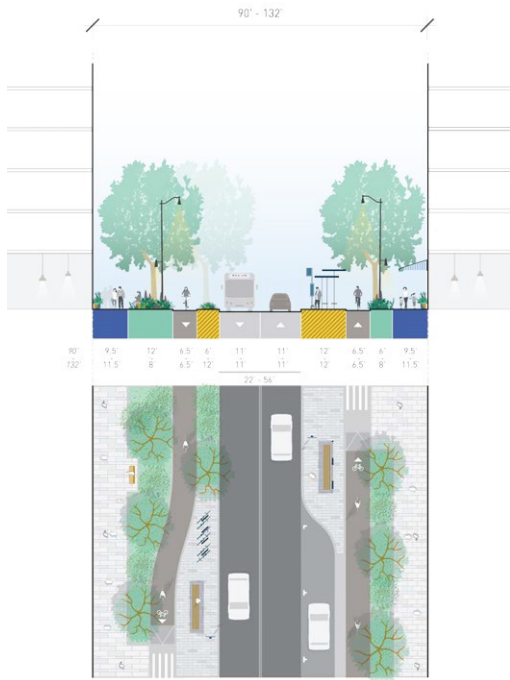


Recommended connections in the 'Heart' of the neighborhood.

- | | | | |
|-------------------------------------|---|-------------|---------------------------------------|
| *Potential Public Space at Ballpark | Festival Street | Green Space | Festival Street |
| Proposed Bike Routes | Proposed Future TRAX with Possible Adjacent Trail | TRAX Line | Connection through Future Development |
| *Multi-Modal Access | *Pedestrian Connection Through Future Development | Bikeway | *Dependent on owner agreement |
| Proposed Crossings | TRAX Line | | |

03 — Existing Plans and Stakeholder Input

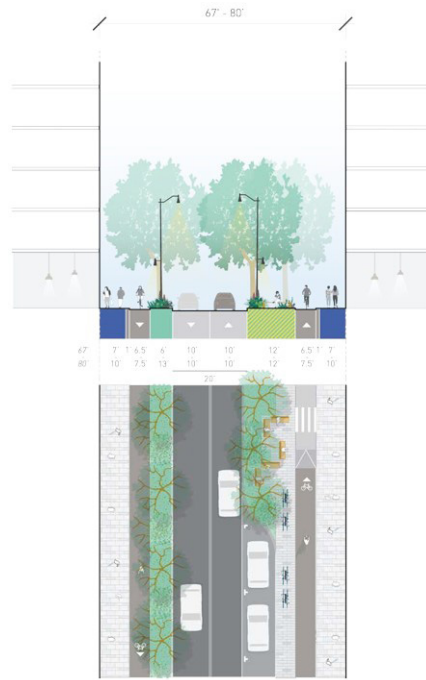
SLC Street Intersection Typologies Design Guide (2020)



1. Urban Village Main Street

Main street in or connecting urban village centers with multiple land uses and building types, where activity, movement, sense of place, and access are important.

- 1300 South
- Main Street
- South West Temple Street



2. Urban Village Street

Predominantly residential street in an urban village with some additional land uses, where neighbors spend time, and where trips begin and end.

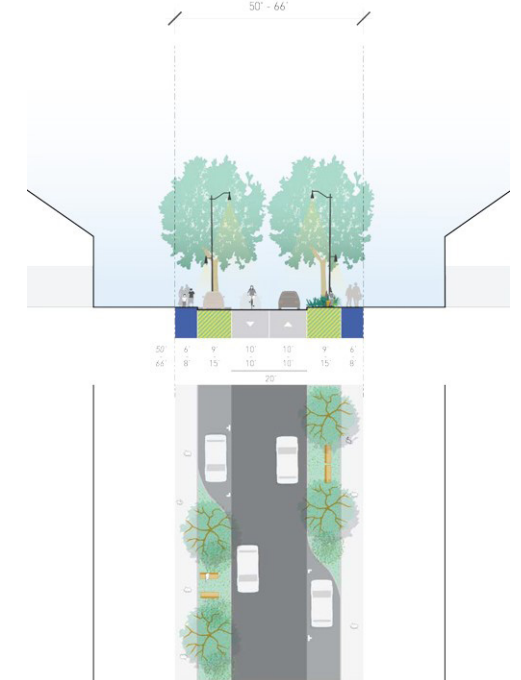
- Richards Street



3. Neighborhood Corridor

Principal street through and/or between neighborhoods, with a greater focus on residential uses than an Urban Village Main Street.

- West Temple Street



4. Neighborhood Green Street

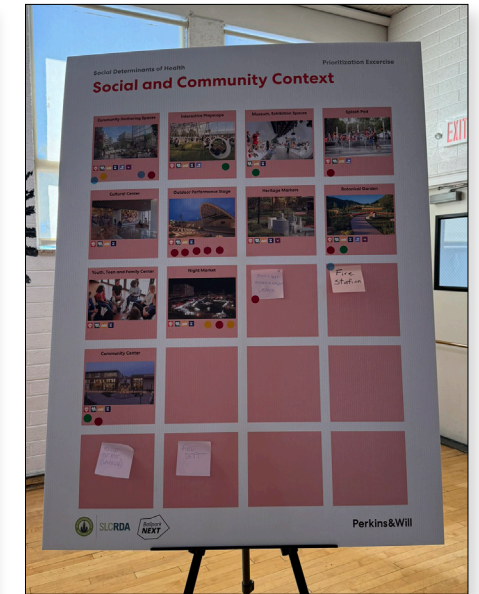
A Neighborhood Street where greening and traffic calming are prioritized, and where walking and bicycling may be higher than on busier corridors.

- Paxton Avenue

03 — Existing Plans and Stakeholder Input

Stakeholder Ideation Session 1: Prioritization Exercise Results

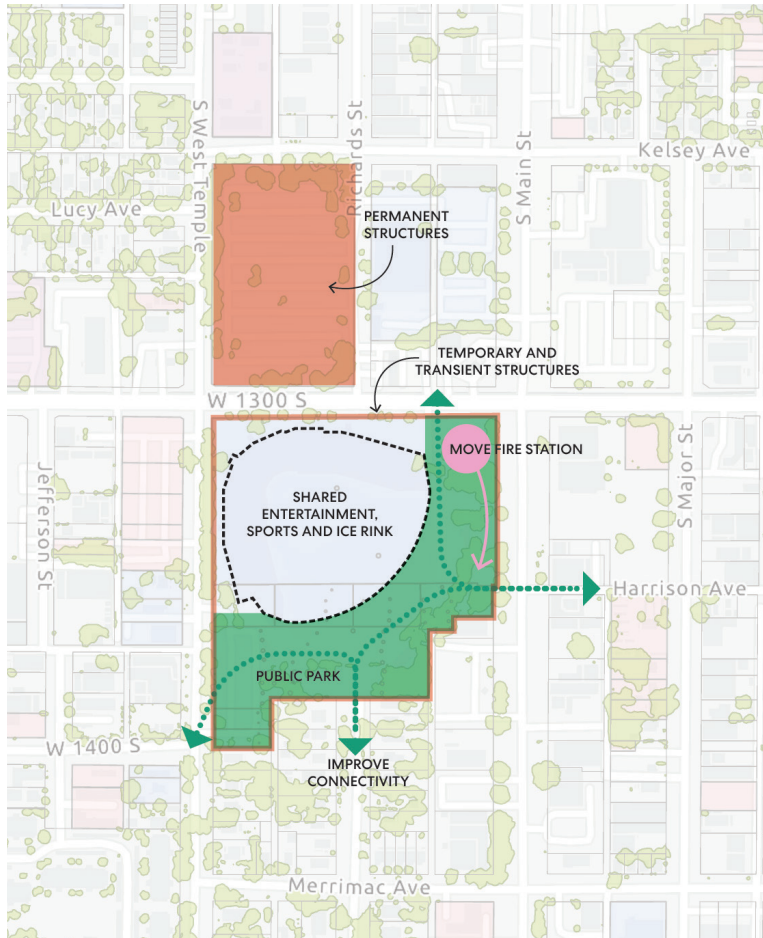
- Public Market/
Food Hall
- Ground Floor Local
Businesses
- Subsidized Amenities
for Ballpark Residents
- Public Library
- Arts and Cultural
Workshops
- Outdoor
Exploratorium
- Produce Market
- Fitness Center
- Wide Sidewalks and
Protected Bike Lanes
- Neighborhood
Square
- Mixed-Use
Development
- Outdoor Performance
Stage
- Community Gathering
Space
- Night Market



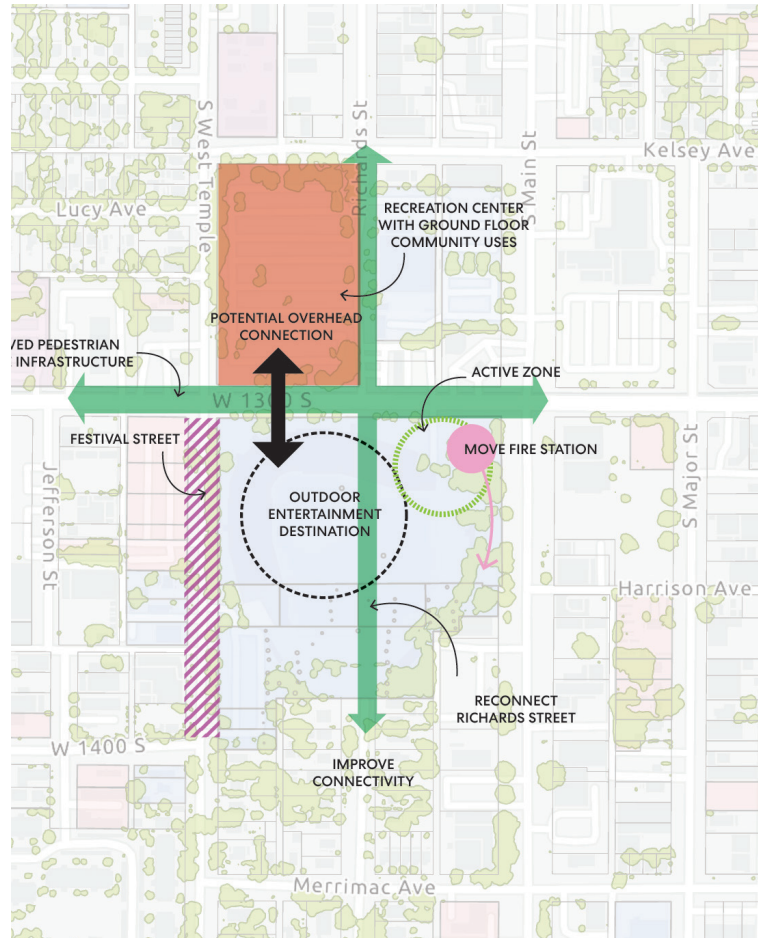
03 — Existing Plans and Stakeholder Input

Stakeholder Ideation Session 1: Breakout Group Takeaways

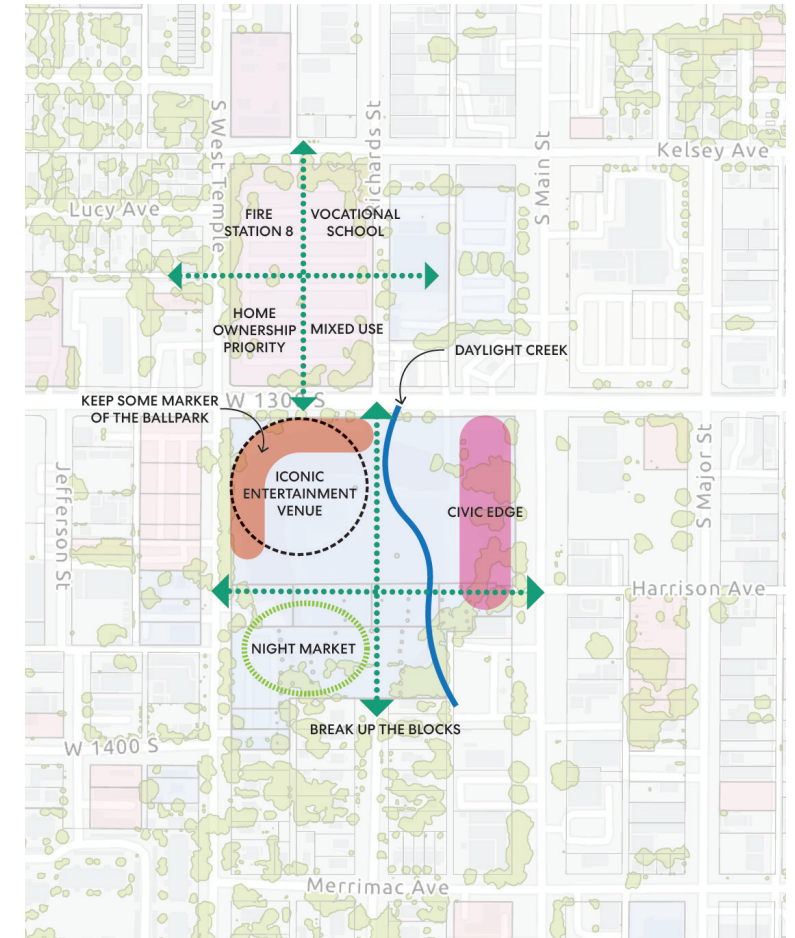
Scenario 1: Preserve the Ballpark



Scenario 2: Adaptive Reuse



Scenario 3: Remove



03 — Existing Plans and Stakeholder Input

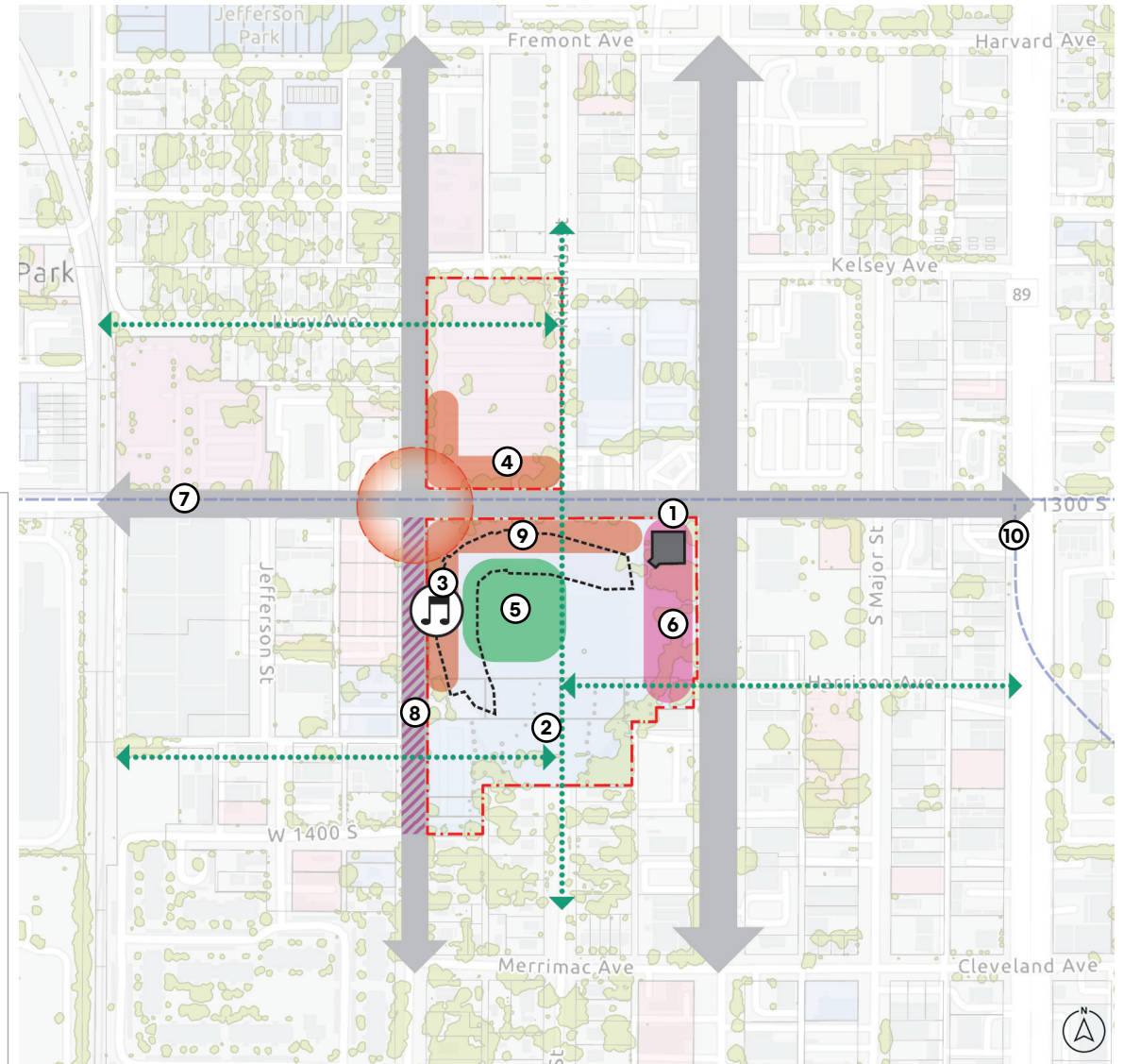
Stakeholder Ideation Session 1: Common Themes

Common themes across breakout groups during the Stakeholder Ideation Session 1 on 25 August 2024 were -

1. Relocate Fire Station 8 to enhance emergency response times while incorporating community space into the station's campus.
2. Enhance connectivity across the blocks by creating pedestrian and/or vehicular streets.
3. Entertainment and Concert Venue
4. Activate the ground floor to accommodate small, local businesses.
5. Create an open green space for children and the community at large.
6. Accommodate civic uses such as a public library, fitness center, etc.
7. Improve 1300 S to be a safe street for pedestrians and vehicles alike.
8. Carry forward the "Festival Street" idea from the Station Area Plan.
9. Consider partial preservation of the Ballpark structure or a symbolic nod to the sites baseball legacy as a marker of history.
10. Consider design moves that acknowledge and celebrate the confluence of Red Butte, Emigration, and Parley's Creeks.

Legend

- Ballpark NEXT site boundary
- Public properties
- Vacant properties
- Festival street
- Park
- Ground floor small businesses
- Civic edge on Main St
- Fire station structure
- Entertainment node
- Smith's Ballpark structure
- Mid-block paths
- Tree cover
- Channelised creeks



Key strategies from stakeholder ideation session

Appendix B:
Health Equity Site Assessment



SLCRDA



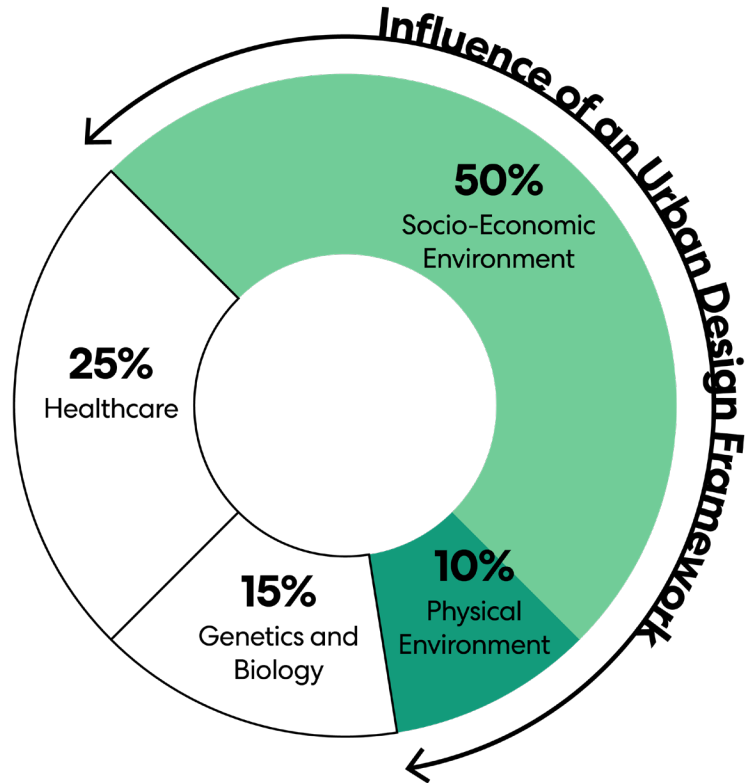
Ballpark Next

Urban Design Framework

Health Equity Site Assessment

Perkins&Will

Why conduct a *Health Equity Assessment*?



The World Health Organization (WHO) defines health as a state of complete physical, mental, and social well-being and not merely the absence of disease or infirmity.

According to the Canadian Institute for Advanced Research, the built environment can influence up to 60% of the factors that determine overall health.

Contents

01 — Health Equity Assessment

- Ballpark Neighborhood Community Profile
- Ballpark Neighborhood Demographic Trends
- Built Environment and Health
- Ballpark Neighborhood Health Overview
- Social Determinants of Health
- Health Outcomes
- Social Vulnerability Index
- Environmental Justice Index
- Key Health Equity Takeaways

02 — Design Recommendations Priority Matrix

- Design Strategies & Program Elements
- Stakeholder Prioritization Exercise Results
- Urban Design Framework Development

Ballpark Neighborhood Community Profile

Population

5,683

Median Age

35.5 years

Median Household Income
(In 2022 Inflation Adjusted Dollars)

\$46,182

Educational Attainment (>25 years)

18.8% of the population has not completed high school

Labor Force Participation

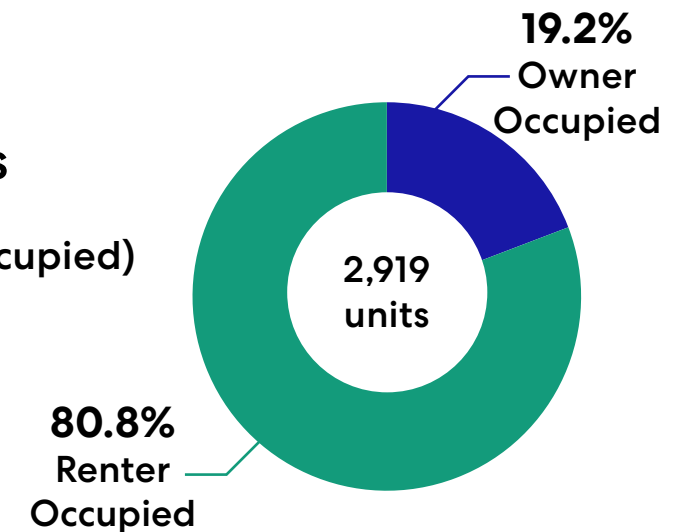
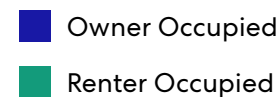
29.6% of the population is not in the labor force

Median House Value
(All Owner-Occupied Housing Units)

\$409,900

Housing Units

3,015 (2,919 occupied)

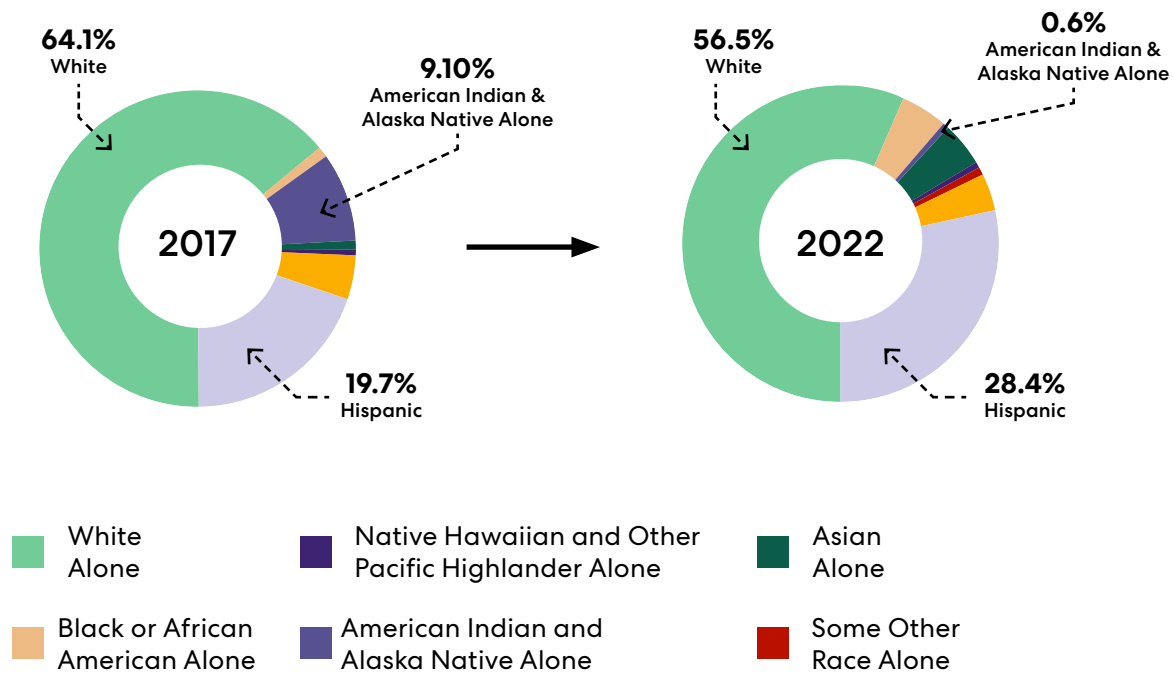


Source: American Community Survey 2022 (5-Year Estimate)

Ballpark Neighborhood Demographic Trends

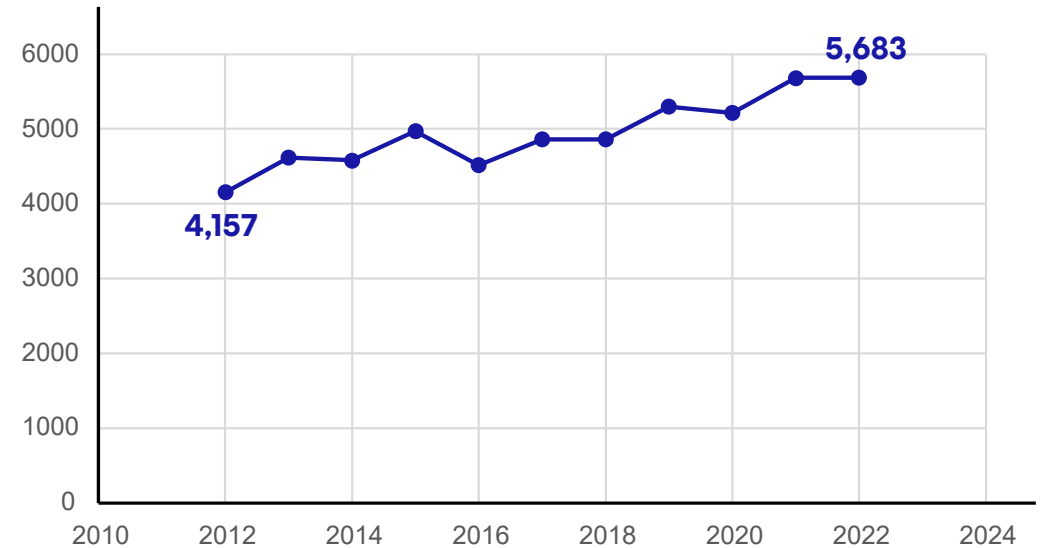
Race

The Ballpark Neighborhood has transformed from a predominantly White neighborhood in the early 20th century to a more diverse community with growing Hispanic, Asian, and Black populations. There has been a significant loss of American Indian and Alaska Native populations from the neighborhood.



Population Growth

The population of the Ballpark Neighborhood has grown steadily over the last 10 years, with a recent influx of young professionals, young families and students. This could be associated with recent residential developments in the neighborhood.



Source: American Community Survey 2012/2017/2022 (5-Year Estimate)

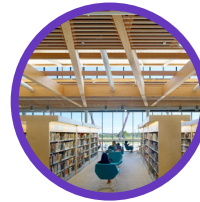
01 — Health Equity Assessment

Identifying Existing Socio-Economic and Health Conditions

Social Determinants of Health



Economic Stability



Education Access and Quality



Healthcare Access and Quality



Neighborhood and Built Environment



Social and Community Context

Design of the built environment influences travel choice, level of daily physical activity, eating / drinking patterns, mental health.



Urban Design Framework

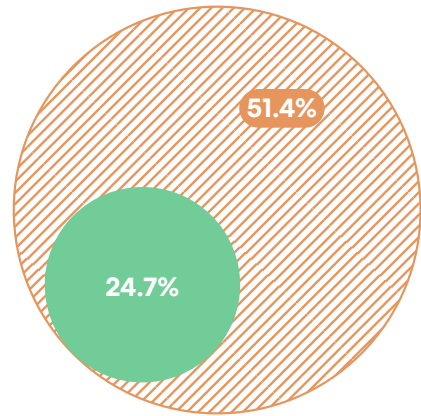
Direct Impact to the Health and Quality of Life of Ballpark Residents

01 — Health Equity Assessment

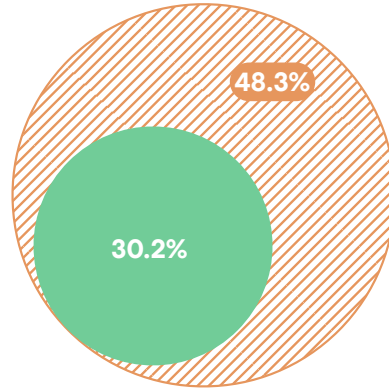
Ballpark Neighborhood Health Overview

Housing cost burden, racial/ethnic minority status, social isolation, and poverty emerge as the most impacted socio-economic health indicators in the neighborhood. While the prevalence of social isolation is nearly on par with the Salt Lake County average, the other indicators show a significant disparity.

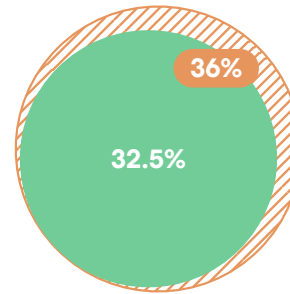
● Salt Lake County ● Ballpark Census Tract



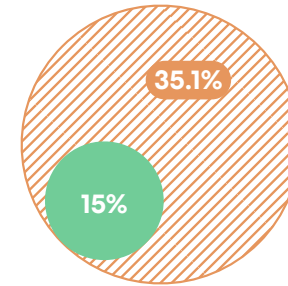
Housing Cost Burden



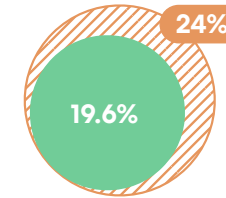
Racial/Ethnic Minority Status



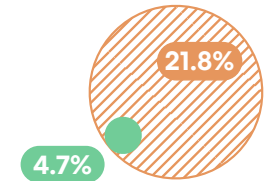
Social Isolation



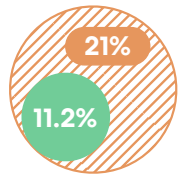
Poverty



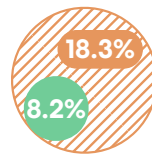
Physical Inactivity



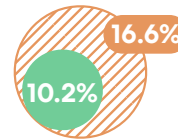
No Broadband



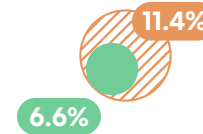
Food Insecurity



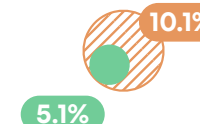
No High School Diploma



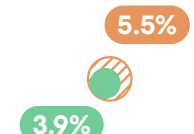
Lack of Health Insurance



Transportation Barriers



Single Parent Households

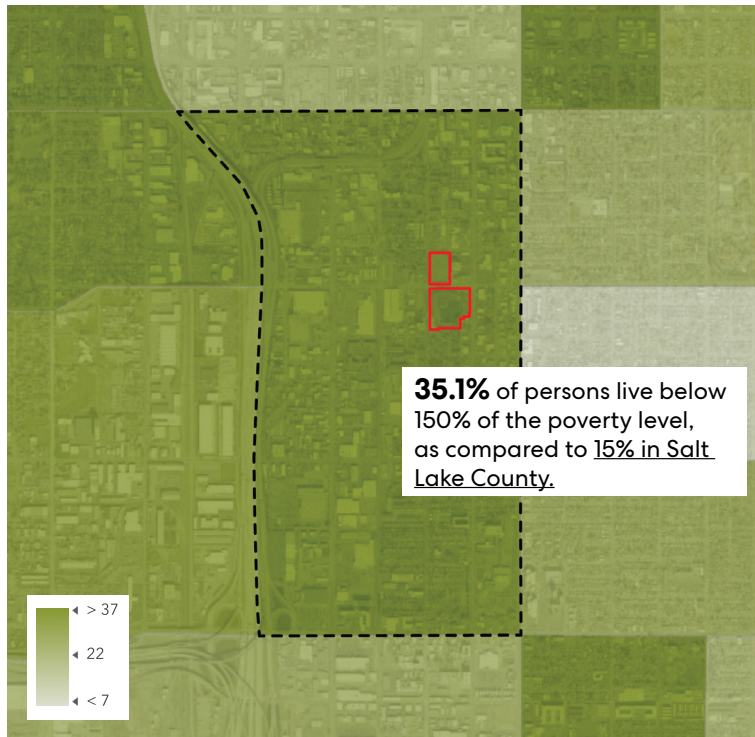


Unemployment

Social Determinants of Health

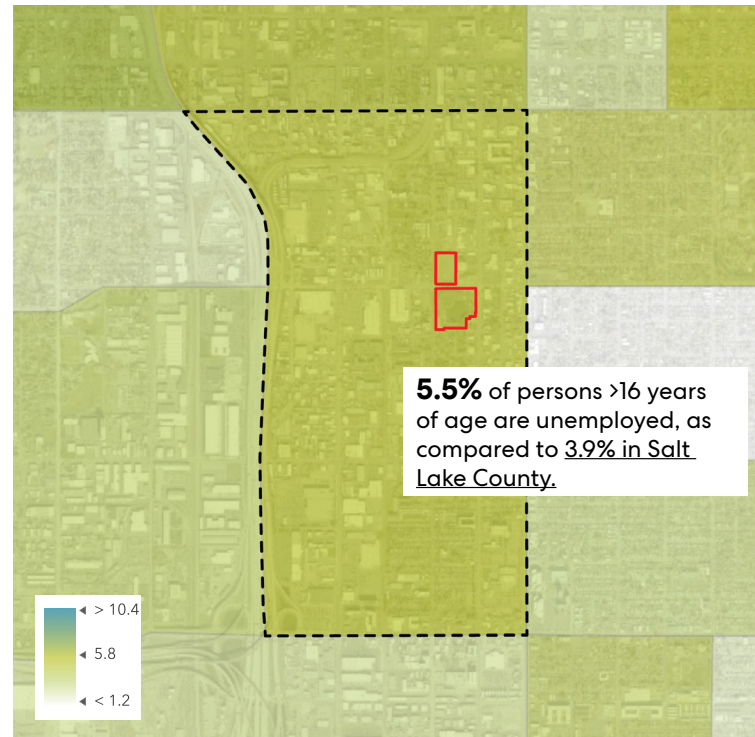
Poverty

Poverty is measured as the percentage of people living below 150% of the poverty level.



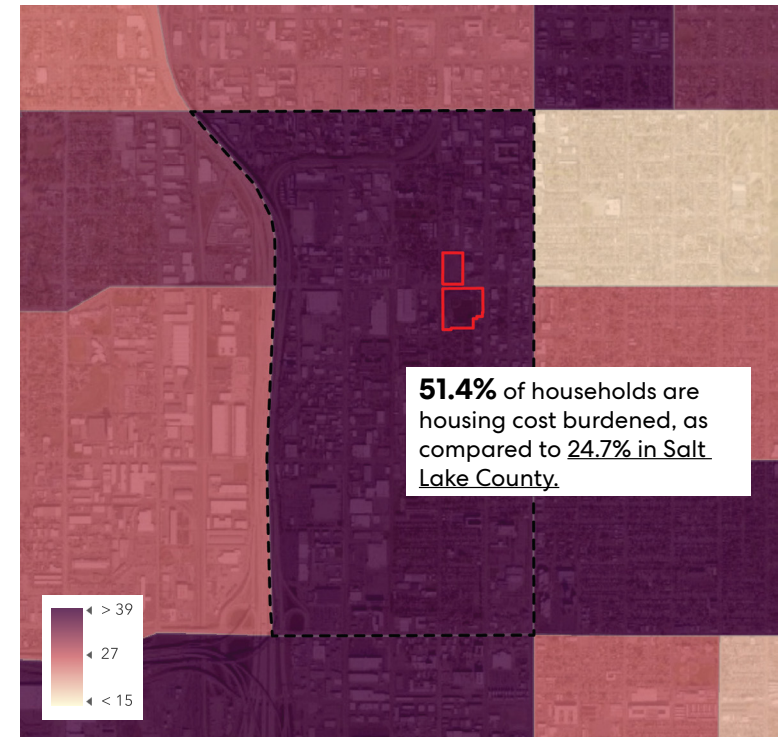
Unemployment

Unemployment is measured as the percentage of people aged 16 and older who are jobless, available to work, and have actively sought employment in the past four weeks.



Housing Cost Burden

Housing Cost Burden is measured as the percentage of households spending more than 30% of their income on housing costs.



Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

Social Determinants of Health

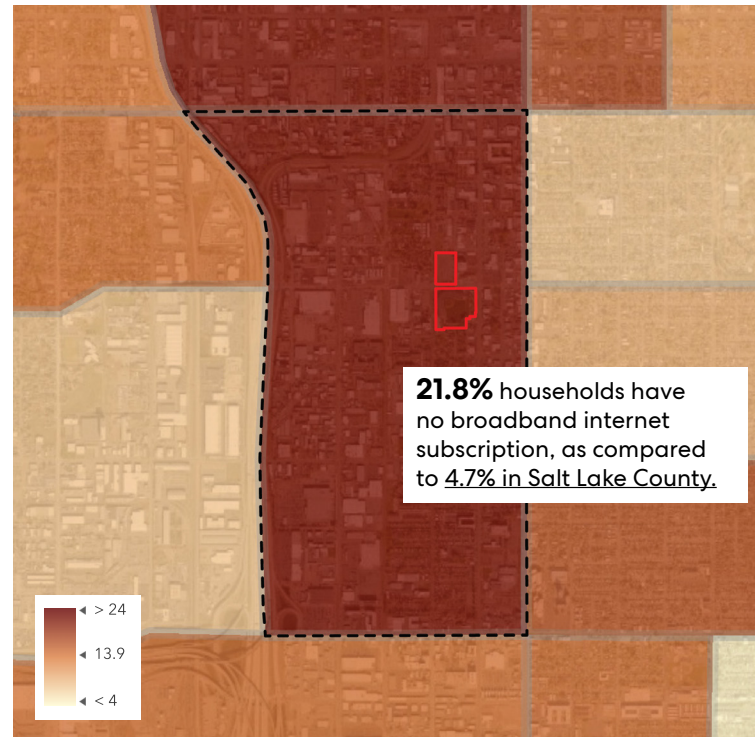
No High School Diploma

No High School Diploma is measured as the percentage of people aged 25 and older who have not earned a high school diploma or its equivalent.



No Broadband

No Broadband is measured as the percentage of households that do not have a broadband internet subscription.

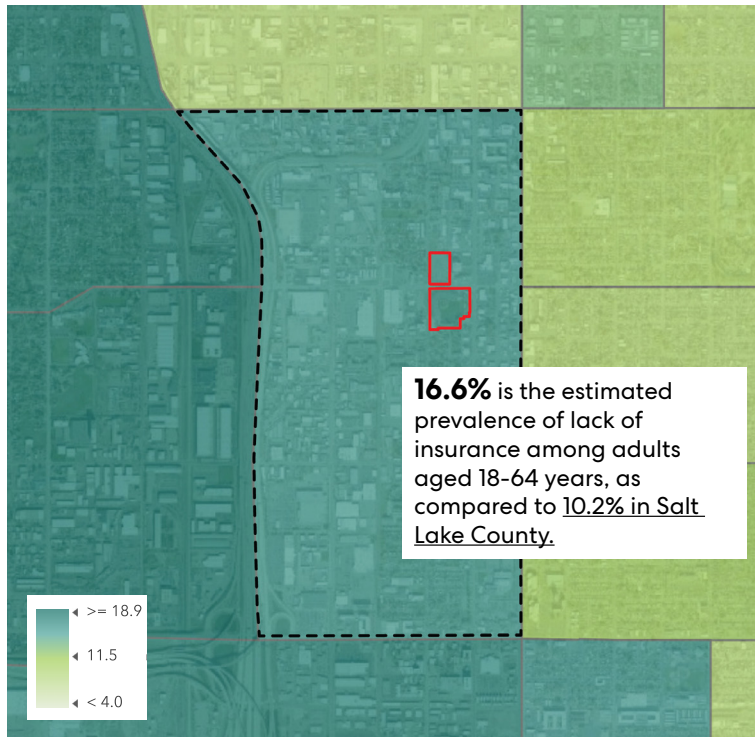


Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

Social Determinants of Health

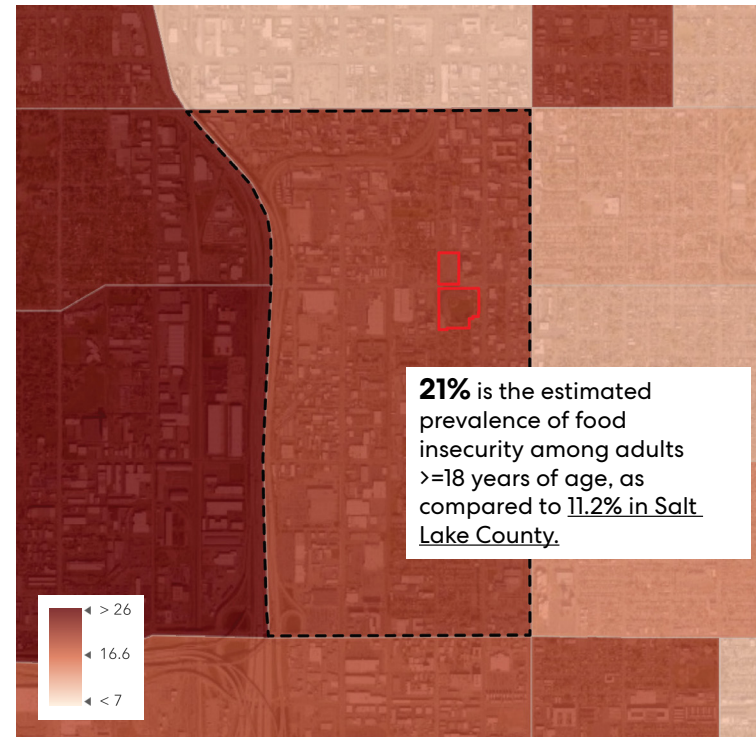
Health Insurance

Health Insurance is measured as the percentage of individuals with any form of health insurance coverage, including private, public, or government programs like Medicaid and Medicare.



Food Insecurity

Food Insecurity is measured as the percentage of households that lack reliable access to a sufficient quantity of affordable, nutritious food.



Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

Social Determinants of Health

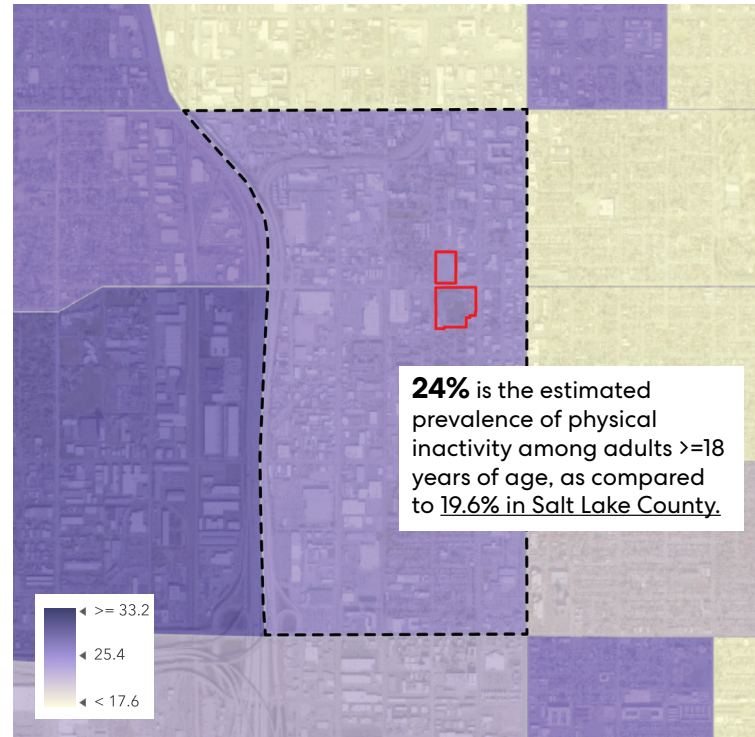
Transportation Barriers

Transportation Barriers refer to the percentage of individuals who face challenges accessing reliable and affordable transportation, which may affect their ability to reach essential services and opportunities.



Physical Inactivity

Physical Inactivity is measured as the percentage of adults aged 18 and older who report not participating in any leisure-time physical activities in the past month.

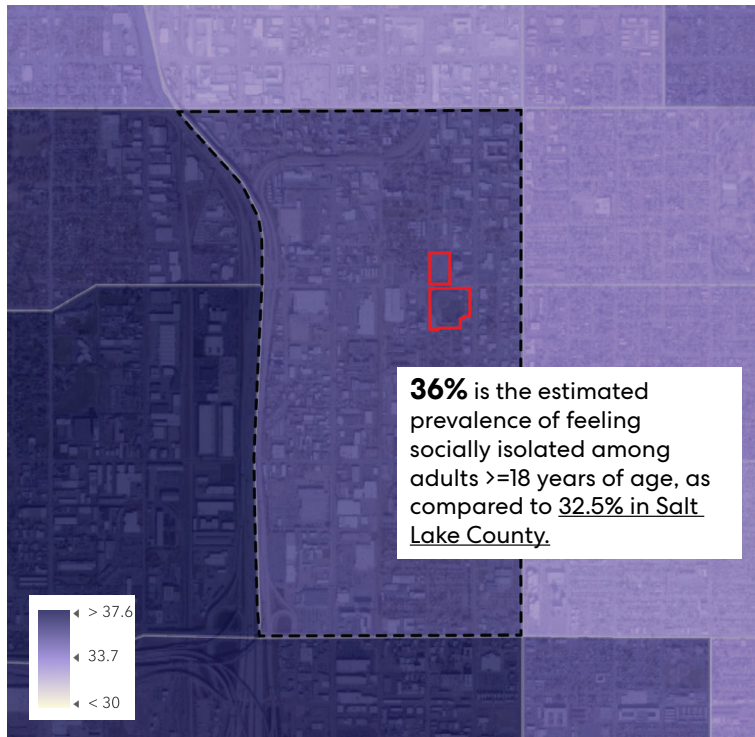


Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

Social Determinants of Health

Social Isolation

Social Isolation is measured as the percentage of adults aged 18 and older who report feeling lonely or lacking social support.



Single Parent Households

Single-Parent Households are measured as the percentage of families with children under 18 that are headed by a single parent, either mother or father.



Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

01 — Health Equity Assessment

Health Outcomes

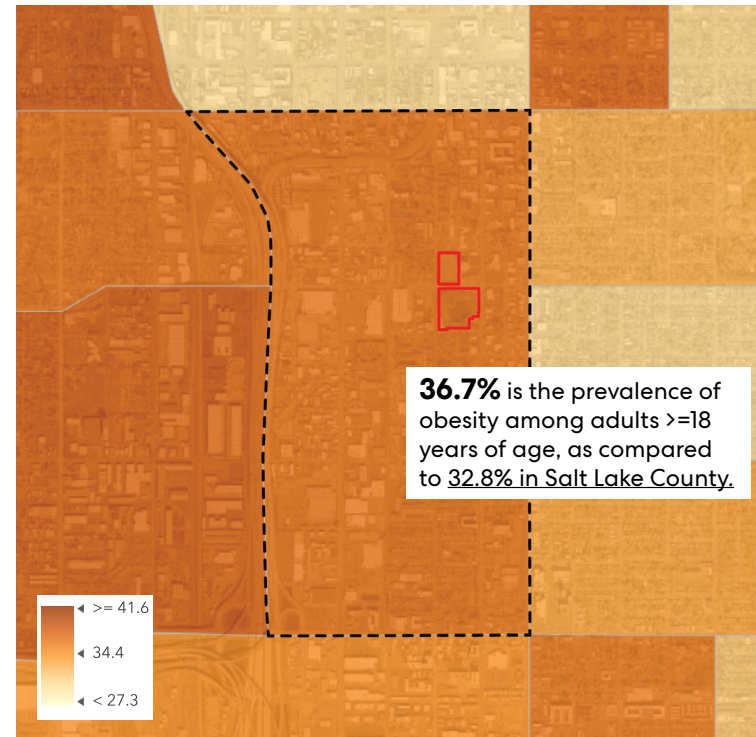
Asthma

Asthma is measured as the percentage of adults aged 18 and older who have ever been diagnosed with asthma by a healthcare professional.



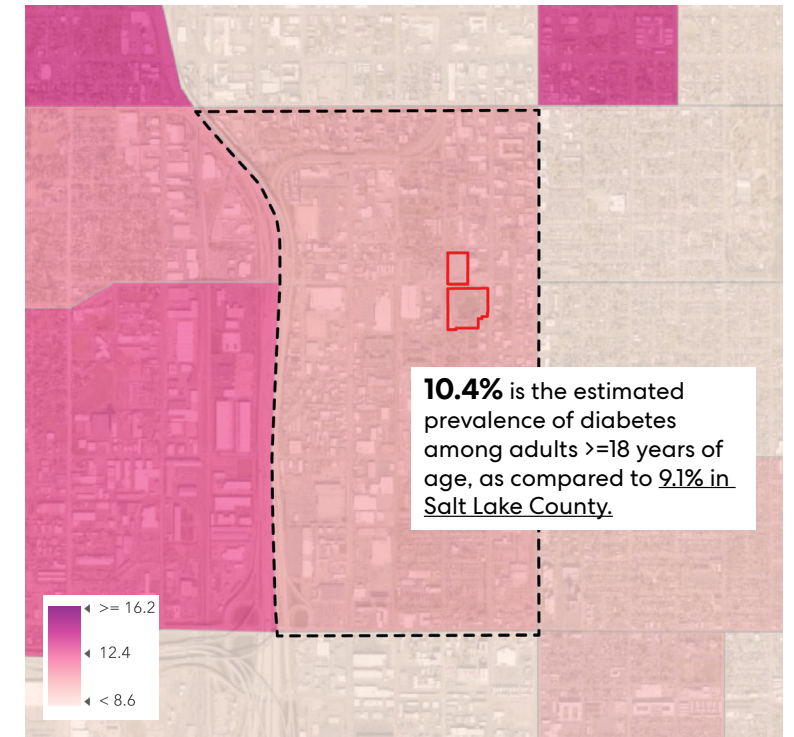
Obesity

Obesity is measured as the percentage of adults aged 18 and older with a body mass index (BMI) of 30 or higher.



Diabetes

Diabetes is defined as the percentage of adults aged 18 and older who have ever been diagnosed with diabetes by a healthcare professional.

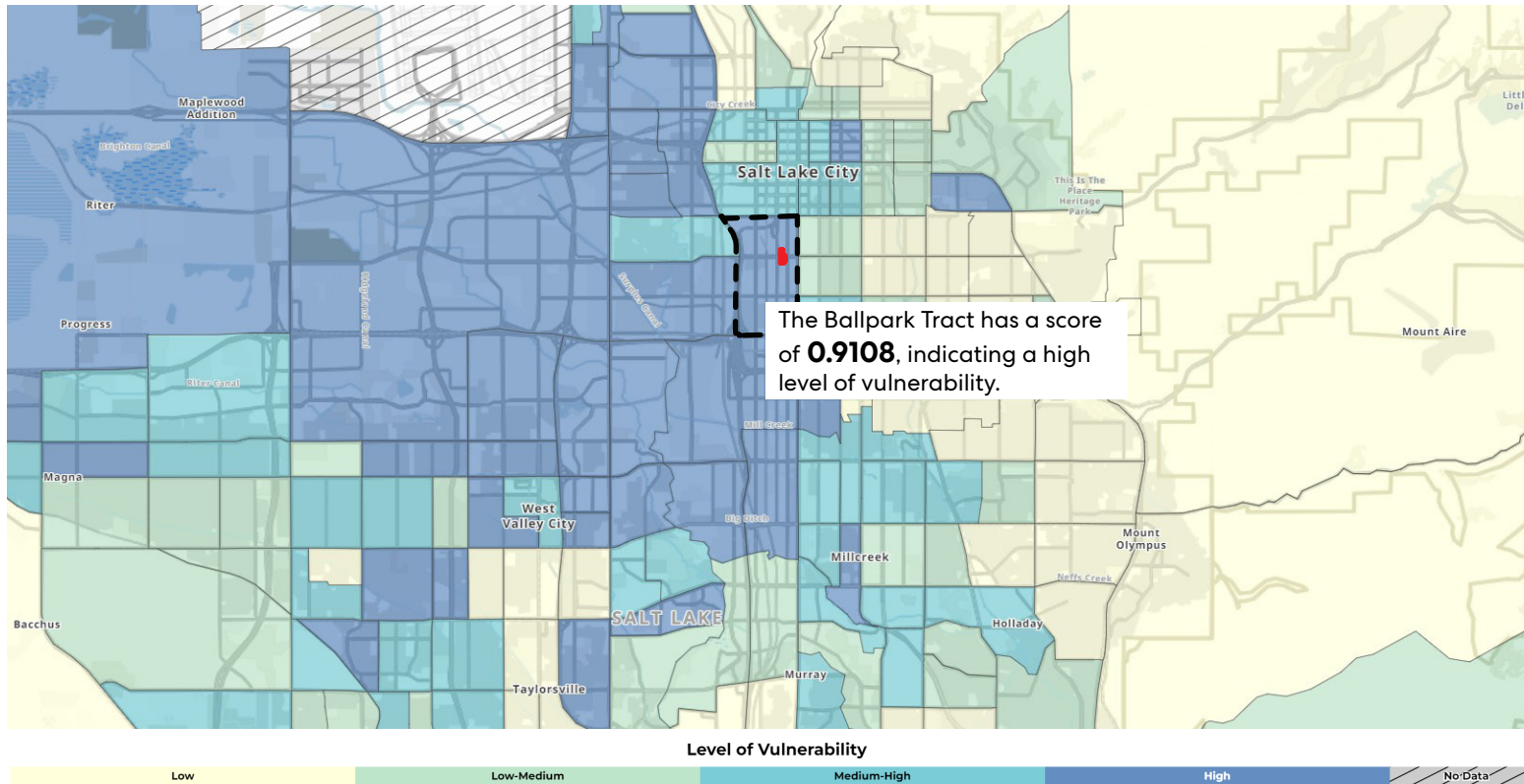


Source: Division of Population Health, National Center for Chronic Disease Prevention and Health Promotion, American Community Survey 2021 (5-Year Estimate)

01 — Health Equity Assessment

Social Vulnerability Index (SVI)

[CDC Social Vulnerability Index](#)



The Centers for Disease Control and Prevention and Agency for Toxic Substances and Disease Registry Social Vulnerability Index is a place-based index, database, and mapping application designed to identify and quantify communities experiencing social vulnerability.

The current CDC/ATSDR Social Vulnerability Index uses 16 U.S. census variables from the 5-year American Community Survey (ACS) to identify communities that may need support before, during, or after disasters. These variables are grouped into four themes that cover four major areas of social vulnerability:

Overall Vulnerability	Socioeconomic Status	<ul style="list-style-type: none"> Below 150% Poverty Unemployed Housing Cost Burden No High School Diploma No Health Insurance
	Household Characteristics	<ul style="list-style-type: none"> Aged 65 & Older Aged 17 & Younger Civilian with a Disability Single-Parent Households English Language Proficiency
	Racial & Ethnic Minority Status	<ul style="list-style-type: none"> Hispanic or Latino (of any race) Black or African American, Not Hispanic or Latino Asian, Not Hispanic or Latino American Indian or Alaska Native, Not Hispanic or Latino Native Hawaiian or Pacific Islander, Not Hispanic or Latino Two or More Races, Not Hispanic or Latino Other Races, Not Hispanic or Latino
	Housing Type & Transportation	<ul style="list-style-type: none"> Multi-Unit Structures Mobile Homes Crowding No Vehicle Group Quarters

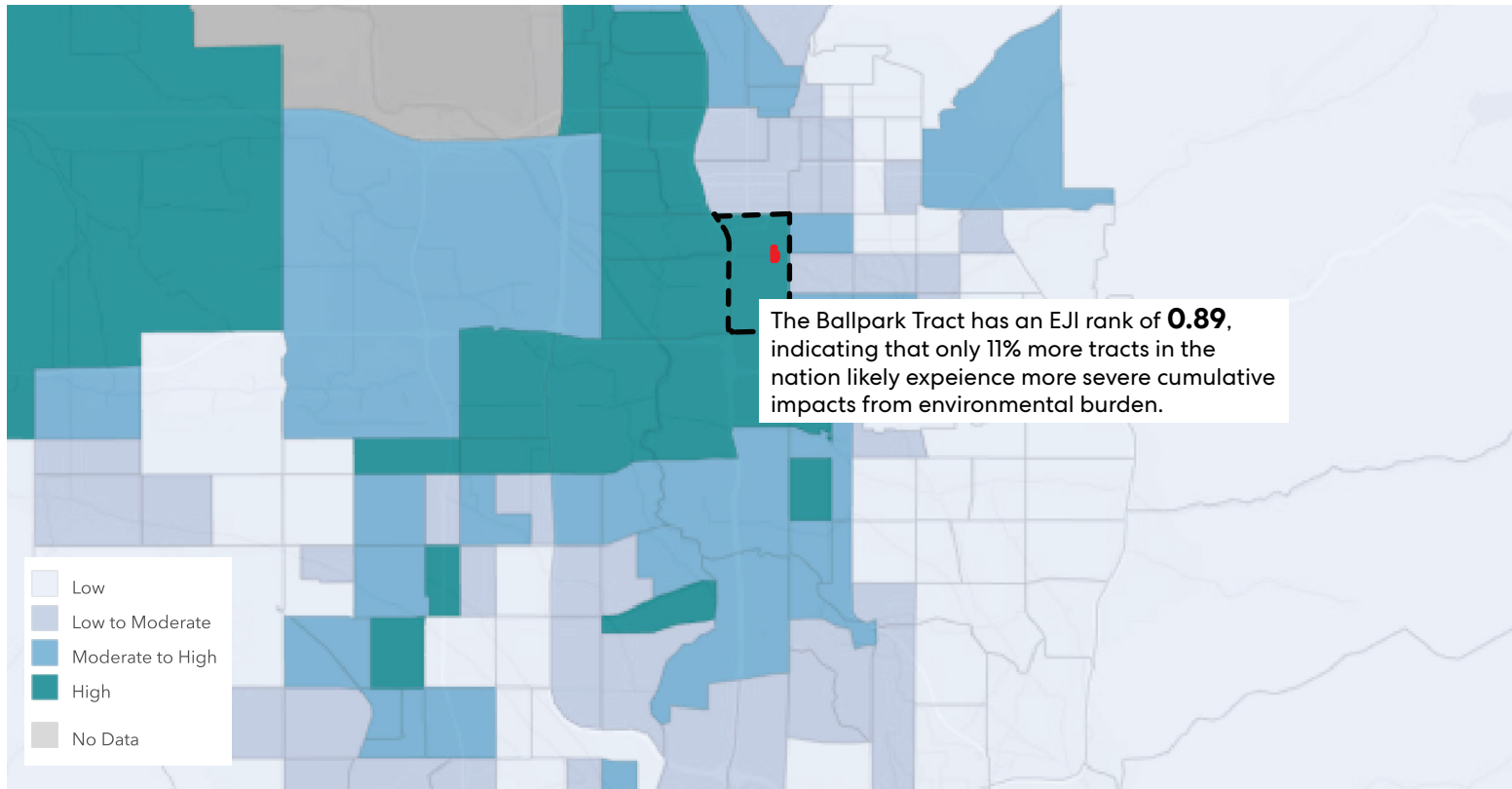
Source: Agency for Toxic Substances and Disease Registry

Ballpark NEXT Health Equity Site Assessment

01 — Health Equity Assessment

Environmental Justice Index (EJI)

[CDC Environmental Justice Index](#)



Environmental Justice is the fair treatment and meaningful involvement of all people, regardless of race, color, national origin, or income, to develop, implement, and enforce environmental laws, regulations, and policies.

Overall Environmental Justice Rank	Social Vulnerability	Racial/ Ethnic Minority Status	Minority Status
		Socioeconomic Status	Poverty
			No High School Diploma
			Unemployment
			Housing Tenure
		Household Characteristics	Housing Burdened Lower-Income Households
			Lack of Health Insurance
			Lack of Broadband Access
			Age 65 and Older
	Age 17 and Younger		
	Housing Type	Civilian with a Disability	
		Speaks English "Less than Well"	
		Group Quarters	
Mobile Homes			
Environmental Burden	Air Pollution	Ozone	
		PM2.5	
		Diesel Particulate Matter	
	Potentially Hazardous & Toxic Sites	Air-Toxics Cancer Risk	
		National Priority List Sites	
		Toxic Release Inventory Sites	
		Treatment, Storage, and Disposal Sites	
	Built Environment	Risk Management Plan Sites	
		Coal Mines	
		Lead Mines	
Transportation Infrastructure	Recreational Parks		
	Houses Built Pre-1980		
	Walkability		
Water Pollution	High-Volume Roads		
	Railways		
Health Vulnerability	Pre-existing Chronic Disease Burden	Airports	
		Impaired Surface Water	
		Asthma*	
		Cancer*	
		High Blood Pressure*	
Diabetes*			
Poor Mental Health*			

*Health vulnerability measures are marked with asterisks as they are calculated differently than other indicators. While most indicators can have a range of values, the health vulnerability indicators only represent whether or not a given census tract experiences a high estimated prevalence of disease.

Source: U.S. Census Bureau, the U.S. EPA, the U.S. Mine Safety and Health Administration, and the U.S. Centers for Disease Control and Prevention

Key Health Equity Takeaways

1. Life Expectancy Gap

Residents in the Ballpark area have a life expectancy that is, on average, 5 years shorter than those living in more affluent neighborhoods of Salt Lake City. In general, west-side residents have a life expectancy nearly ten years less than their east-side counterparts. This disparity is closely tied to social determinants like income, education, and access to healthcare.

2. High Rates of Chronic Disease

The prevalence of **chronic diseases such as diabetes and asthma is significantly higher in the Ballpark** census tract, with rates nearly double those of the city average. These conditions are often linked to environmental factors and limited access to preventive care.

3. Limited Access to Healthy Foods

Over 30% of households in the Ballpark tract are in food deserts, suggesting these households have limited access to fresh, healthy foods. This contributes to poor nutrition and higher rates of diet-related illnesses.

4. Housing Instability

Nearly **30% of residents in the Ballpark area spend more than half of their income on housing**, leaving little room for other essential expenses like healthcare and transportation. This high cost-burden is a significant factor in health inequity.

5. Economic Instability

The **Ballpark area has a high percentage of people living below 150% of the poverty level**, with rates more than double of the Salt Lake County average. This impacts access to quality education, healthy foods, and basic services such as broadband internet subscriptions.

6. Environmental Exposure Risk

The **Ballpark area has higher exposure to environmental hazards, including poor air quality and proximity to industrial waste sites**. These factors contribute to respiratory issues (like asthma) and other health problems, disproportionately affecting low-income and minority populations.

02 — Design Recommendations and Priority Matrix

Design Strategies and Program Elements

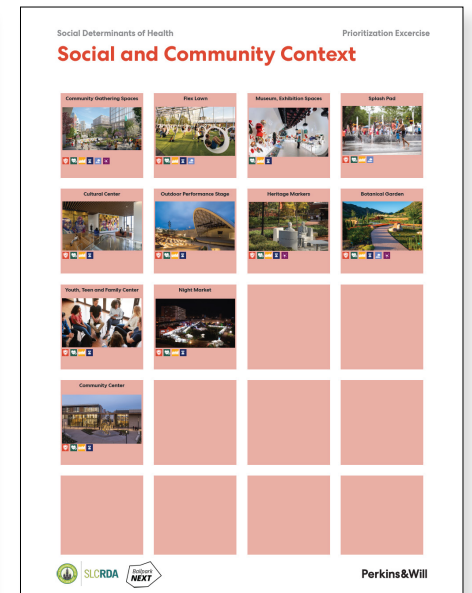
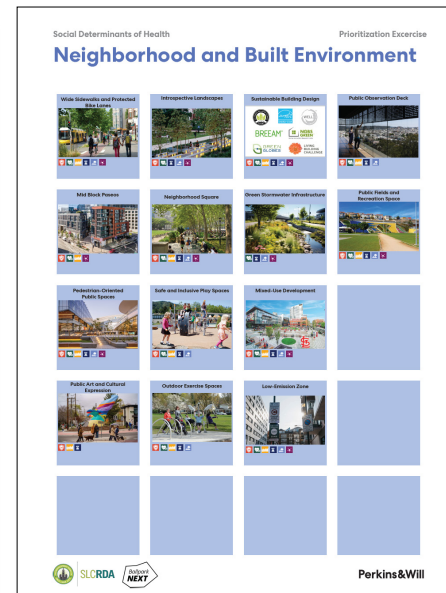
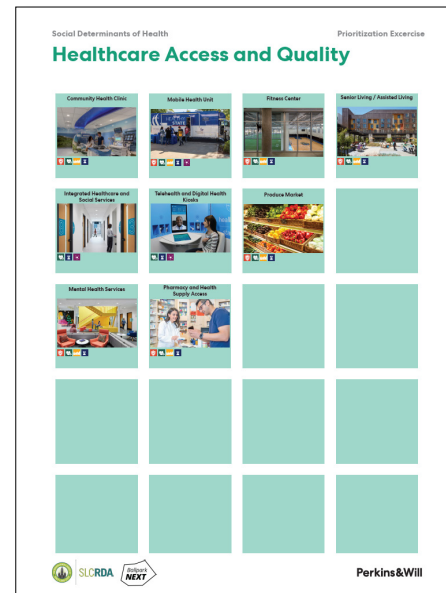
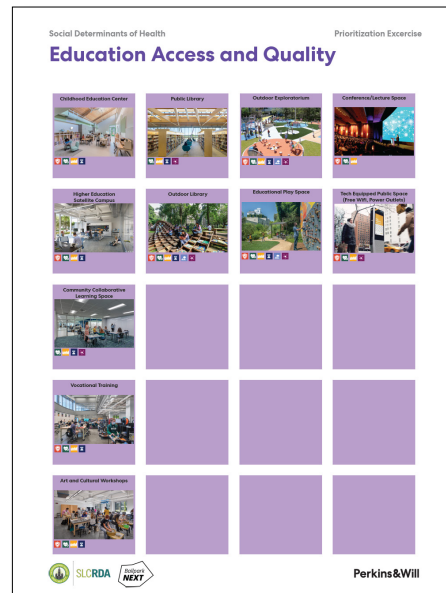
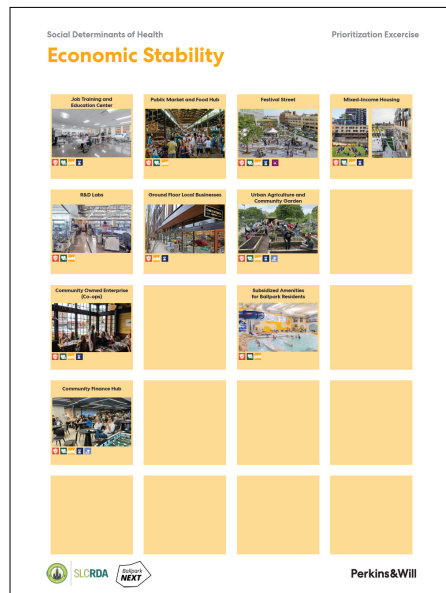
Economic Stability

Education Access and Quality

Healthcare Access and Quality

Neighborhood and Built Environment

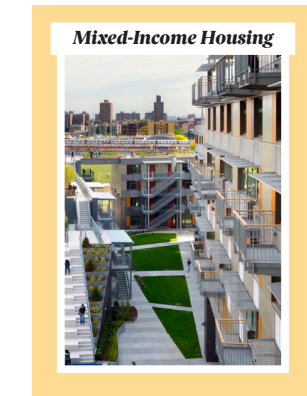
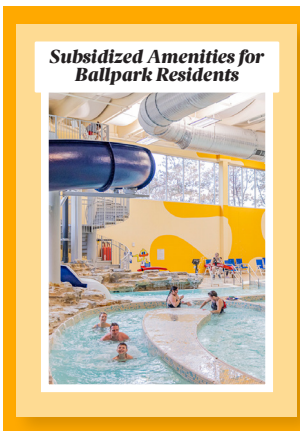
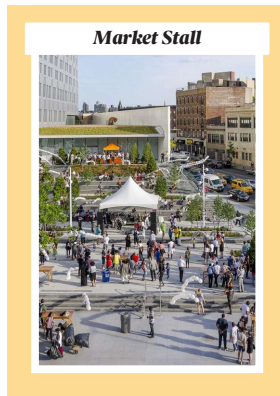
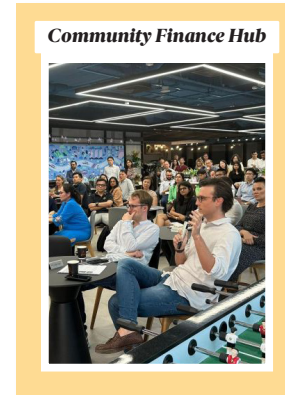
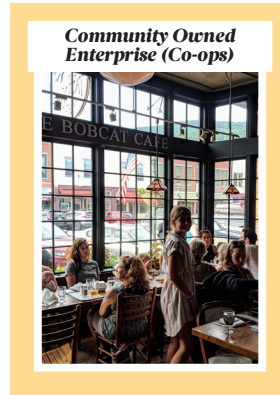
Social and Community Context



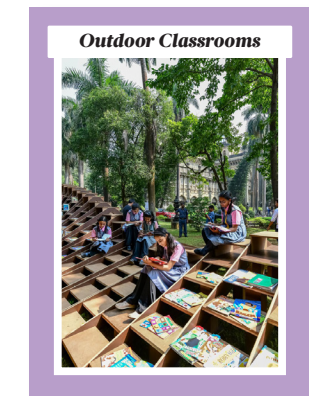
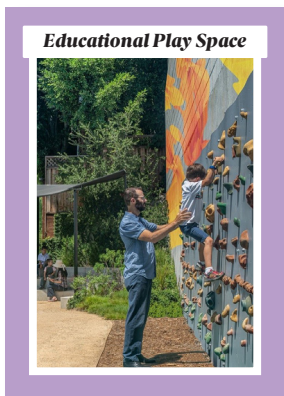
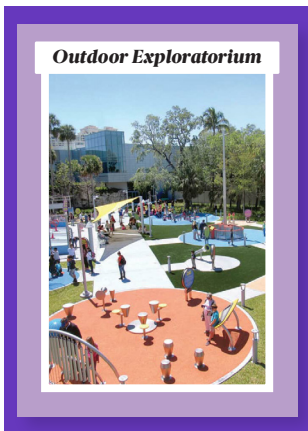
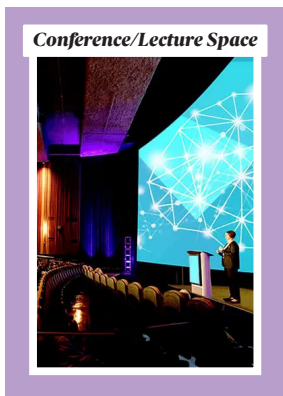
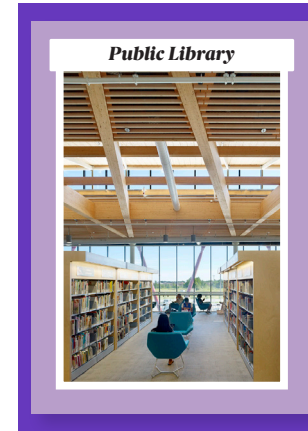
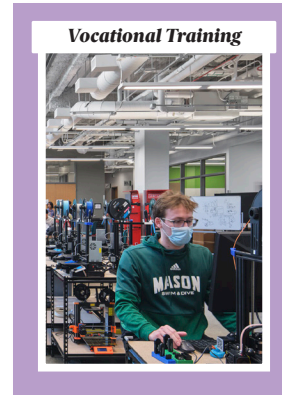
02 — Design Recommendations and Priority Matrix

Economic Stability

Design Strategies and Programs



Design Strategies and Programs



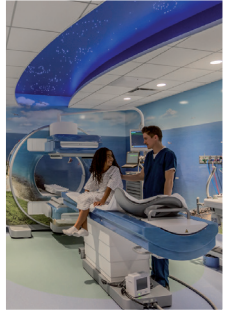
02 — Design Recommendations and Priority Matrix

Healthcare Access and Quality

Design Strategies and Programs

 Top Priorities

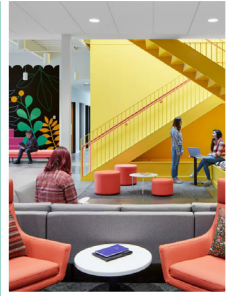
Community Health Clinic



Integrated Healthcare and Social Services



Mental Healthcare and Social Services



Mobile Health Unit



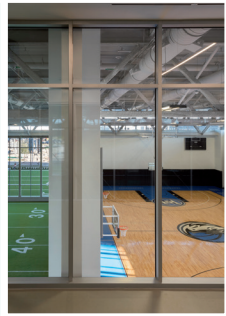
Telehealth and Digital Health Kiosks



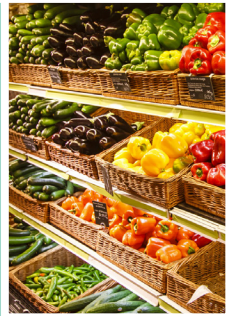
Pharmacy and Health Supply Access



Fitness Center



Produce Market



Senior Living/Assisted Living



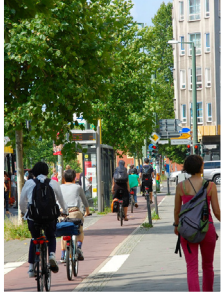
02 — Design Recommendations and Priority Matrix

Design Strategies and Programs

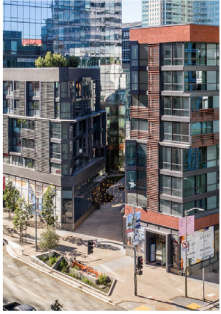
 Top
Priorities

Neighborhood and Built Environment

Wide Sidewalks and Protected Bike Lanes



Mid Block Paseos



Pedestrian-Oriented Public Spaces



Public Art and Cultural Expression



Introspective Landscapes



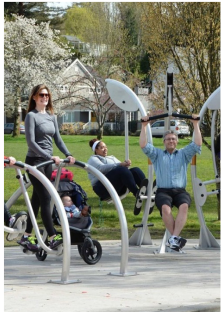
Neighborhood Square



Safe and Inclusive Play Spaces



Outdoor Exercise Spaces



Sustainable Building Design



Green Stormwater Infrastructure



Mixed-Use Development



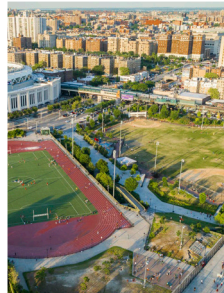
Low-Emission Zone



Public Observation Deck



Sports Fields and Recreation Space

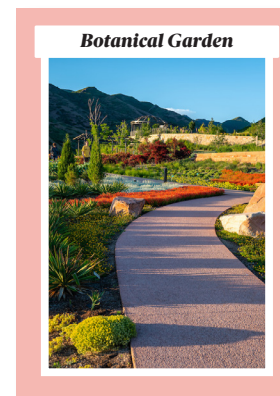
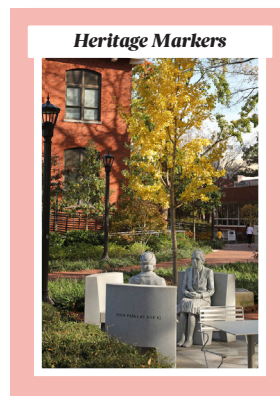
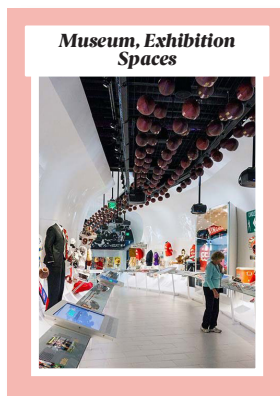
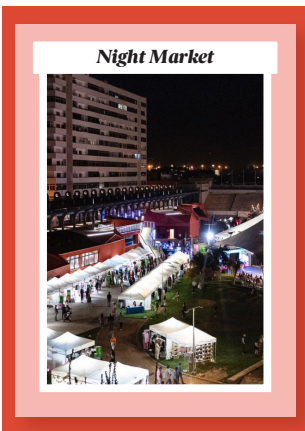
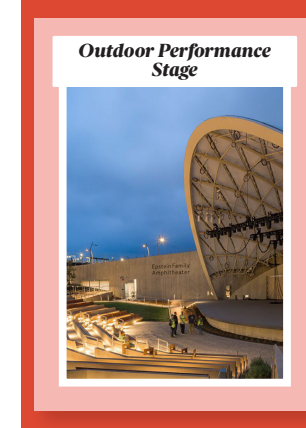
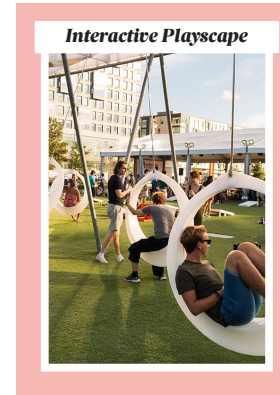
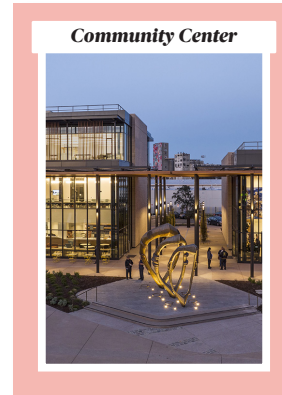
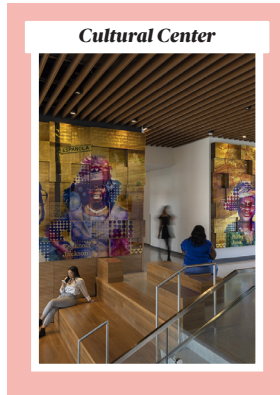
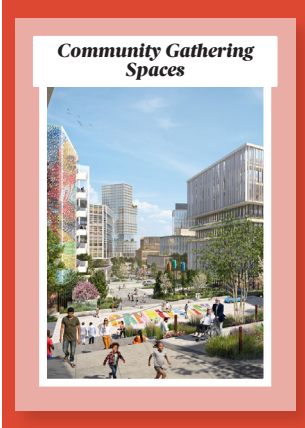


02 — Design Recommendations and Priority Matrix

Social and Community Context

Design Strategies and Programs

 Top Priorities



02 — Design Recommendations and Priority Matrix

Stakeholder Prioritization Exercise Results

During Stakeholder Ideation Session 1, approximately 30 participants took part in an exercise where each was given five stickers to prioritize one program on each board. This activity identified the following programs as top priorities for the Ballpark NEXT project.

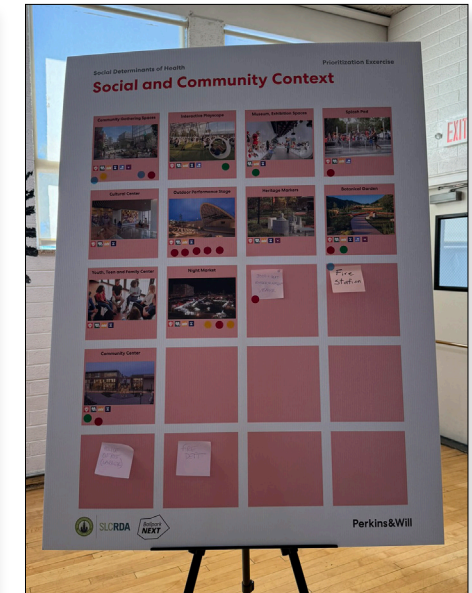
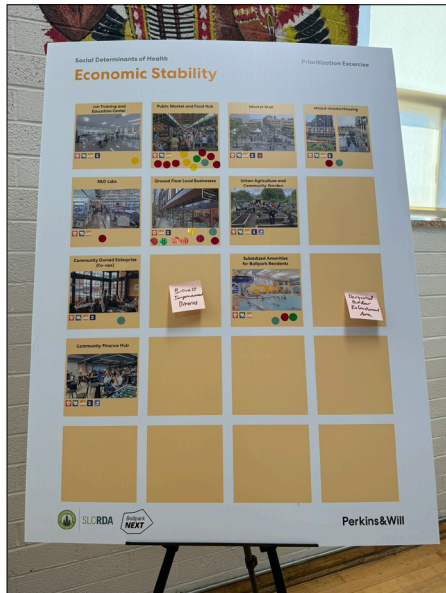
- Public Market/ Food Hall
- Ground Floor Local Businesses
- Subsidized Amenities for Ballpark Residents

- Public Library
- Arts and Cultural Workshops
- Outdoor Exploratorium

- Produce Market
- Fitness Center

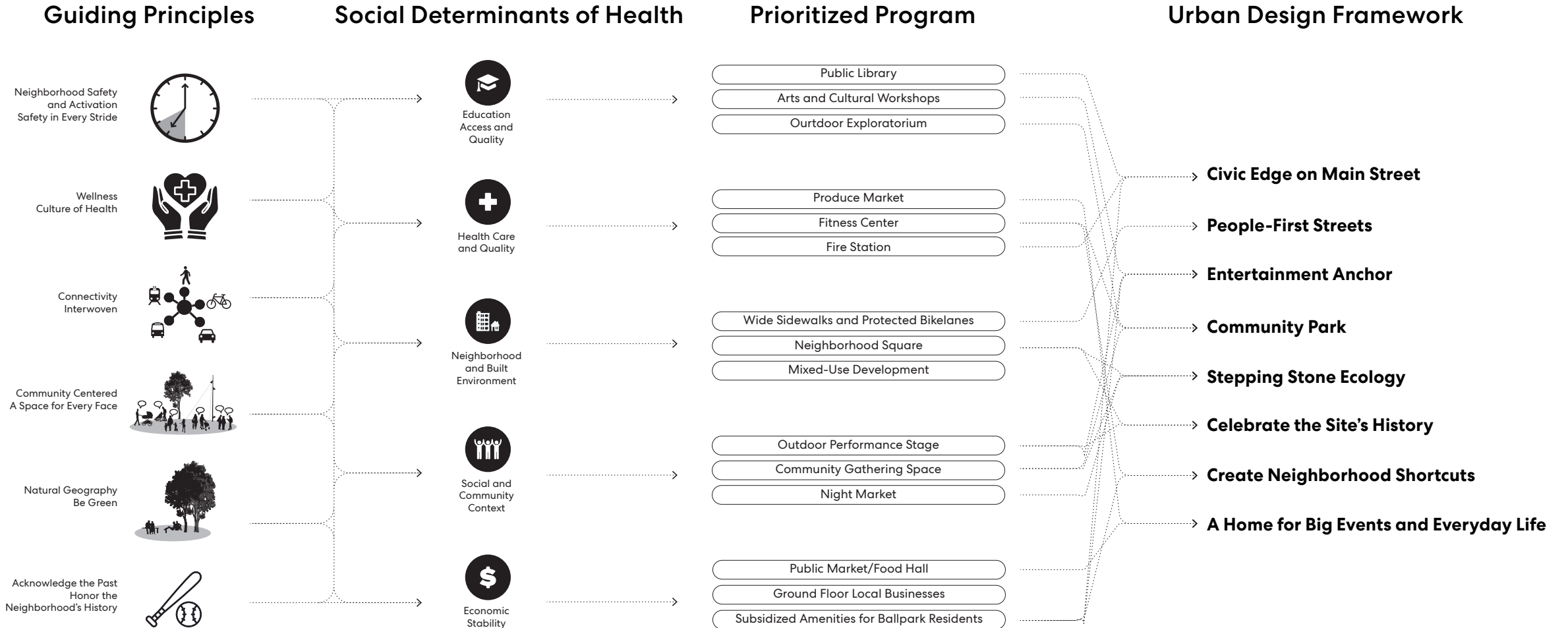
- Wide Sidewalks and Protected Bike Lanes
- Neighborhood Square
- Mixed-Use Development

- Outdoor Performance Stage
- Community Gathering Space
- Night Market



02 — Design Recommendations and Priority Matrix

Urban Design Framework Development



Appendix C:

Transportation and Civil Existing Conditions Narrative

RDA Ballpark NEXT

Transportation and Civil Existing Conditions Narrative

September 2024

A. SITE CONTEXT

i. Nearby Land Use and Amenities

The area adjacent to the site is primarily commercial with a mix of some residential and industrial. Both single-family and high-density residential units can be found to the south and southeast of the ballpark. The Ball Park Neighborhood offers few shopping and dining accommodations. Most of the surrounding area is commercial business, most of which are lacking storefronts. Salt Lake City's downtown hub is located than two miles north of the site.

The two closest green spaces/parks reside along Jefferson Street. The park to the north, Jeffersons Rectangular Park, consists of an open green space originally designed as an emergency detention pond for the city. South of the ballpark, but still along Jefferson Street, is a park called Jefferson Park. Jefferson Park has a swing set and playground as well as a large open green space.

ii. Vehicular Transportation

Figure 1 shows the greater study area. The site's main frontage is located along 1300 South, a five-lane roadway to the north.

1300 South adjacent to the site is owned and maintained by Salt Lake City.

Main Street, a 3-lane roadway with buffered bike lanes and street parking, borders the site to the east.

West Temple runs west of the site and is a 2-lane roadway with regulated street parking.

The site also includes a surface level parking lot north of 1300 South along West Temple. Half a mile to the west is Interstate 15 with on and off ramps servicing 1300 South.

State Street to the east is a major roadway carrying traffic north and south. It's a six-lane road with two turn lanes at the 1300 South intersection.

iii. Exiting and Historical Traffic Volumes

Existing 2023 data for the study area extracted from WFRC's *Utah Stateside Annual Average Daily Traffic (AADT) – Historic & Forecast* web map¹, and is shown in **Figure 2**.

UDOT's *Map of Annual Average Daily Traffic (AADT)* provides historical data for state routes. The average AADT of the following roadways through the study area are summarized in this study:

- 900 South
- 1300 South
- 1700 South
- 300 West
- West Temple
- Main Street
- State Street

Historical traffic data for the years 2021 and 2023 are summarized in **Table 1**.

Between 2021 and 2023, traffic throughout the study area grew approximately 1.2% a year.

¹ <https://unifiedplan.org/traffic-volume-map/>

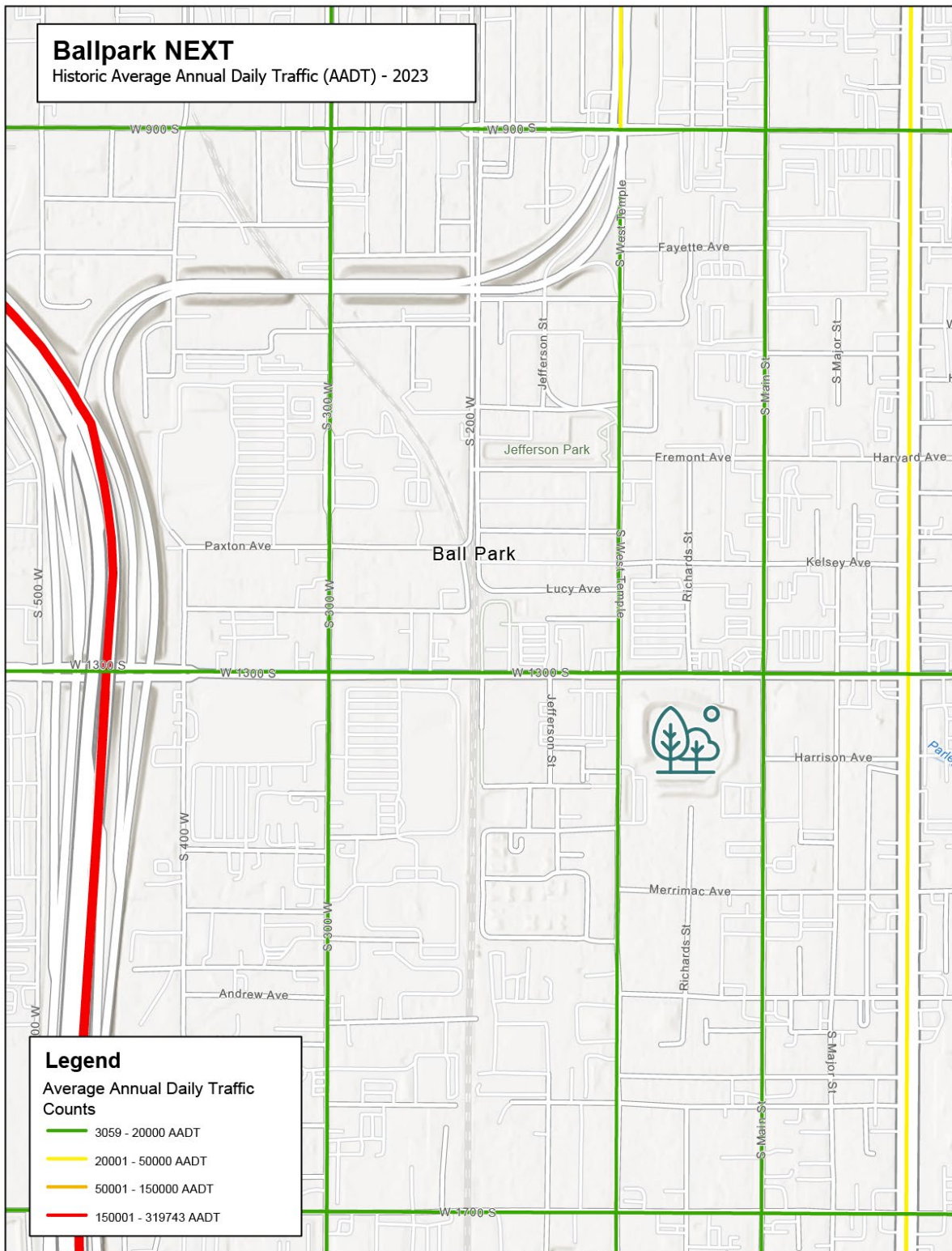


Figure 2: Historic (2023) Average Annual Daily Traffic

Table 1: Historic Traffic Volumes (2021-2023)

Segment	Historical AADT			Annual Growth Rate (2021 – 2023)
	2021	2022	2023	
900 South	11,554	11,669	11,832	1.2%
1300 South	17,772	17,950	18,201	1.2%
1700 South	11,489	11,604	11,766	1.2%
300 West	16,957	17,126	17,366	1.2%
West Temple	11,126	11,237	11,394	1.2%
Main Street	10,715	10,823	10,974	1.2%
State Street	28,232	29,813	30,409	3.8%

iv. Future Traffic Volumes

Future forecasted traffic data were taken from the WFRC *Utah Stateside Annual Average Daily Traffic (AADT) – Historic & Forecast* web map. The forecasted 2050 AADT values for major roadways in the study area are shown in **Figure 2**.

The forecasted traffic count data is presented in **Table 2**. Between 2023 and 2050 there is an expected average 1.3% annual growth rate.

Table 2: Forecasted Traffic Volumes (2050)

Segment	AADT		Annual Growth Rate (2023 – 2050)
	2023	2050	
900 South	11,832	16,800	1.3%
1300 South	18,201	19,100	0.2%
1700 South	11,766	16,900	1.4%
300 West	17,366	25,250	1.4%
West Temple	11,394	23,911	2.8%
Main Street	10,974	14,000	0.9%
State Street	30,409	39,000	0.9%

v. Safety

Crash data for the years 2019 through 2023 were obtained from the Utah Department of Transportation (UDOT). The data reported a total of 1,732 crashes within the study area. Removing crashes recorded on Interstate 15 as well as on and off ramps, 793 crashes were reported.

Figure 3 shows crashes reported by year and crash severity. The years with the highest number of crashes within the 5-year study period were 2019 and 2021 with 190 reported crashes. However, 2022 saw the highest number of fatal crashes, with 3 reported fatalities.

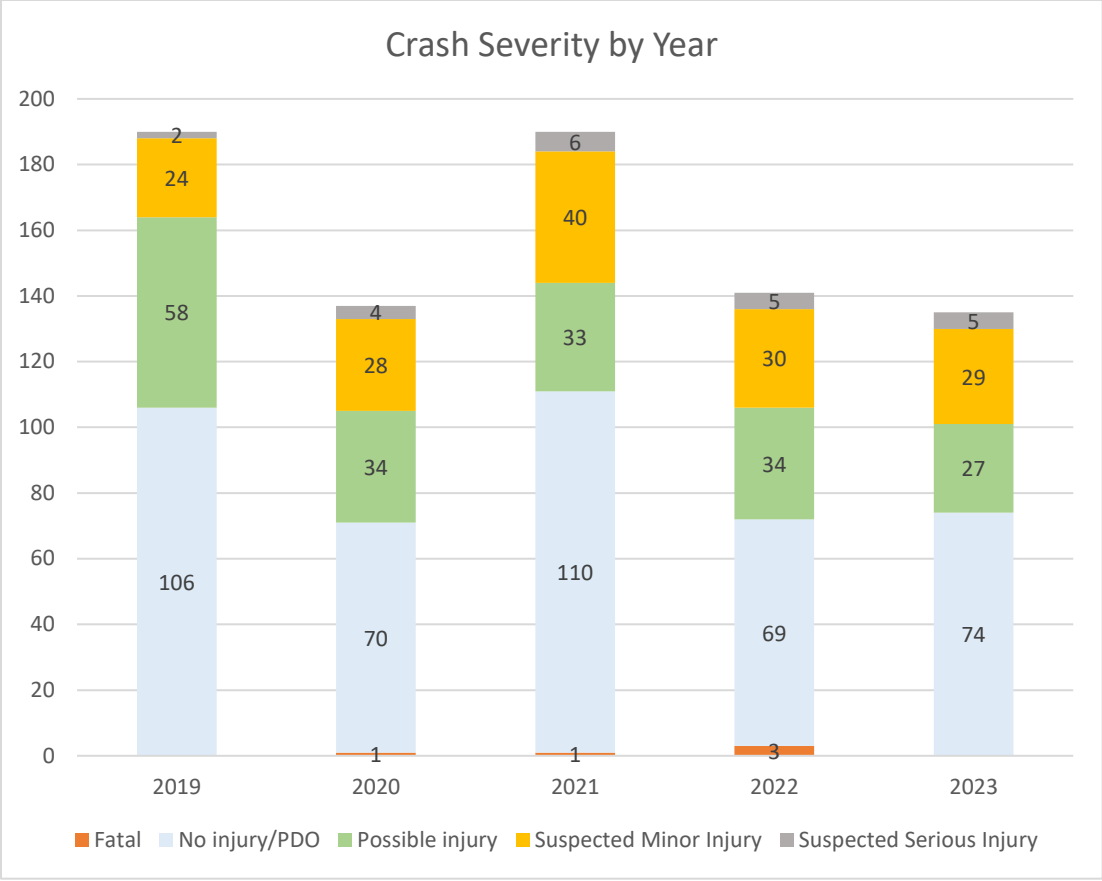


Figure 4: Crash Severity (2019-2023)

Figure 4 shows the distribution of all crashes to identify crash “hot-spots” while also highlighting fatal or suspected serious crashes involved a pedestrian or cyclist.

Figure 5 summarizes pedestrian and bicycle involved crashes. A total of 98 crashes were reported involving a pedestrian or cyclist. 2021 saw the most pedestrian involved crashes with 17 reported crashes. In 2023 bicycle-involved crashes were the highest with 12 reported crashes.

There were 3 reported fatal pedestrian-involved crashes in the study period. All occurred in 2022; two occurred on State Street and one at the intersection of 1300 South and Major Street, just half a block away from the Smiths Ballpark. All three crashes occurred in the evening without daylight. One incident occurred while the pedestrian was in an unmarked crosswalk (crossing Major Street on the north side of 1300 South), the second one occurred while the pedestrian was in a marked crosswalk crossing State Street at Harvard Avenue, and the third fatal crash occurred while the pedestrian was in the travel lane (not in a crosswalk or intersection).

A total of 10 crashes occurred where the pedestrian is suspected to have sustained serious injuries. Three of these crashes occurred on 1300 South and two at the intersection of 1300 South and State Street.

Between 2019 and 2023, a total of 31 bicycle-involved crashes were reported in the study area. The most crashes (12) were reported in 2023, one crash resulted in a possible injury and 11 crashes resulted in suspected minor injury. One fatal bicycle-involved crash occurred in 2020 on State Street just north of Kensington Avenue.

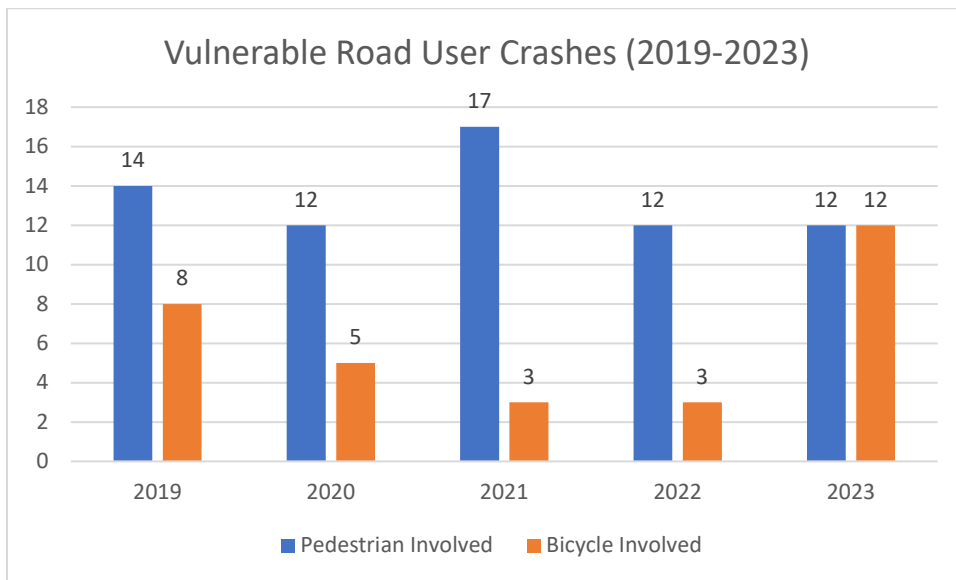


Figure 6: Vulnerable Road User Crashes (2019-2023)

vi. Non-motorized Transportation

Generally, most streets surrounding the site have sidewalks. There are some industrial areas in the vicinity that currently lack sidewalks; these streets usually lead to light industrial or manufacturing \uses. Accessing the ballpark via bicycle is possible on Main Street, as the street offers a buffered bike lane in both directions. Other bicycle facilities in the study area include a 2-way cycle tack on 900 South and 300 West. 1700 South offers a painted bike lane until the TRAX lines. From there, going east, the painted bike lane becomes a buffered bike lane, offering slightly more protection and separation between cyclists and vehicular traffic.

All major intersections along 1300 East are signalized intersection. A midblock crossing at Richard Street offers an additional crossing opportunity with a pedestrian activated beacon (formerly known as “HAWK Light”).

Figure 7 illustrates transit and active transportation infrastructure in the study area.

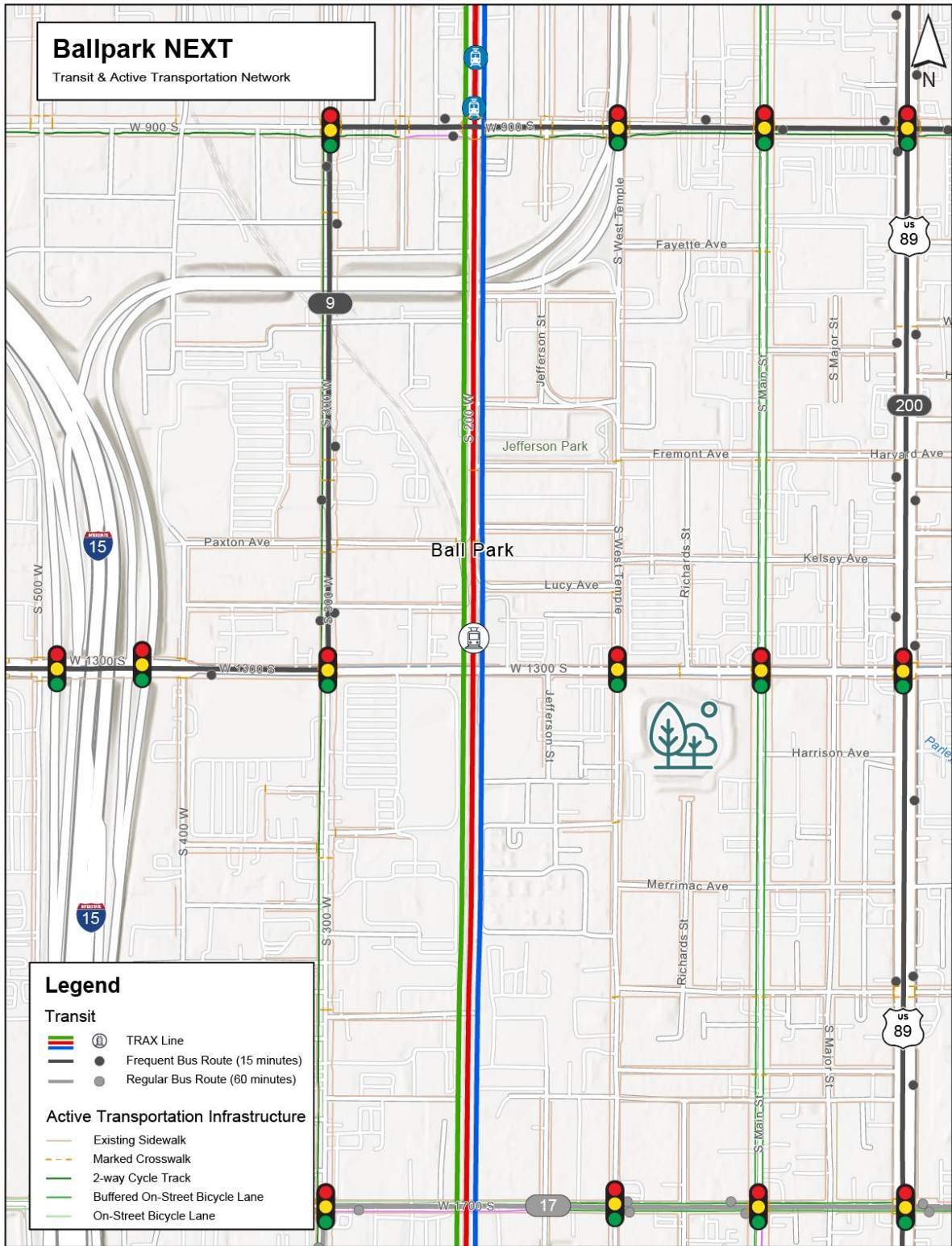


Figure 7: Transit and Active Transportation Infrastructure

vii. Transit

Utah Transit Authority (UTA) operates three bus routes and three TRAX lines in the study area. Study Area routes are summarized in **Table 3** and **Figure 7**.

Table 3: Transit Routes

Route Number	Name	Start Station	End Station	Frequency	Operation Hours (weekday)
701	TRAX Blue Line	Salt Lake Central Station	Draper Town Center	15 minutes	4:21 am – 12:05 am
703	TRAX Red Line	U. of U. Medical Center Station	Daybreak Parkway Station	15 minutes	4:56 am – 11:56 am
704	TRAX Green Line	West Valley Central Station	Airport Station	15 minutes	4:48 am – 11:21 pm
9	900 South	U. of U. Union Building	Orange Street	30 minutes	4:05 am – 12:47 am
17	1700 South	U. of U. Union Building	Central Pointe Station	30 minutes	6:34 am -8:22 pm
200	State Street North	North Temple Station	Murray Central Station	30 minutes	4:08 am – 12:30 am

There are 30 transit stations within the study area as illustrated in **Figure 7**.

Figure 8 and **Figure 9** show boardings and alightings for bus stops, and TRAX stations in the study area respectively. As applicable, data for bus stop pairs was combined, such as stations with a northbound and southbound or eastbound and westbound direction.

Data is based on UTA’s Stops and Most Recent Ridership database² which shows average weekday ridership and is updated monthly. For this report, the most recent average ridership data is from August 2024, which was published on September 26th.

²<https://maps.rideuta.com/portal/apps/sites/#/uta-open-data/datasets/d11224f80a424c8497189edee00f746b/about>

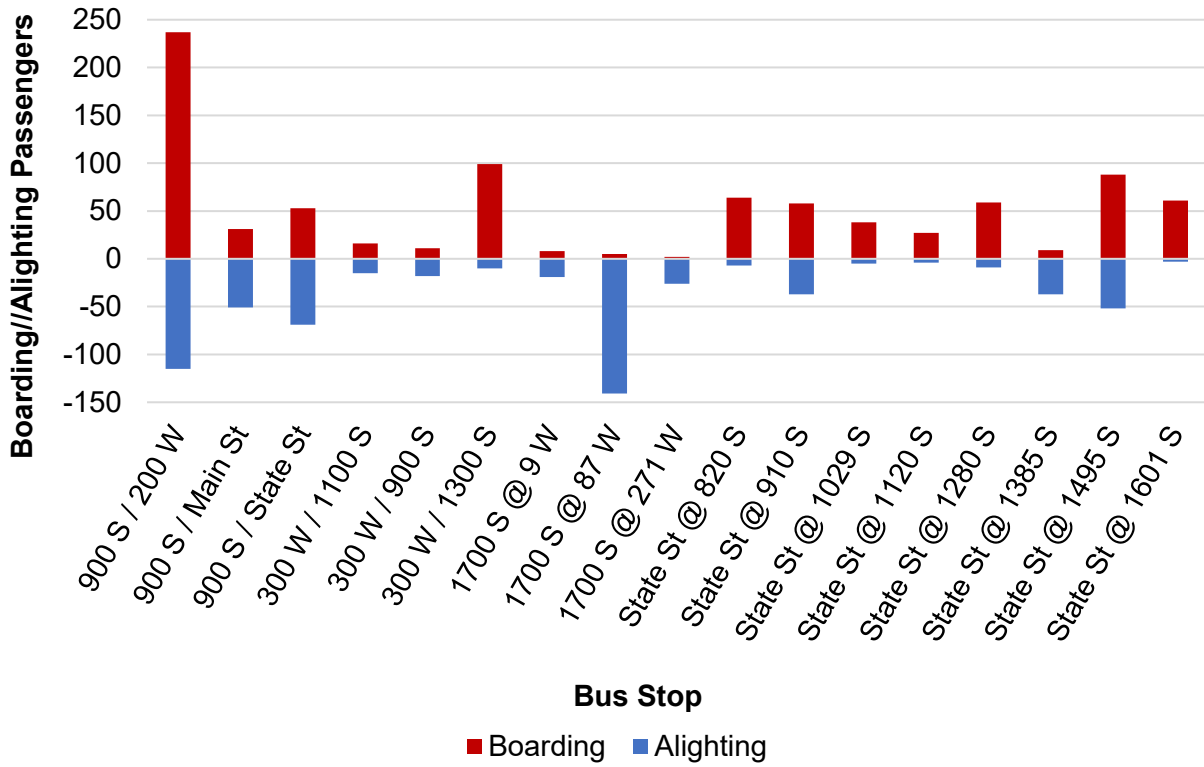


Figure 8: Bus Stop Boardings and Alightings

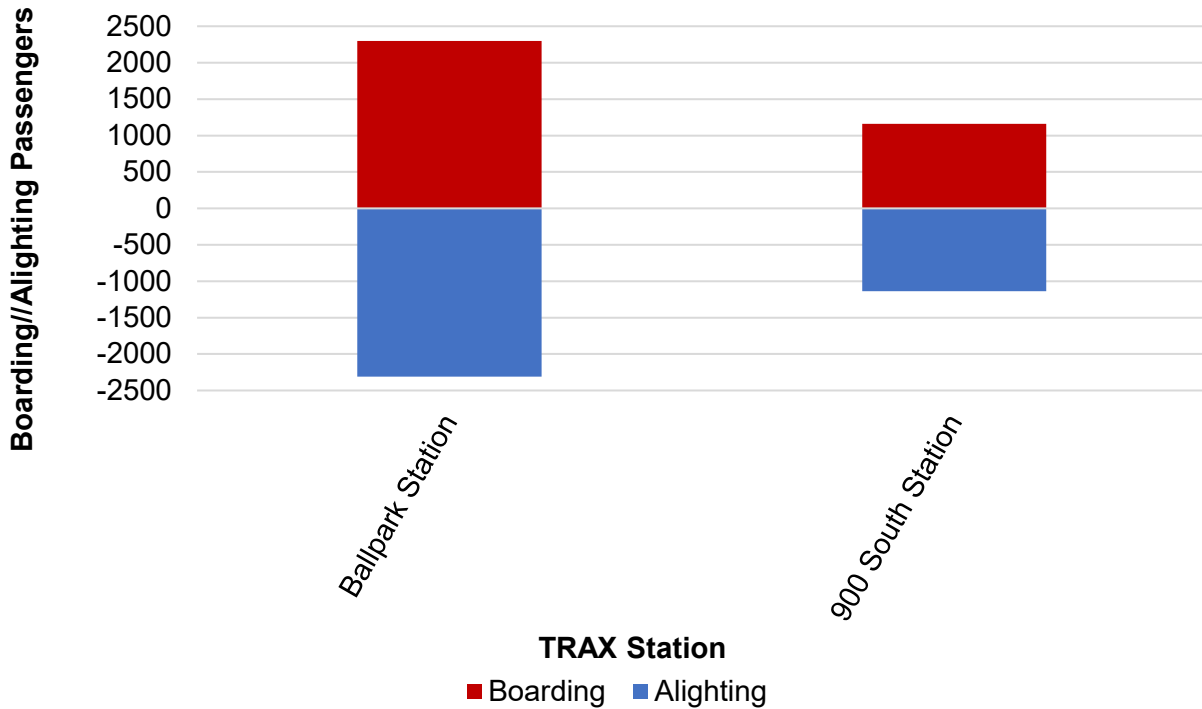


Figure 9: TRAX Stop Boardings and Alightings

viii. Previous Plans & Studies

a. Ballpark Station Area Plan (SAP) - 2022

The Ballpark SAP recommends or identifies the following:

- Study the potential future lane reconfiguration of 1300 South to eliminate or narrow traffic lanes and expand and improve the sidewalk.
- Utilize existing alleyways, midblock, and truncated connections to create a system of bike and pedestrian pathways through the neighborhood.
- Reconfigure Ballpark TRAX Station to allow access from both the east and west sides of the station as well as the north end of the platform from Lucy Avenue/200 West on the west side of the TRAX rails.
- Consider re-developing TRAX Station parking and bus turnaround for higher density uses and to provide neighborhood amenities.
- Install pedestrian crossings east and west of TRAX on 1300 South on either side of the UTA crossing barrier.
- In-line bus service at Ballpark Station with riders accessing the platform directly from sidewalk for Westbound busses on 1300 South or by crossing 1300 S for eastbound busses.
- Redevelop part of the current SLC and UTA owned properties – most of which is surface parking on north side of 1300 S between Main and TRAX line to transit supportive uses that include retail, shops, and service near the Ballpark Station platform.

- The unused rail spur between 200 West and 400 West between Paxton Avenue and 900 South is planned for future TRAX extension and adjacent trail into the Granary District.
- Improve pedestrian and bicyclist comfort by providing adequate sidewalk width, ADA accessibility, park strips on primary routes, pedestrian level lighting, ongoing maintenance of facilities, street furniture, improved bike lane markings and crossings, bike parking, and reducing sidewalk network gaps.
- Potential future transit hub at 1700 S serving light rail and east-west bus service
- Existing bikeway on Main Street with striped bike lanes.
- A buffered bike lane is planned for 900 South from I-15 to 300 West.
- A marked shared roadway is planned to connect Paxton Ave to 1300 S underpass
- Transforming West Temple and plazas adjacent to the stadium from 1300 South to Albermarle Avenue into a Festival Street.
- Improve east-west connectivity and crossings at TRAX lines to the north and the south of 1300 South to allow pedestrians and cyclists to move east-west without having to go to 1300 or 1700 South.

b. Salt Lake City Pedestrian & Bicycle Master Plan – 2015

The plan identifies existing bikeways on Main Street, 300 East and 1300 South, east of State Street and areas of future improvements. Recommendations include adding an eastbound bike lane on 1300 South from 500 West to 300 West, and a protected bike lane on 300 West.

On Paxton Avenue, shared lane markings were recommended from 400 West to West Temple before turning into a neighborhood byway from West Temple to 200 East via Kelsey Avenue and Edith Avenue. To make this a safe route, a HAWK signal at State Street and 120 feet of protected bike lane on the east side of State Street is recommended. A HAWK signal or protected intersection jog could also be considered for crossing 300 West on Paxton Avenue.

From 200 West near Ballpark Station to the intersection of 400 West and 900 South, there is an unused rail spur. The plan recommends a trail along this right-of-way to connect the Ballpark neighborhood to the Granary District.

c. Salt Lake City Transit Master Plan (SLC TMP) – 2017

A major emphasis of the SLC TMP is developing a Frequent Transit Network (FTN). Tier 1 FTN corridors include 200 West, State Street, 500 East, 900 East, and 1300 East. The latter two are important for building out the east-west grid. With the future addition of a grade separated railroad crossing as a FTN tier 2 upgrade at 900 S, both corridors would have frequent service between Redwood Rd and 1500 E. Another tier 2 upgrade is 1700 S to provide frequent service between State Street and Foothill Drive which will increase east-west mobility. As part of a FTN, there needs to be access to transit within a quarter mile walk of any location. Overall, the Ballpark neighborhood has adequate access except for the area between I-15 and Main Street from 1400 South to 2100 South. First and last mile service strategies such as ride share, bike share, and a community shuttle pilot are recommended in this area.

Other recommended improvements to the area include increasing bike parking at transit stops, storage on transit, and committing to ongoing funding that identifies and constructs bicycle and pedestrian safety improvements.

B. SITE ANALYSIS

i. Site Characteristics

The combined 13.82-acre site generally slopes from east to west at below half of a percent. Web Soil Survey states that the soils in the surrounding area consist of Urban Land. The portion of the site north of 1300 South is covered in paved parking where there are very few landscape islands breaking up the asphalt pavement. The north lot does however host some larger landscaping trees along the north, south, and west property boundaries. The west side of the ballpark property also consists of larger landscaping trees along its property boundary.

i. Easements and Restrictions

There has not been an ALTA survey completed for the subject site. An ALTA survey should be completed for the site to verify all easements and/or encumbrances.

ii. Hydrology

According to the FEMA Firm Panel 49035C0282H effective 8/2/2012 the parking lot portion of the site located north of 1300 S is located in Zone X. FEMA states that Zone X is “Area of Minimal Flood Hazard”. The portion of the site to the south of 1300 S is not located in any FEMA flood hazards.

Dry Creek, Emigration Creek and Parleys Creek are all flowing underground in two different pipe networks beneath 1300 South. These waterways are contained in both a 72-inch by 120-inch reinforced concrete channel and a separate 60-inch reinforced concrete pipe. Coordination with Salt Lake City flood control will need to take place to determine if these structures can be tapped into for site drainage discharge if needed.

ii. Utilities

- a. Water** – As shown in Figure 10, existing water lines are located in the roadways along the north, east, and west property boundary. 1300 South contains two water mains, a 16-inch and a 36-inch. Future development will not be able to connect to these water mains due to their size. West Temple to the west of the site contains a 6-inch cast iron pipe. If water connections along West Temple are desired, the existing water line will need to be upsized to a minimum 12-inch water line. There is a similar situation along the east side of the property in Main Street. The existing 6-inch cast iron pipe water line in Main Street will need to be upsized to a 12-inch water line if connected to in the future. A fire flow test should be completed to verify flow rates and pressures in the existing system.

- b. Sewer** – As shown in Figure 10, Salt Lake City Public Utilities map, West Temple has an existing 36-inch sewer main line flowing from south to north in the center of the street. An existing 48-inch sewer main flows from east to west under the south side of 1300 South. An existing 8-inch sewer flows south to north under the east side of Main Street. There are existing

service laterals extending into the property along the north and west sewer mains. The exact size, location and condition of the laterals is unknown at this time.

The south-west corner of the ballpark property has an existing 12-inch PVC sanitary sewer cutting through the property. It is likely that the 12-inch sewer has an existing utility easement.

c. Natural Gas – Unknown at this time. Further investigation needed.

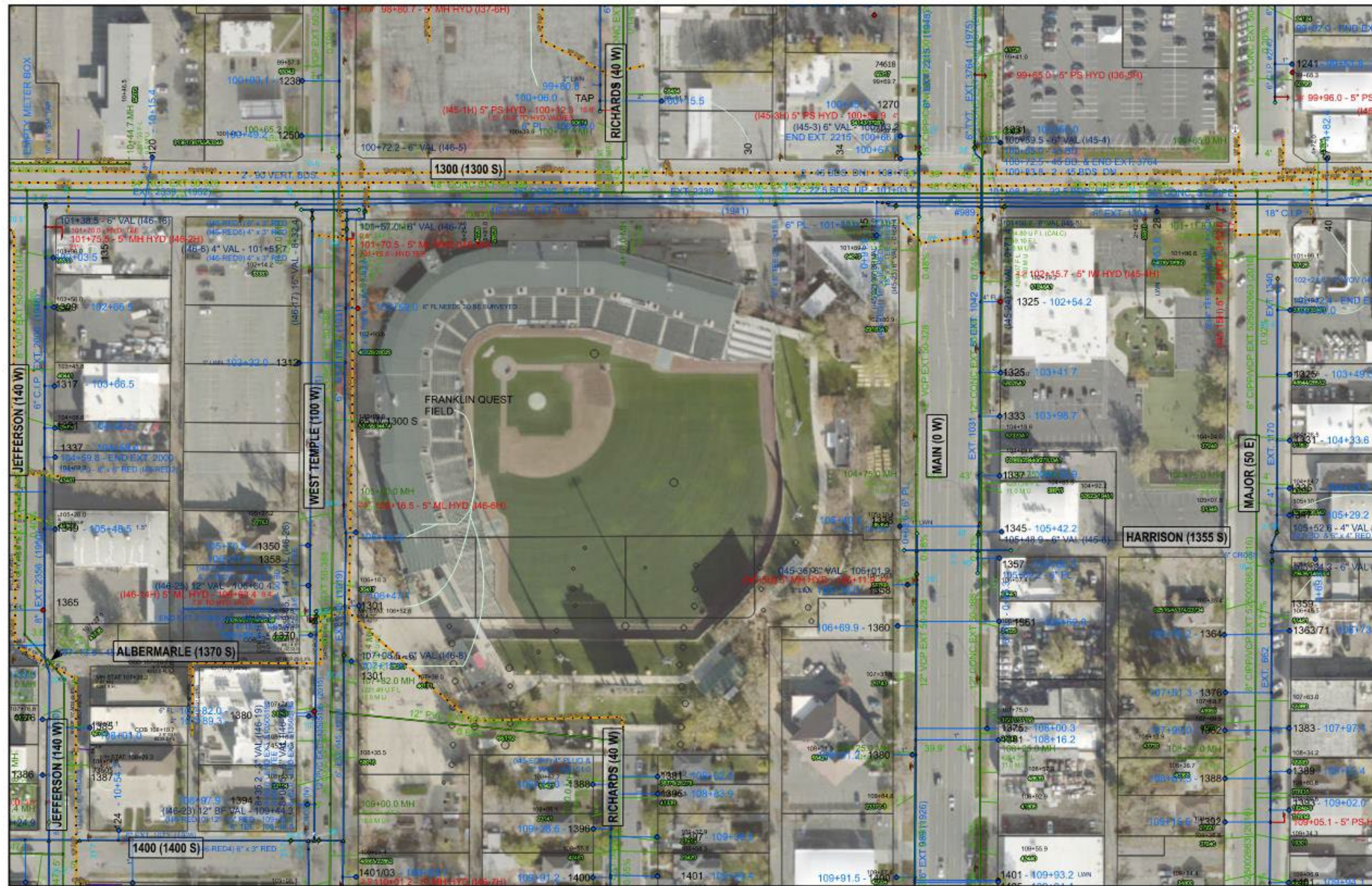
iii. Stormwater Management

In the north lot, the existing storm drain network under the parking lot consists of 15-inch reinforced concrete pipe and captures water throughout a series of catch basins. The north parking lot further drains underground to the north.

The ballpark portion of the property has a system of underground water storage tanks. The underground storage tanks can be found beneath the bleachers near the first and third base. These retention basins may be incorporated into future design after further investigation of the structural integrity.

As mentioned in Section B.ii. Hydrology, there are buried creeks that run under 1300 South. Dry Creek, Emigration Creek and Parleys Creek flow beneath the road. If a drainage connection is desired, special permission will need to be obtained.

As the site is developed, the design will need to meet Salt Lake City's requirements for stormwater quantity and quality. Salt Lake City requires that the 100-year storm be detained with a maximum of 0.2 cfs/acre release rate. The city also requires that their MS4 permit requirements are fulfilled by retaining the 80th percentile storm on site.



- Water Main
- Private Water Main
- Sewer Main
- Private Sewer Main
- Storm Mains
- Street Light

1300 S West Temple, Salt Lake City, UT



Figure 10: Existing Utilities

Date:	July 1, 2025
To	Marc Asnis, Perkins+Will
From	Brent Crowther, Kimley-Horn
Re:	BallPark Next Area Plan, Off-site Parking Inventory

Off-site parking inventory

A parking count was conducted to estimate available parking within a 1/4-mile radius of Smith's Ballpark. The analysis included both on-street and off-street parking. Note that the inventory does not consider existing utilization of off-site parking by existing residents or businesses. It is an estimate of the total supply of parking within proximity to the BallPark Next area venue.

On-street parking availability is summarized in Table 1. For streets with parallel parking, an average stall length of 22 feet was used to estimate capacity. For angled parking, a stall width of 7.5 feet was assumed.

Off-street parking availability summarized in Table 2. The inventory consists of larger nearby parking lots where event patrons may choose to park to attend events at the BallPark Next venue. The distance from each street and lot to Smith's Ballpark is shown in Figure 1.

Table 1 - On-Street Parking Availability

Number	Street Name	Extents	Street Side	Number of Spaces
1	Fremont Avenue	200 West to West Temple	North	30
			South	20
2	Paxton Avenue	200 West to West Temple	North	18
			South	18
3	Lucy Avenue	200 West to West Temple	North	23
			South	23
4	West Temple	Fremont Avenue to Merrimac Avenue	East	30
			West	23
5	Fremont Avenue	West Temple to State Street	North	45
			South	45
6	Richard Street	Fremont Avenue to Paxton Avenue	East	45
			West	45
7	Foulger Street	North of Kelsey Avenue	East	5
			West	5
8	Major Street	North of Kelsey Avenue	East	5
			West	5
9	Kelsey Avenue / Paxton Avenue	West temple to State Street	North	38
			South	38
10	Main Street	Harvard Avenue to Cleveland Avenue	East	80
			West	80
11	Major Street	Kelsey Avenue to Cleveland Avenue	East	62
			West	62
12	Jefferson Street	1300 South to Albemarle Avenue	East	8
			West	0
13	Jefferson Street	Albemarle Avenue to Local Access Road	East	14
			West	20
14	1400 South	Front Street to West Temple	North	13
			South	13
15	Merrimac / Cleveland Avenue	West Temple to State Street	North	52
			South	45
16	Richard Street	North of Merrimac Avenue	East	13
			West	13
Total Spaces:				936

Table 2 - Off-Street Parking Availability

Lot	Lot Name	Location	Number of Spaces
A	Calvary Baptist Church	North of Harvard Avenue between Main Street and State Street	71
B	TRAX Park and Ride	North of 1300 South by the Ballpark TRAX Station	167
C	Ballpark West Lot	West of Smith's Ballpark	107
D	Challenger School Lot	East of Smith's Ballpark	72
E	Commercial Lot	South of Cleveland Avenue between Main Street and Major Street	74
F	SLCC Lot	North of 1700 South between State Street and 300 East	1050
Total Spaces:			1541

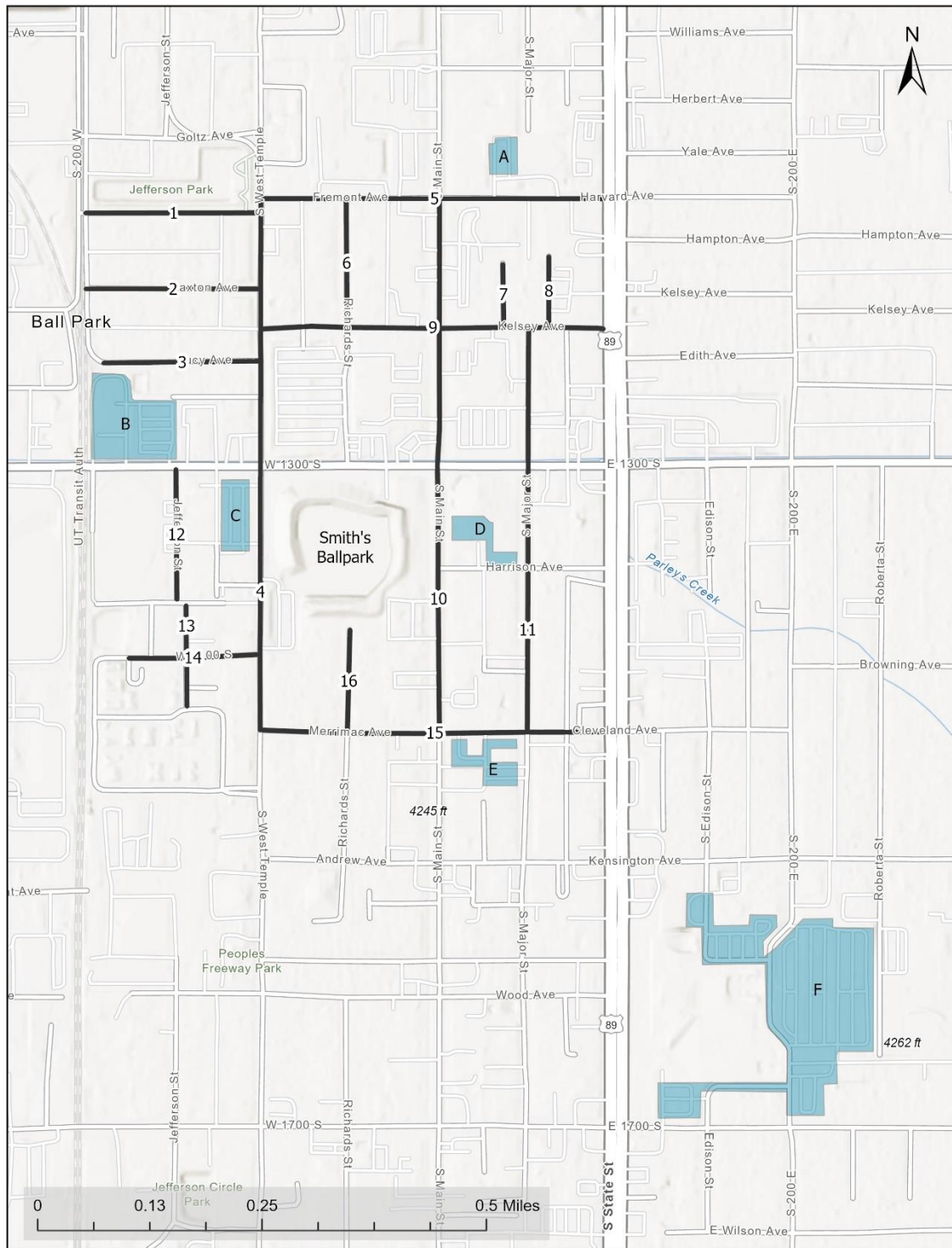


Figure 1 - Off-Site Parking Locations

Appendix D:

Existing Structure Limited Condition Assessment

November 6, 2024

Marc Asnis
Project Manager
Perkins&Will, Inc.
2 Bryant Street, Suite 300
San Francisco, CA 94105
RDA Ballpark Site – Existing Structure Limited Condition Assessment
Salt Lake City, UT

Dear Mr. Asnis:

At your request, Kimley-Horn performed a limited condition assessment of Smith's Ballpark located in Salt Lake City, Utah. The objective of our work was to visually observe the general condition of the structural elements within the stadium and identify deficient items to assess the feasibility of leaving the existing structural system for future construction. Our scope of work to date includes a field visit, review of as-built documents and preparation of this report.

BACKGROUND INFORMATION

Smith's Ballpark is an existing baseball stadium that is roughly 30 years old, designed using the 1991 Utah Building Code. The main structural framing system consists of cast-in-place (CIP) concrete bearing walls and steel framing and columns with precast risers in the elevated seating area, composite steel deck on the second and third floors, and metal deck on steel trusses for the high roof. The lateral force resisting system of the structure is special concrete shear walls at the ground level, and special steel moment frames at elevated levels. Infill, non-load bearing concrete masonry unit (CMU) walls were also observed at the ground level. Underneath the first level of precast seating risers there are cast-in-place concrete tunnels comprised of retaining walls that lead to the field and dugouts.

Smith's Ballpark is divided into three areas, named the south wing, center structure, and east wing. Refer to the schematic site plan shown below. The south and east wings are open-air areas where most of the seating is located. The center structure contains enclosed, suite-style seating. Smith's Ballpark has three levels, the ground level, second floor (concourse area), and the third floor (suite level). An expansion joint between the three structures is along column lines 14 and 30, refer to the as-built documents for column lines. These three areas are structurally isolated and are identifiable in the field due to the presence of a double column and framing line.

The ground floor includes light storage areas, office spaces for employees, and locker rooms for the athletes. There are two retention basins on this level at the south and east wings, past the isolation joints. The basin in the east wing is fifteen feet wide by sixty-four feet long, and the one in the south wing is ten feet wide by about eighty-five feet in length. The basins store excess runoff from the stadium until it can be released back to the city. The entirety of the ground floor is slab-on-grade construction with cast-in-place load-bearing walls that have continuous footings, as well as isolated footings for the steel columns near the perimeter of Smith's Ballpark.

The main concourse level is comprised of concrete on composite deck supported on non-composite steel framing, elevated precast risers for seating above, and CMU infill walls that enclose various concession stands and other small spaces. The main concourse level is supported by CIP concrete bearing walls below. The main entrance into the stadium is comprised of elevated CIP concrete stairs. A masonry façade is attached to the steel framing inside the concourse area and around the exterior of the stadium.

The third floor suite level is comprised of normal weight concrete on composite concrete deck supported by non-composite steel framing, and precast concrete seating risers supported on steel framing. The center structure at this level is enclosed and contains suite-style seating. The south and east wings are open areas for general seating. The roof of Smith's Ballpark is comprised of an untopped metal deck, bearing on custom steel trusses.

The existing structure was designed for the following loads and soil bearing:

1.	SUPERIMPOSED LOADS USED IN DESIGN	
	A.	SEISMIC LOADS PER UBC 1991
		(1) ZONE 3
		(2) SITE COEFFICIENT, S 15
		(3) IMPORTANCE FACTOR 10
		(4) STRUCTURE COEFFICIENT (RW) 6
	B.	WIND PRESSURE PER UBC 1991
		(1) REFERENCE WIND VELOCITY 70 MPH
		(2) EXPOSURE CLASSIFICATION B
		(3) IMPORTANCE FACTOR 10
	C.	SUPERIMPOSED GRAVITY LOADS:
		(1) ROOF WITH DRIFT PER UBC APPENDIX 30 PSF
		(2) FLOORS:
		(A) SUITES + PRESS BOX 60 PSF
		(B) CONCOURSE + EXITS 100 PSF
		(C) FIXED SEATING AREAS 75 PSF
2.	FOUNDATIONS	
	A.	FOUNDATION DESIGN BASED ON GEOTECHNICAL INVESTIGATION BY KLEINFELDER, FILE NO. 30-8018-05.001, APRIL 14, 1993.
	B.	ALLOWABLE TOTAL LOAD BEARING PRESSURE USED FOR DESIGN 3,000 PSF
	C.	COMPACTED STRUCTURAL FILL REQUIRED BELOW ENTIRE BUILDING AREA

Kimley-Horn performed a limited structural condition assessment to document the existing framing system of Smith's Ballpark. The following sections of this report discuss the assessment and subsequent findings.

FIELD VISIT

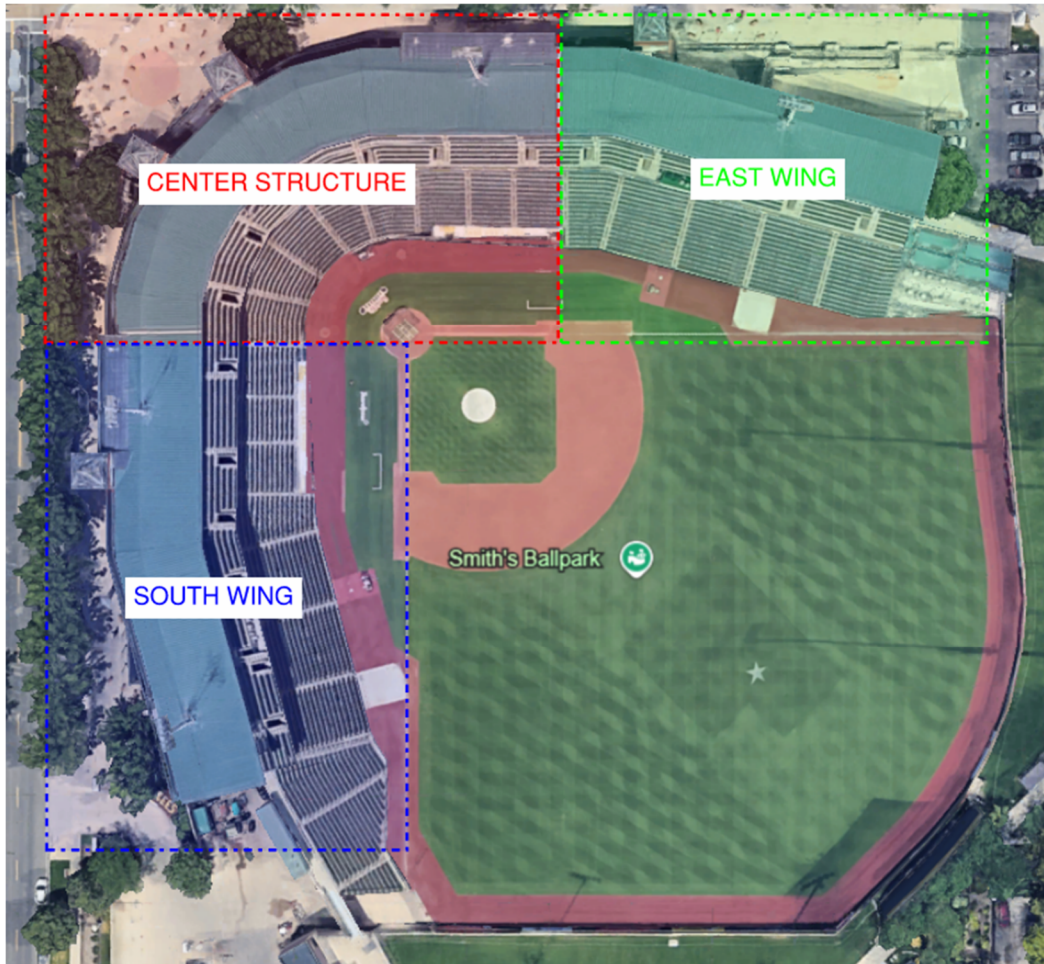
Kimley-Horn performed an evaluation of Smith's Ballpark on October 23, 2024. Christine Herrick, Josh Lampson, and Nathan Grimm performed the assessment of the structures which included a visual survey and limited hammer sounding of select concrete areas. No destructive or intrusive testing was performed as part of our evaluation. Enclosed areas or otherwise inaccessible spaces were visually reviewed where accessible.

In general, the condition of the structures was typical for steel structures of similar age and construction type in this region. No immediate life-safety issues were observed. Overall, the structure appeared to be

performing well and the deficiencies that need to be addressed relate to typical serviceability and maintenance items for outdoor exposure conditions.

Observations will be discussed in more detail in the following sections. Documented distress can be viewed in the photos provided in Appendix A. Rating Guidelines are set forth in Appendix B.

SITE MAP



STRUCTURAL OBSERVATIONS

LEVEL	STRUCTURAL ELEMENT	RATING	KEY OBSERVATIONS	RECOMMENDATIONS
Ground Floor	Slab-on-Grade	Fair	<ul style="list-style-type: none"> • Medium cracks at reentrant corners near doorways (Appendix A, Photos 1 & 2) • Wide cracks independent of control joints in tunnel areas (Appendix A, Photo 3) 	<ul style="list-style-type: none"> • Seal cracks with sealant, epoxy finish, or mortar
	CIP Concrete Bearing/Retaining Walls	Fair	<ul style="list-style-type: none"> • Small/hairline horizontal and vertical cracks in tunnel areas (appendix A, Photos 6 & 7) • Efflorescence at steel connections (Appendix A, Photo 5) 	<ul style="list-style-type: none"> • Seal cracks with sealant, epoxy finish, or mortar • Further investigation of CIP walls in tunnels areas recommended should they remain in place, especially at locations where new openings in existing shear walls are desired. • Remove efflorescence from effected areas using abrasive blasting or a non-destructive chemical solution
	Infill CMU Walls	Fair/Poor	<ul style="list-style-type: none"> • Large vertical and stair-step cracking in tunnel areas (Appendix A, Photos 9 & 10) 	<ul style="list-style-type: none"> • Seal cracks with sealant, epoxy finish, or mortar • If visual appearance is important to Owner, remove and replace

STRUCTURAL OBSERVATIONS

LEVEL	STRUCTURAL ELEMENT	RATING	KEY OBSERVATIONS	RECOMMENDATIONS
<p align="center">2nd Floor (Concourse Level)</p>	Elevated Composite Slab	Good	<ul style="list-style-type: none"> Sparse hairline cracks typical in concrete of this age and exposure 	<ul style="list-style-type: none"> N/A
	Steel Framing & Connections	Good	<ul style="list-style-type: none"> <10% of total steel members had corrosion – most occurred near drainage areas frequently exposed to water (Appendix A, Photos 11-14) Paint was still in good condition; minor chipping was found All steel-to-steel connections appeared to be in good condition with no missing structural elements (appendix A, Photos 15 & 16) Corrosion observed in outer columns and cantilevered beams near elevated seating area, especially at expansion joints. Note that most corrosion was encountered at steel-to-precast riser connections frequently exposed to water. (Appendix A, Photos 11-13) 	<ul style="list-style-type: none"> Abrasively blast areas that are rusted and repaint where necessary
	Precast Concrete Risers	Good	<ul style="list-style-type: none"> Water damage was observed only at select steel connections that experienced the highest runoff concentrations where the risers are attached to steel beams (Appendix A, Photos 17-20). 	<ul style="list-style-type: none"> Assess condition of concrete after abrasive blasting of steel connections. If deterioration is present, repair as needed with epoxy, sealant, or grout.

STRUCTURAL OBSERVATIONS

LEVEL	STRUCTURAL ELEMENT	RATING	KEY OBSERVATIONS	RECOMMENDATIONS
2nd Floor (Concourse Level)	Steel Stair Framing	Fair	<ul style="list-style-type: none"> Steel framing on underside of stairs is rusted and a noticeable amount of paint is peeling (Appendix A, Photos 21 & 22) 	<ul style="list-style-type: none"> Abrasively blast all rust off steel plates and repaint affected area.
3rd Floor (Suite Level)	Elevated Composite Slab	Good/Fair	<ul style="list-style-type: none"> Medium-sized cracks spanning along deck width throughout (Appendix A, Photos 23-26), primarily propagating from columns and area drains. 	<ul style="list-style-type: none"> Seal cracks with sealant, epoxy finish, or mortar
	Steel Framing & Connections	Good	<ul style="list-style-type: none"> Minor corrosion and paint chipping in columns, steel trusses, and connections (Appendix A, Photo 27) 	<ul style="list-style-type: none"> Abrasively blast areas that are rusted and repaint where necessary
	Untopped Metal Roof Deck	Very Good	<ul style="list-style-type: none"> Minor corrosion and paint chipping (Appendix A, Photos 28-30) 	<ul style="list-style-type: none"> Abrasively blast areas that are rusted and repaint where necessary

DISCUSSION – GROUND LEVEL

Slab on Grade: Throughout the bottom floor slab-on-grade we observed a pattern of medium-sized cracks at forty-five-degree angles that occurred at reentrant corners, typically around door openings. See photos 1 and 2 in Appendix A. These cracks may have occurred due to a lack of trim bars at these corners, or inadequate placement of trim bars. Subtle movements of the wall near the corners can contribute to these types of cracks as well, whether it be expansion/contraction due to temperature changes or deflection due to lateral load. Salt Lake City experienced a notable earthquake in 2020, which could have contributed to some of the concrete distress noted throughout this report.

Along with frequent cracking at reentrant corners, we also noticed wide horizontal cracks that were independent of the control joints in the tunnels that lead to the field and dugouts. See photo 3 in Appendix A. Cracks like these can occur for a variety of reasons such as repeated heavy loading, improper subgrade preparation, or excessive ground motion during the structure's life. These cracks are non-structural but should be repaired to prevent them from expanding.

CIP Concrete Bearing/Retaining Walls: The cast-in-place concrete walls in Smith's Ballpark are on the ground floor and run along the corridor below the main concourse area, as well as the tunnels that lead to the field. The walls in the corridor were observed to be in good condition with very limited hairline cracks that do not appear to impact the structural integrity of the walls. See photo 4 in Appendix A for the typical condition of the walls in this area. Efflorescence was observed on select concrete walls near the retention basins on the south and east wings of the structure. It occurs on the upper portion of the wall where there are steel connections to the CIP concrete bearing walls in this area and surface water is able to run through small joints. See photo 5 in Appendix A. The efflorescence appears to only be surface-level and does not seem to affect the capacity of the concrete.

The cast-in-place walls along the tunnels were found to have small horizontal and vertical cracks. This occurred in every tunnel, and the horizontal cracks are believed to be flexural cracks caused by the pressure of the earth fill behind the walls. Photos 6 and 7 in Appendix A document the cracks observed in the tunnels. KH recommends these walls be further investigated should they remain in place or be removed for new construction.

Infill CMU Walls: The infill CMU walls on the ground floor are located at the outer extents of each wing as well as along the tunnels to the field. Based on our observation, the CMU walls at the outer extents of the structure were in overall good shape with very minimal water damage. See photo 8 in Appendix A.

Inside the tunnels, the infill CMU walls have large vertical and stair-step cracks at the corners and edges. See photos 9 and 10 in Appendix A. Both can be a result of differential settlement, active soil pressure behind the wall, or excessive deflection of a member above the wall that distributes unforeseen vertical loads to the wall.

DISCUSSION – SECOND FLOOR

Elevated Slab: The second-floor diaphragm consists of concrete on composite metal deck. During the site visit, we found the deck to be in overall good condition with occasional hairline cracks that can be expected in concrete of this age.

Steel Framing and Connections: The steel framing on the second floor was exposed and observed to be in good condition. All the members were intact, and the connections were observed without any missing hardware (except for one connection in the Women's Bathroom). We observed corrosion on less

than ten percent of steel members throughout the structure. Concentrated areas of corrosion were observed at the end of the cantilevered beams where they connect to the precast concrete seating risers. Other concentrated areas of steel corrosion were observed near the expansion joints. See photos 11 through 14 in Appendix A for examples of observed steel corrosion. This corrosion may be due to water leakage and intrusion, since it is concentrated near drainage areas and expansion joints, where water is more likely to collect. None of the observed corrosion appears to impact the structural integrity of the steel framing and connections. Typical maintenance procedures, such as abrasive blasting and repainting are recommended.

Precast Concrete Seating Risers: Precast concrete seating risers span between cantilevered steel beams in the elevated levels of Smith's Ballpark. These precast members were observed to be in good condition. See photos 17 through 20 in Appendix A. Efflorescence was observed and believed to be caused by water damage where the precast risers connect to the steel beams. The precast risers are simply-supported in a single-span condition. This span condition allows water to freely flow between the joints on each end of the risers and deteriorate the concrete below.

Steel Stair Framing: Significant water damage was observed on the underside of the steel stair framing at the elevated seating levels, as documented in photos 21 and 22 in Appendix A. We observed that the steel stair framing has rusted to the point where a large amount of paint was peeling away from the steel. This occurs regularly at the exposed stairs on this level in varying degrees of severity. It's believed that water collects on the steps and slowly seeps through the joints, causing the steel to oxidize. We suspect preventative maintenance procedures, such as abrasive blasting and re-painting, should be enough to mitigate this issue.

DISCUSSION – THIRD FLOOR

Elevated Slab: Similar to the second floor (concourse level), the third floor (suite level) elevated slab is concrete on composite metal deck framed across steel beams. We observed large cracks forming along the width of the deck on each wing of Smith's Ballpark. We observed that the cracks typically started from the column flanges on this level and continued across the full length of the deck, with cracks forming parallel to the deck span on one or both sides of the steel beam. See photos 23 through 26 in Appendix A. These cracks are not believed to be a major structural concern. We believe the cracks are due to the negative moment introduced over the support element, and the lack of reinforcing to mitigate cracking over the beam. This beam does not carry any deck load, as it is parallel to the deck span. Negative moment is likely transferred here due to the bay aspect ratio being less than 2:1, allowing the deck to transfer load to the beam framing as a two-way slab. Other medium-sized cracks were found in this area that may be due to expansion and contraction of the concrete in the continuous deck. These cracks may have been prevented by having reinforcing located perpendicular to the framing member. Overall, the third-floor deck was observed to be in good/fair condition.

Steel Framing and Connections: The steel framing on the third floor (suite level) was observed to be in better condition than that of the second floor since a majority of these members are in enclosed or covered spaces. The steel columns and trusses supporting the roof deck above were observed with minimal signs of oxidization and chipping paint and seemed to be in good condition. See photo 27 in Appendix A.

Untopped Metal Roof Deck: The roof above the third floor (suite level) showed minimal signs of corrosion and was observed to be in good condition. See Photos 28 through 30 in Appendix A. The paint was observed without chips or peeling areas, and the metal deck was observed to be securely fastened to the roof framing.

REPAIR RECOMMENDATIONS

Concrete Slabs: No observed cracks in concrete slabs appear to affect the structural integrity of the members. It is our belief that cracks in the concrete slabs are related to serviceability of the structure and require preventative maintenance to prevent future deterioration. We recommend sealing these cracks with concrete sealant, epoxy, or a patching mortar depending on the desired finish on each level.

Concrete Walls: A majority of the distress observed in the concrete walls does not appear to affect their structural integrity. We recommend the horizontal cracks in the concrete walls located in the tunnel areas be further investigated should these walls remain in place. At all other locations, we recommend sealing cracks with concrete sealant, epoxy, or a patching mortar depending on the desired finish on each level. Where applicable, we recommend removing the efflorescence from effected concrete areas using abrasive blasting or a non-destructive chemical solution.

Infill CMU Wall: Although the cracks that formed in the infill CMU in the tunnels on the ground floor are believed to be nonstructural, they were observed to be large. We recommend using a sealant, epoxy, or patching mortar to repair these cracks, or choosing to demo and replace with new infill walls.

Structural Steel Framing, Steel Stair Framing, and Steel Connections: Based on our site observations, we believe that the deterioration of the steel members and connections does not impact the structural integrity of the members. We recommend preventative maintenance of all steel components and connections. To prevent further deterioration, KH recommends abrasive blasting to remove all rust and applying a new layer of protective paint.

Precast Concrete Seating Risers: The precast risers were observed to be in overall good condition. Limited areas were observed that would benefit from preventative maintenance procedures. These areas are typically located where the precast risers come into contact with deteriorating portions of steel beams. We recommend removing the efflorescence from effected areas using abrasive blasting or a non-destructive chemical solution. After removing efflorescence and cleaning the surface, a structural engineer should be reengaged to review connections to verify that significant section lost at connections is not present. After this observation, additional repair may be required.



STRUCTURAL ASSESSMENT SUMMARY

Based on Kimley-Horn's observation of the site, no immediate structural retrofits are recommended for the building in the existing condition, and Smith's Ballpark was observed to be in an overall good condition. Kimley-Horn recommends that preventative maintenance activities be carried out by the Owner for prolonged serviceable life of the structure to remain in place.

We appreciate the opportunity to work with Perkins&Will, Inc. on this project. If you have any further questions, or if we can be of further assistance, please contact Kimley-Horn.

Very truly yours,

KIMLEY-HORN AND ASSOCIATES, INC.

Christine Herrick, SE

Josh Lampson, PE

Appendix A –

Site and Building Photos

RDA Ballpark
Appendix A

KHA Job No: 193072002

KHA Rep: Christine Herrick

Date: 11/1/2024

Page: 1 of 15

Photo No. 1



Remarks:

SOG Crack at Reentrant Corner

Location:

Ground Floor

Photo No. 2



Remarks:

SOG Crack at Reentrant Corner

Location:

Ground Floor

RDA Ballpark
Appendix A

Photo No. 3



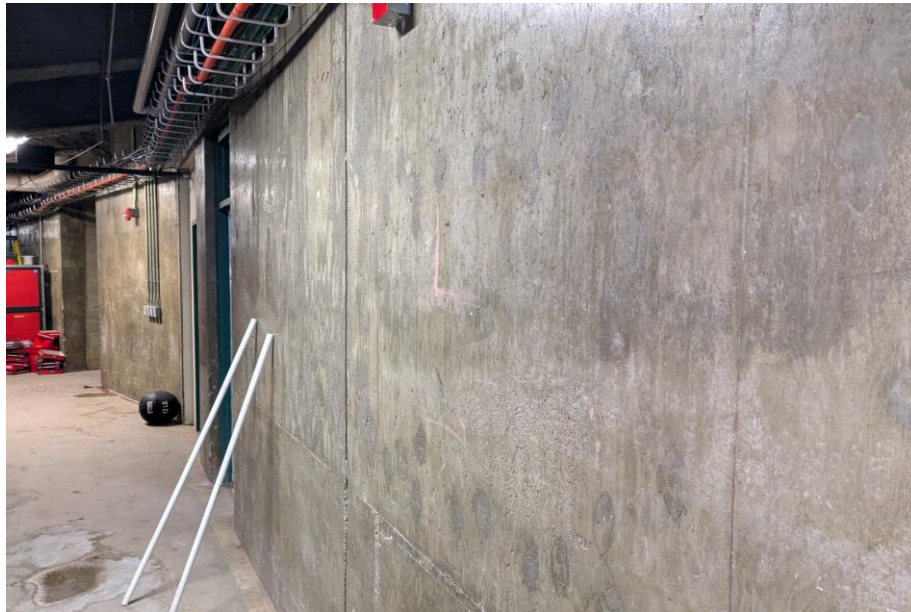
Remarks:

SOG Crack Between Control Joints

Location:

Ground Floor

Photo No. 4



Remarks:

Typical CIP Wall Along Back Corridor

Location:

Ground Floor

RDA Ballpark
Appendix A

Photo No. 5



Remarks: Efflorescence at CIP Wall and Steel Connection

Location: Ground Floor

Photo No. 6



Remarks: Cracking and Efflorescence in CIP Wall

Location: Ground Floor Tunnel

RDA Ballpark
Appendix A

Photo No. 7



Remarks:	Cracking in CIP Wall
Location:	Ground Floor Tunnel

Photo No. 8



Remarks:	CMU Wall at End of Wing
Location:	Ground Floor

RDA Ballpark
Appendix A

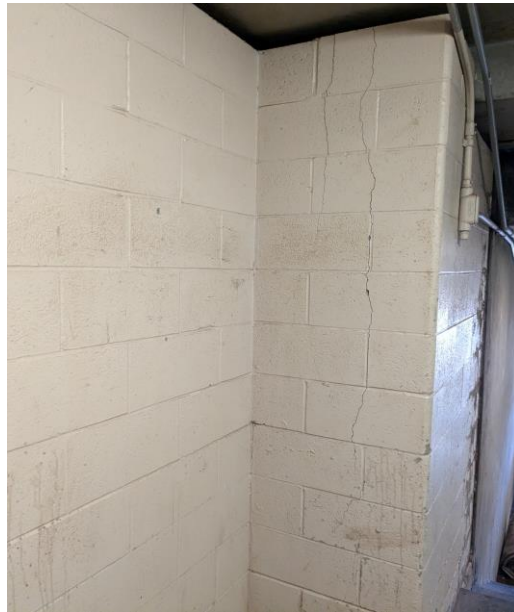
KHA Job No: 193072002

KHA Rep: Christine Herrick

Date: 11/1/2024

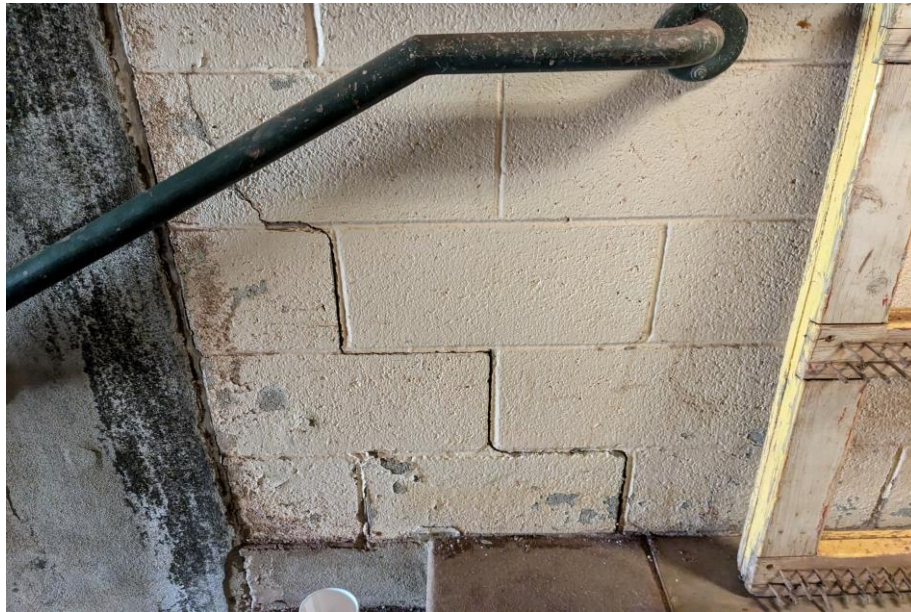
Page: 5 of 15

Photo No. 9



Remarks:	Cracking in CMU Infill
Location:	Ground Floor Tunnel

Photo No. 10



Remarks:	Cracking in CMU Infill
Location:	Ground Floor Tunnel

Photo No. 11



Remarks:	Corrosion at Precast Riser Connection
Location:	Second Floor - Below Elevated Seating

Photo No. 12



Remarks:	Corrosion at Double Colum Line
Location:	Second Floor - Below Elevated Seating

RDA Ballpark
Appendix A

KHA Job No: 193072002

KHA Rep: Christine Herrick

Date: 11/1/2024

Page: 7 of 15

Photo No. 13



Remarks:	Corrosion at Double Colum Line
Location:	Second Floor - Below Elevated Seating

Photo No. 14



Remarks:	Column Rust
Location:	Second Floor

RDA Ballpark
Appendix A

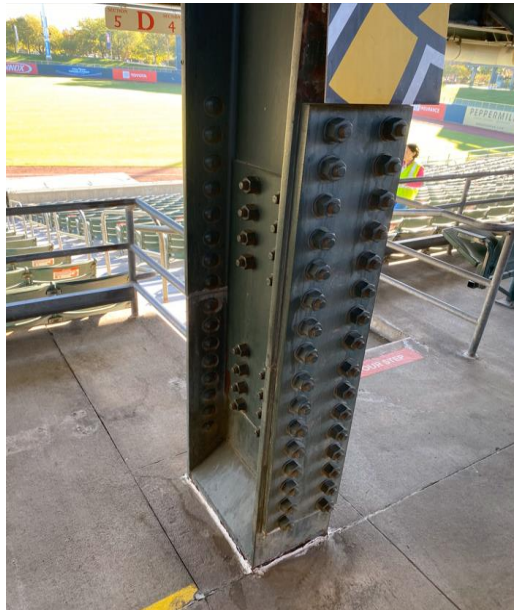
KHA Job No: 193072002

KHA Rep: Christine Herrick

Date: 11/1/2024

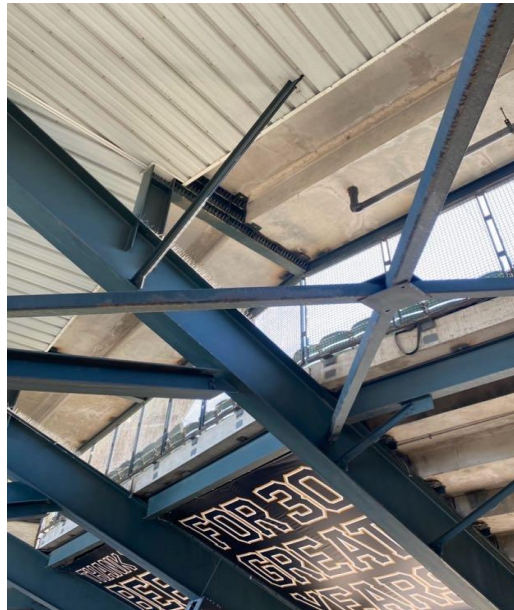
Page: 8 of 15

Photo No. 15



Remarks:	Typical Column Splice
Location:	Second Floor

Photo No. 16



Remarks:	Typical Steel Framing
Location:	Second/Third Floor

Photo No. 17



Remarks:	Precast Risers and Steel Framing
Location:	Second Floor - Under Elevated Seating

Photo No. 18



Remarks:	Precast Risers and Steel Framing
Location:	Second Floor - Under Elevated Seating

Photo No. 19

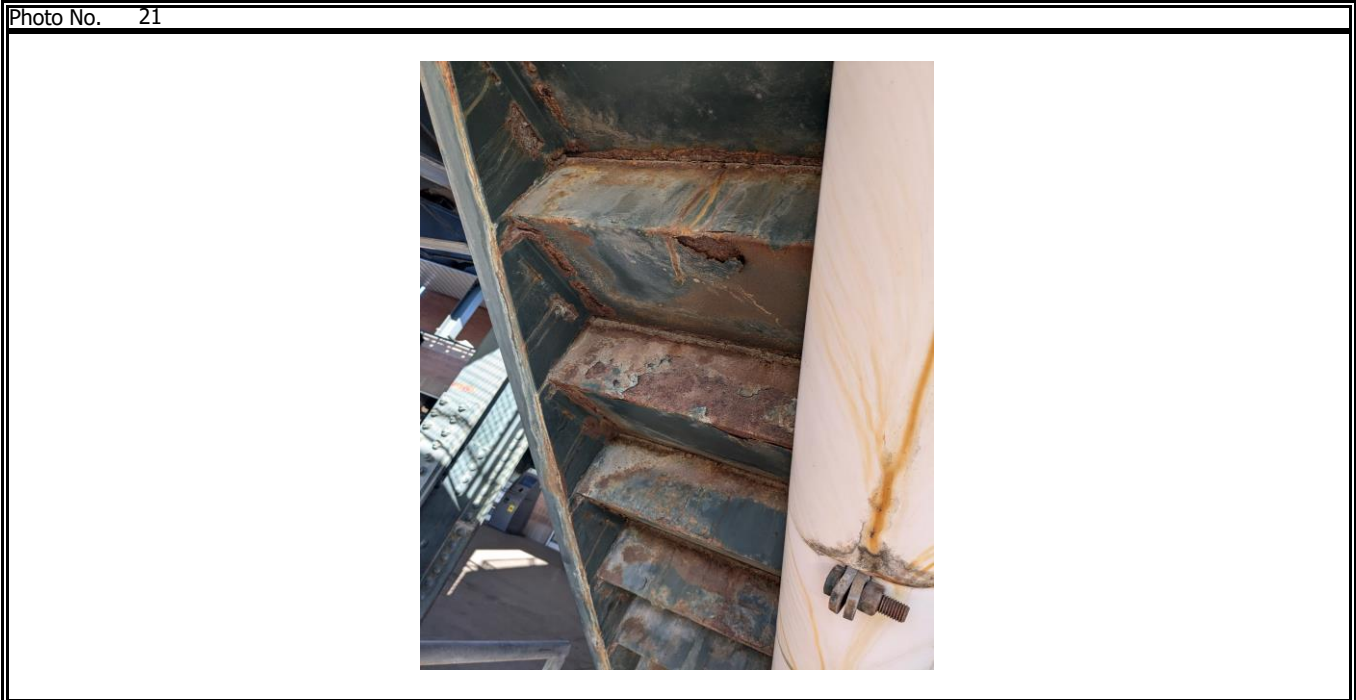


Remarks:	Precast Risers and Steel Framing
Location:	Second Floor - Under Elevated Seating

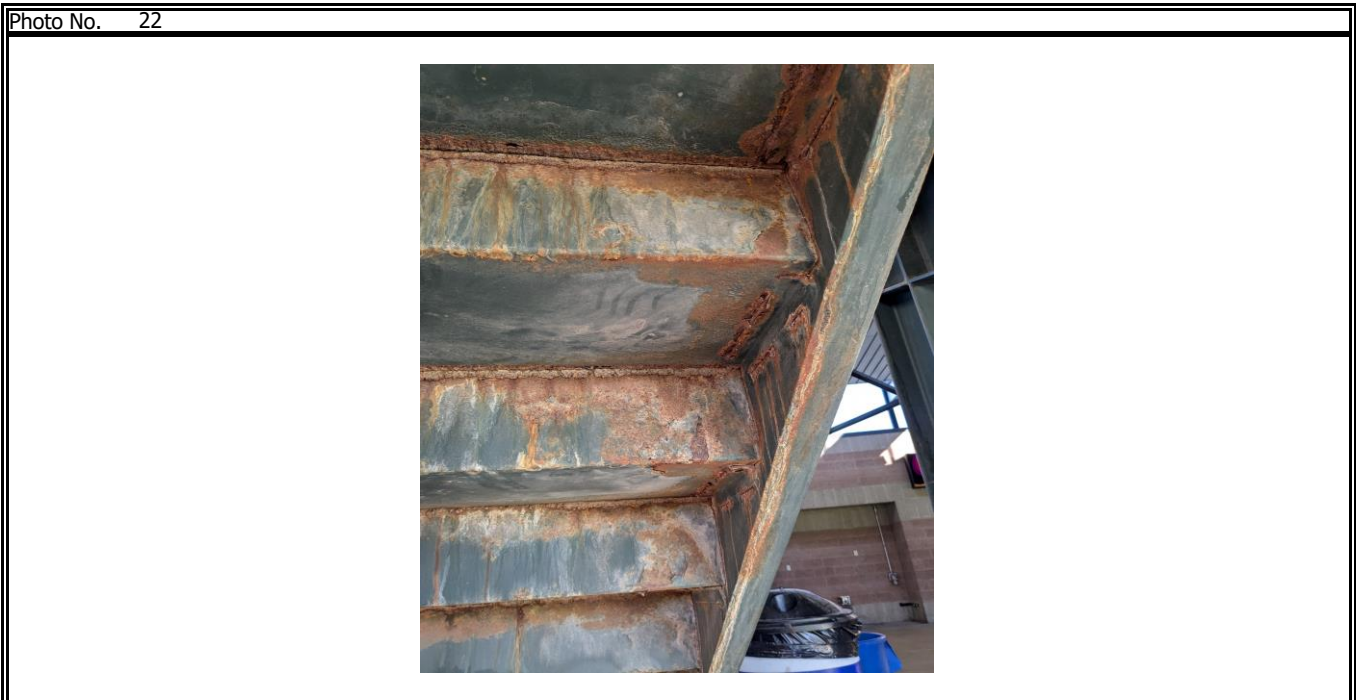
Photo No. 20



Remarks:	Precast Risers and Steel Framing
Location:	Second Floor - Under Elevated Seating



Remarks:	Underside of Stairwell
Location:	Second Floor



Remarks:	Underside of Stairwell
Location:	Second Floor

RDA Ballpark
Appendix A

Photo No. 23



Remarks:

Elevated Slab Cracking

Location:

Suite Level

Photo No. 24



Remarks:

Elevated Slab Cracking

Location:

Suite Level

RDA Ballpark
Appendix A

Photo No. 25



Remarks:	Elevated Slab Cracking
Location:	Suite Level

Photo No. 26



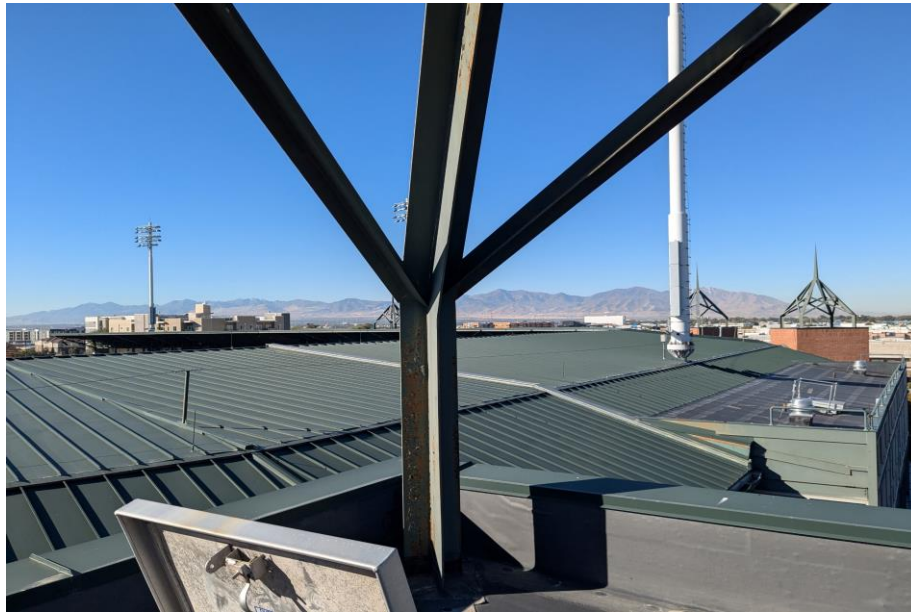
Remarks:	Elevated Slab Cracking
Location:	Suite Level

Photo No. 27

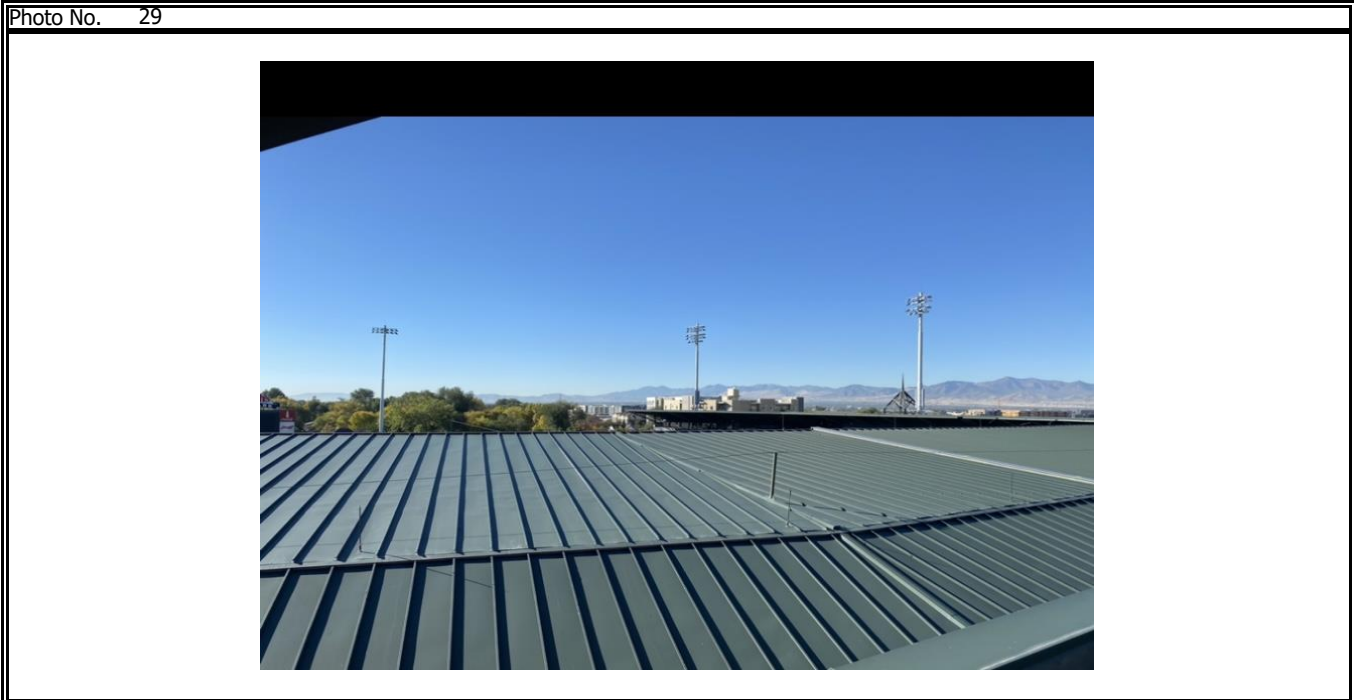


Remarks:	Typical Steel Framing and Connections
Location:	Suite Level

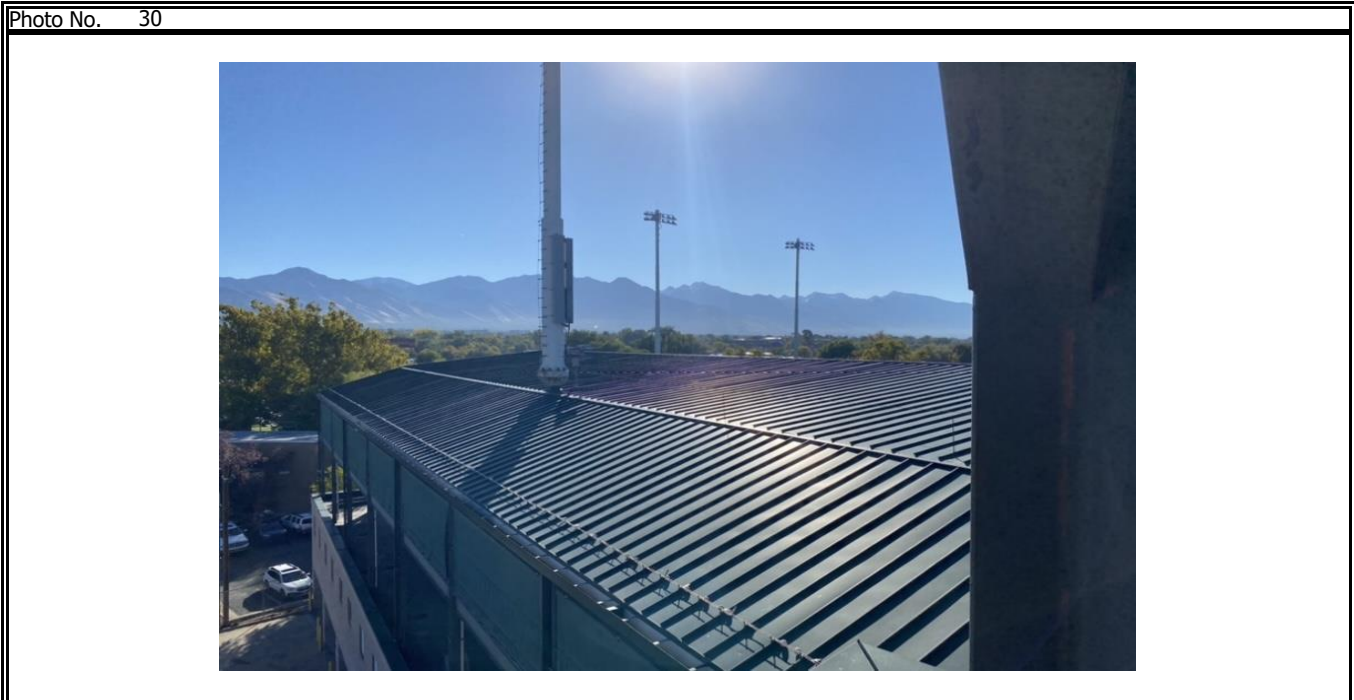
Photo No. 28



Remarks:	Untopped Roof Deck
Location:	Roof Level



Remarks:	Untopped Roof Deck
Location:	Roof Level



Remarks:	Untopped Roof Deck
Location:	Roof Level

Appendix B – Rating Guidelines

The following narrative provides a summary of the rating guidelines.

Rating Guidelines

Very Good – rating denotes no life-safety issues, no immediate losses of strength or performance, including aesthetics, and no short-term changes in performance with regular maintenance and observation. Overall, the structure has minor cosmetic deficiencies, very minor concrete damage, and no leaks or leaching.

Good – rating denotes no life-safety issues, no immediate losses of strength or performance, including aesthetics, and no short-term changes in performance with regular maintenance and observation. A structural system is said to be in good condition if there is minor concrete damage, minimal rust, and minor leaks or leaching. An operational system is said to be in good condition if the system is in good working order with minor cleaning or routine maintenance required.

Fair – rating denotes no life-safety issues and functional performance, but repairs are needed to maintain the current level of service. There are some aesthetic issues and inconveniences to patrons. Without repairs, the deterioration will continue to accelerate. Fair condition is assigned to the structural system if moderate damage, rust, leaks, or leaching is found in several locations or if severe damage is found in a few locations.

Poor – rating denotes obvious problems, even to the casual observer, that without immediate remediation will result in further loss of structural member capacity. This condition can produce noticeable deflections in members, cause loose concrete to spall away, and presents the possibility of an unsafe condition to vehicles and/or pedestrians in the near future. The system may still be functioning at this state, but repair costs will increase rapidly with the amount of time that passes before the item is corrected. The structural system is considered poor if severe damage is found in several locations. A poor assessment is assigned to any operational system that requires replacement.

N/A – Not Applicable to this garage. While typically included as a part of our normal condition assessment, this particular category of items was not originally installed in this garage or was not part of the scope of this evaluation

Definitions:

The photographs provided below are sample representative photographs of each definition taken at other locations. These photographs were not taken at Smith's Ballpark.



Figure 1: Failing concrete patch



Figure 2: Concrete Delamination



Figure 3: Concrete spall

1. **Patches** – Repairs to concrete that require a filler material, usually grout or repair mortar. Patches may be required due to the following causes: reinforcing steel corrosion, concrete spalling, concrete delaminations, failing grout pockets, or external forces, such as automobile collisions. See Figure 1.
2. **Delamination** – A delamination of concrete occurs when bleed (excess) water rises in concrete to just below the concrete surface as the concrete is curing. This leaves a thin layer of concrete that separates from the concrete body. Delaminations are detectable by sounding the suspected concrete with a metal object, such as a chain or hammer, and listening for the distinctive dead/hollow sound produced. The sounding instrument will produce a crisp metal ring when sounding over solid concrete. See Figure 2.
3. **Spall** – A spall is generally a piece of concrete that separates from the main body of the concrete member. Examples include a grout patch that has shrunk away from its pocket or a corner of a member that has broken off and is in the process of falling away or has fallen away already. See Figure 3



Figure 4: Fine crack



Figure 5: Medium Crack



Figure 6: Wide crack



Figure 7: Leaking and leaching concrete

4. **Cracking** – Cracks are qualitatively assigned values of Fine (F), Medium (M) or Wide (W). A fine crack is a hairline crack less than 0.01” thick and can sometimes be very hard to see. A medium crack is in between 0.01” and 1/32” wide and a wide crack is anything greater than 1/32” wide. Cracks are qualitatively rated according to their dominant characteristics. See Figures 4, 5 and 6 for fine, medium and wide width cracks, respectively.

5. **Leaking** – Leaking is determined by any observable sign of fluid flow through a crack or joint. See Figure 7.
6. **Leaching** – Leaching occurs when water flows through a crack, thereby dissolving away the components of the concrete, mainly calcium, and depositing it in the form of a whitish film or stalactite on the surface. The whitish film is also called efflorescence. See Figure 7.



Figure 8: Ponding Water

7. **Ponding** – Ponding is the settling of water in low areas where it has no way to drain. This not only presents a pedestrian safety hazard, especially if it turns into ice, but as water penetrates the concrete the deterioration mechanism is accelerated. See Figure 8.



Figure 9: Structural steel rust & scaling

8. **Rust and Scaling** – Rust occurs when steel is oxidized. Air, water and chlorides are some agents that typically cause steel to rust. Scaling is a more advanced condition of rust and occurs when rust is deeper than the surface and flakes off in pieces or 'scales'. See Figure 9.

Appendix E:
1300 South Lane Reduction Evaluation



MEMORANDUM

To: Marc Asnis
From: Brent Crowther, PE; Leslie Morton, PE
Kimley-Horn and Associates, Inc.
Date: May 12, 2025
Subject: 1300 South Lane Reduction Evaluation

Introduction

Salt Lake City is proceeding with the redevelopment of the 13.5-acre Ballpark Site at the corner of West Temple and 1300 South. The plan aims to balance historic preservation with new development, including partial reuse of the stadium while creating a mixed-use area focused on arts, culture, and community gathering spaces.

The purpose of this memorandum is to provide information and analysis regarding the possibility of reducing lanes on 1300 South between 300 West to State Street in Salt Lake City. The lane reduction would support the development by providing room for enhanced pedestrian facilities, and improved safety for people crossing I-15.

EXISTING FACILITIES

1300 South is an east-west corridor featuring two lanes in each direction of travel, accompanied by a center turning lane. This center lane alternates between a dedicated left turn lane and a two-way left turn lane along the entire study corridor. Following the intersection at 1300 South and State Street, the road transitions into a three-lane facility, consisting of a center turn lane and one travel lane in each direction.

1300 South is classified as an arterial city street in the Salt Lake Transportation Master Plan. The Plan defines arterial city streets as “generally Multi-Lane streets carrying high traffic volumes at relatively high speed limits” and as “commuter streets [that] typically offer controlled access to abutting property”.¹

LANE REDUCTION SCENARIO

The proposed reduction of 1300 South would create a three-lane road with one lane each direction and a center turn lane alternating between a left turn lane and a two-way turn lane. The lane

¹ https://www.slc.gov/transportation/wp-content/uploads/sites/11/2018/12/TransportationMasterPlan_20181113-2.pdf

reduction would begin east of 300 West (mid-block between 300 W and the TRAX tracks) and end at State Street. The road would widen to two-lanes in each direction at Main Street.

TRAFFIC OPERATIONS ANALYSIS

Turning movement count data were collected on Tuesday, February 11, 2025, for the following study area intersections:

- 400 West and 1300 South
- 300 West and 1300 South
- West Temple and 1300 South
- Main Street and 1300 South
- State Street and 1300 South

Study area intersections were analyzed based on average total delay for signalized and unsignalized intersections as presented in the Transportation Research Board’s Highway Capacity Manual, 7th Edition (HCM 7). Under the unsignalized analysis, the level of service (LOS) for a two-way stop controlled (TWSC) intersection is determined by the computed or measured control delay and is defined for each minor movement. LOS for a two-way stop-controlled intersection is not defined for the intersection. LOS for signalized and unsignalized intersections is defined in **Table 1**.

Table 1 - Level of Service Definition

Level of Service	Signalized Intersection Average Total Delay (sec/veh)	Unsignalized Intersection Average Total Delay (sec/veh)
A	≤10	≤10
B	>10 and ≤20	>10 and ≤15
C	>20 and ≤35	>15 and ≤25
D	>35 and ≤55	>25 and ≤35
E	>55 and ≤80	>35 and ≤50
F	>80	>50

Definitions sourced from the Highway Capacity Manual, 7th Edition, Transportation Research Board

Synchro 12 Analysis and Optimization Software was used to analyze study area intersections for level of service (LOS) and total delay. A LOS of D or better is typically considered acceptable.

Scenario: 2025 Existing

Operational analysis results for the existing weekday AM and PM peak hour traffic volumes for 2025 are shown in **Table 2**. Study area intersections all operate at acceptable LOS in each peak hour evaluated in this analysis, apart from the intersection of 1300 South and 300 West, which operates at a LOS F in the PM peak hour.

Excessive queueing is defined as queues that extend beyond storage lengths or flow into adjacent intersections. The only movement to experience excessive queueing in this scenario is the southbound through/right movement at the intersection of 300 West and 1300 South.

Table 2 - 2025 Existing Scenario Operational Analysis

Intersection	2025 Existing (AM)		2025 Existing (PM)	
	Delay (LOS)	Queue (ft)	Delay (LOS)	Queue (ft)
1300 South / 400 West Two-Way Stop Control	Worst Movement- Westbound Left 10.2 (LOS B)	50 (Southbound Right)	Worst Movement Westbound Left 13.7 (LOS B)	72 (Southbound Right)
1300 South / 300 West Signalized	22.3 (LOS C)	223 (Eastbound Through)	103.1 (LOS F)	1233 (Southbound Through/Right)
1300 South / West Temple Signalized	12.4 (LOS B)	270 (Eastbound Through/Right)	12.1 (LOS B)	239 (Eastbound Through/Right)
1300 South / Main Street Signalized	12.7 (LOS B)	171 (Eastbound Through/Right)	16.8 (LOS B)	224 (Northbound Through/Right)
1300 South / State Street Signalized	23.1 (LOS C)	307 (Eastbound Through/Right)	33.5 (LOS C)	543 (Eastbound Through/Right)

2025 Existing Travel Time on 1300 S

Travel time data was recorded for each day in the month of April 2025. Travel time was recorded from 7 am to 7 pm each weekday.

The data shows that the free flow travel time during off-peak hours, is approximately 2 minutes and 18 seconds. During the AM peak hour, the average increases to 3 minutes 15 seconds, and during the PM peak period the average increases to 4 minutes 55 seconds.

During the month of April, travel times of over 8 minutes 30 seconds were observed, as illustrated in **Figure 1**.

Based on the data, segment travel time exceeded 5 minutes (as compared to free flow of just over 2 minutes) on 20 of 21 counting days, primarily during the PM peak period.

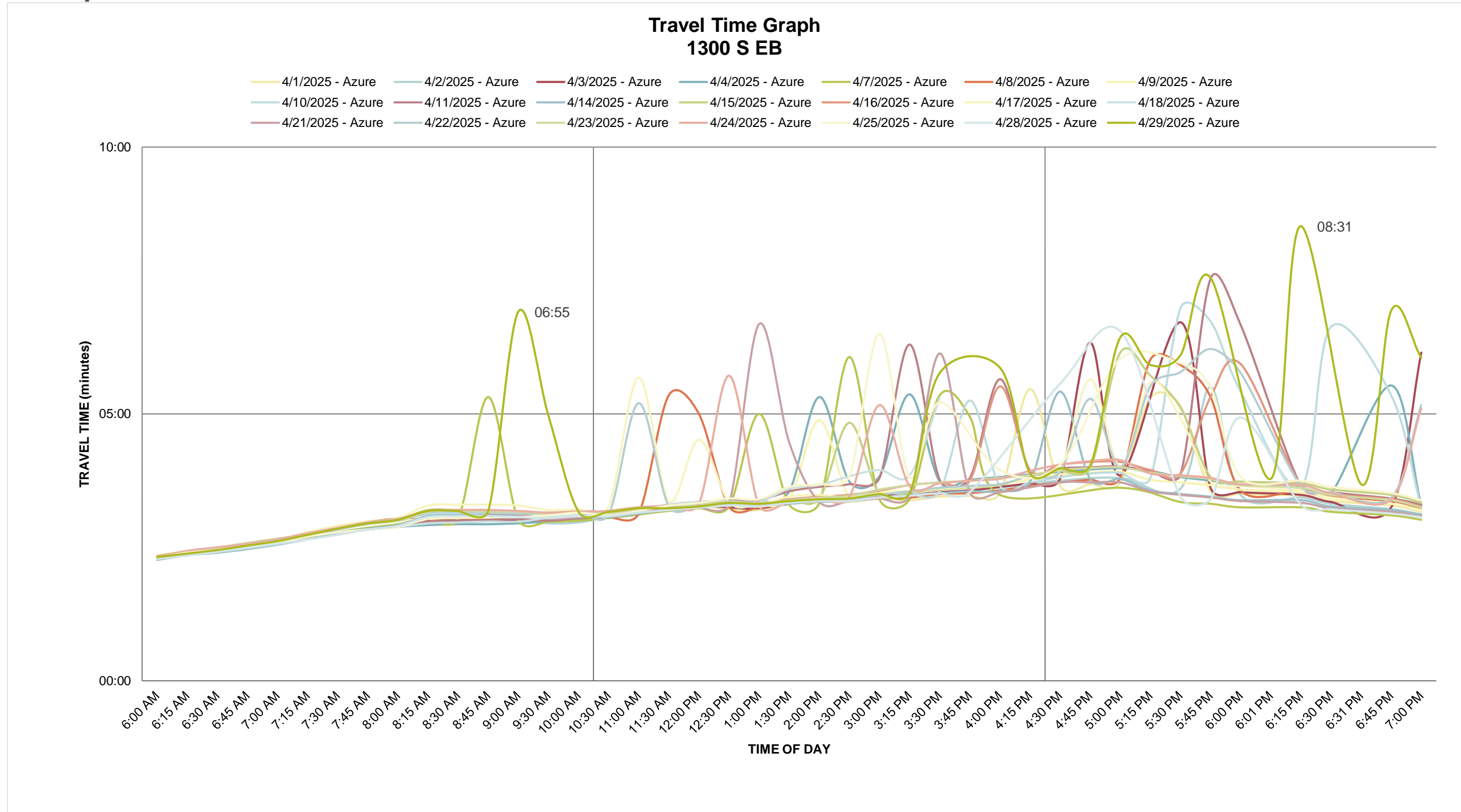


Figure 1 – Travel Time on 1300 South, I-15 to State Street, April 2025

Scenario: 2025 Existing Volumes with Lane Reduction

Operational analysis results for the weekday AM and PM peak hours in the 2025 existing volumes with lane reduction scenario are shown in **Table 3**. The lane reduction scenario of 1300 South would reduce the facility to a three-lane roadway with single lanes in each travel direction and a center turn lane that alternates between a dedicated left turn lane and a two-way left turn lane beginning at 300 South and ending at State Street. The roadway would flare out at Main Street and State Street to allow for dedicated right turn lanes.

Table 3 - 2025 Existing Volumes with Lane Reduction Scenario Operational Analysis

Intersection	2025 Lane Reduction AM		2025 Lane Reduction PM	
	Delay (LOS)	Queue (ft)	Delay (LOS)	Queue (ft)
1300 South / 400 West Two-Way Stop Control	Worst Movement Westbound Left 13.5 (LOS B)	50 (Southbound Right)	Worst Movement Northbound Right 96.8 (LOS F)	400 (Eastbound Through)
1300 South / 300 West Signalized	26.7 (LOS C)	471 (Eastbound Through)	124.4 (LOS F)	850 (Eastbound Through)
1300 South / West Temple Signalized	27.3 (LOS C)	777 (Eastbound Through/Right)	25.6 (LOS C)	712 (Eastbound Through/Right)
1300 South / Main Street Signalized	16.4 (LOS B)	284 (Eastbound Through)	31.5 (LOS C)	713 (Eastbound Through)
1300 South / State Street Signalized	26.0 (LOS C)	463 (Eastbound Through)	43.5 (LOS D)	833 (Eastbound Through)

The study area intersections all operate at acceptable LOS in each peak hour evaluated in this analysis, apart from the intersection of 1300 South and 300 West, which operates at a LOS F in the PM peak hour, and the intersection of 1300 South and 400 West which operates at a LOS F in the PM peak hour.

Several study intersections experience excessive queues in this scenario, with queues exceeding turn lane storage lengths or backing up into adjacent intersections. The following movements experience excess queuing:

- Eastbound through at the intersection of 400 West and 1300 South
- Eastbound through/right at the intersection of 300 West and 1300 South
- Southbound through/right at the intersection of 300 West and 1300 South
- Westbound through/right at the intersection of West Temple and 1300 South
- Eastbound through at the intersection of Main Street and 1300 South
- Eastbound left/through/right at the intersection of State Street and 1300 South

CONCLUSIONS

1300 South is vital for traffic flow to and from I-15. Reducing it to three lanes would cause congestion and delays. The traffic analysis shows vehicle queues would exceed storage lengths at intersections on 400 West, 300 West, West Temple, Main Street, and State Street in the lane reduction scenario. Critically, queues are expected to extend west of 400 W into the I-15 / 1300 South interchange area, potentially affecting its efficiency.

While a lane reduction would offer benefits to the BallPark Next redevelopment by providing wider pedestrian facilities on 1300 South and reducing street crossing distances, it would also lead to decreased efficiency for commuters due to reduced capacity at critical intersections. Considering the vital role of 1300 South as an arterial road connecting to I-15, and the proven negative impacts on traffic operations, it is essential to maintain the existing lane configuration to ensure mobility, safety, and overall effectiveness of the interchange area.

ATTACHMENTS

SimTraffic Performance Reports

- PM Peak Period (no changes)
- PM Peak Period (with lane reduction)

PM Peak Period (no changes)

2: 400 West & 1300 South Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBR	SBR	All
Denied Delay (hr)	0.1	0.2	0.0	0.0	0.0	0.0	0.0	0.3
Denied Del/Veh (s)	0.5	2.5	0.1	0.0	0.0	0.2	0.2	0.5
Total Delay (hr)	0.1	0.1	0.1	0.7	0.0	0.3	0.3	1.6
Total Del/Veh (s)	0.5	0.9	13.7	2.3	2.6	6.8	9.7	2.1
Vehicles Entered	1035	255	39	1053	18	140	98	2638
Vehicles Exited	1035	256	39	1051	18	140	98	2637
Hourly Exit Rate	1035	256	39	1051	18	140	98	2637
Input Volume	1044	248	42	1082	19	139	95	2668
% of Volume	99	103	93	97	96	101	103	99

3: 300 West & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.3	0.0	0.0	8.5	32.8	9.9
Denied Del/Veh (s)	0.0	0.0	0.0	0.3	0.0	0.1	2.9	0.3	0.3	257.9	258.6	262.7
Total Delay (hr)	1.6	4.7	0.3	1.2	6.8	1.0	3.9	4.9	0.9	12.9	51.5	15.2
Total Del/Veh (s)	27.7	21.5	5.9	28.9	35.4	33.8	43.6	38.6	29.1	450.0	460.2	459.8
Vehicles Entered	199	772	204	149	679	105	317	455	109	96	373	110
Vehicles Exited	198	773	204	149	681	106	316	454	109	90	352	102
Hourly Exit Rate	198	773	204	149	681	106	316	454	109	90	352	102
Input Volume	200	778	205	152	681	105	317	458	107	120	454	134
% of Volume	99	99	100	98	100	101	100	99	102	75	77	76

3: 300 West & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	51.5
Denied Del/Veh (s)	50.1
Total Delay (hr)	104.9
Total Del/Veh (s)	103.1
Vehicles Entered	3568
Vehicles Exited	3534
Hourly Exit Rate	3534
Input Volume	3711
% of Volume	95

4: West Temple & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	3.6	0.2	0.2	3.9	0.3	0.3
Total Delay (hr)	0.1	2.3	0.1	0.1	2.0	0.1	0.4	0.3	0.1	0.6	0.4	0.2
Total Del/Veh (s)	14.3	10.4	9.2	13.8	8.8	7.0	42.2	33.4	12.0	40.5	37.5	13.9
Vehicles Entered	37	785	58	26	807	37	37	27	30	53	35	44
Vehicles Exited	37	784	58	26	807	38	37	28	30	53	34	44
Hourly Exit Rate	37	784	58	26	807	38	37	28	30	53	34	44
Input Volume	39	821	57	25	811	39	36	26	30	55	36	44
% of Volume	95	96	102	105	100	97	103	109	100	97	94	99

4: West Temple & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.1
Denied Del/Veh (s)	0.2
Total Delay (hr)	6.7
Total Del/Veh (s)	12.1
Vehicles Entered	1976
Vehicles Exited	1976
Hourly Exit Rate	1976
Input Volume	2018
% of Volume	98

5: Main Street & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.1	0.1	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	3.5	0.8	3.6	3.7	0.6	3.7
Total Delay (hr)	0.2	1.4	0.1	0.3	2.1	0.1	1.8	2.0	0.2	0.9	1.5	0.2
Total Del/Veh (s)	16.2	6.7	3.3	25.6	11.1	9.8	52.1	40.1	9.1	50.2	38.6	7.6
Vehicles Entered	50	743	73	36	672	44	123	180	92	65	140	98
Vehicles Exited	51	744	73	36	672	44	123	180	92	65	139	98
Hourly Exit Rate	51	744	73	36	672	44	123	180	92	65	139	98
Input Volume	55	778	73	39	672	45	126	178	93	64	142	98
% of Volume	92	96	100	93	100	98	98	101	99	101	98	100

5: Main Street & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.4
Denied Del/Veh (s)	0.7
Total Delay (hr)	10.9
Total Del/Veh (s)	16.8
Vehicles Entered	2316
Vehicles Exited	2317
Hourly Exit Rate	2317
Input Volume	2362
% of Volume	98

6: State Street & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	2.6	2.3	0.2	2.2
Total Delay (hr)	1.8	8.2	1.2	1.1	4.2	0.1	2.1	5.9	0.1	1.2	11.9	0.2
Total Del/Veh (s)	58.5	54.9	17.5	41.9	34.6	3.7	43.1	27.5	2.8	31.7	34.2	5.8
Vehicles Entered	108	523	233	90	431	78	176	760	95	130	1238	146
Vehicles Exited	108	524	235	90	431	78	176	761	95	130	1238	146
Hourly Exit Rate	108	524	235	90	431	78	176	761	95	130	1238	146
Input Volume	113	543	240	89	436	77	176	755	93	133	1223	144
% of Volume	96	97	98	101	99	101	100	101	102	98	101	101

6: State Street & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.3
Denied Del/Veh (s)	0.3
Total Delay (hr)	37.9
Total Del/Veh (s)	33.5
Vehicles Entered	4008
Vehicles Exited	4012
Hourly Exit Rate	4012
Input Volume	4023
% of Volume	100

7: 1300 South Performance by movement

Movement	EBT	WBT	All
Denied Delay (hr)	0.0	0.0	0.0
Denied Del/Veh (s)	0.0	0.0	0.0
Total Delay (hr)	3.0	2.9	5.9
Total Del/Veh (s)	12.3	11.6	11.9
Vehicles Entered	880	889	1769
Vehicles Exited	879	888	1767
Hourly Exit Rate	879	888	1767
Input Volume	915	892	1807
% of Volume	96	100	98

Total Network Performance

Denied Delay (hr)	52.8
Denied Del/Veh (s)	26.2
Total Delay (hr)	171.9
Total Del/Veh (s)	83.8
Vehicles Entered	7127
Vehicles Exited	7091
Hourly Exit Rate	7091
Input Volume	24958
% of Volume	28

Queuing and Blocking Report
Baseline

02/26/2025

Intersection: 2: 400 West & 1300 South

Movement	EB	EB	EB	WB	WB	WB	NB	SB
Directions Served	T	T	R	L	T	TR	R	R
Maximum Queue (ft)	5	12	24	74	3	4	91	88
Average Queue (ft)	0	1	2	24	0	0	37	42
95th Queue (ft)	5	10	12	58	4	2	68	72
Link Distance (ft)	380	380			700	700	693	529
Upstream Blk Time (%)								
Queuing Penalty (veh)								
Storage Bay Dist (ft)			70	145				
Storage Blk Time (%)		0						
Queuing Penalty (veh)		0						

Intersection: 3: 300 West & 1300 South

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	NB	SB
Directions Served	L	T	T	R	L	T	TR	L	L	T	TR	L
Maximum Queue (ft)	210	336	324	225	250	444	468	197	233	251	263	124
Average Queue (ft)	112	166	159	64	109	214	231	94	140	149	156	41
95th Queue (ft)	201	274	261	161	231	376	400	185	207	231	243	92
Link Distance (ft)		700	700			731	731			1094	1094	
Upstream Blk Time (%)							0					
Queuing Penalty (veh)							0					
Storage Bay Dist (ft)	150			140	150			230	230			100
Storage Blk Time (%)	2	10	13	0	1	23		0	0	1		3
Queuing Penalty (veh)	7	21	27	0	5	36		0	1	2		7

Intersection: 3: 300 West & 1300 South

Movement	SB	SB	SB
Directions Served	L	T	TR
Maximum Queue (ft)	195	1082	1088
Average Queue (ft)	151	996	986
95th Queue (ft)	267	1221	1233
Link Distance (ft)		1038	1038
Upstream Blk Time (%)		71	61
Queuing Penalty (veh)		0	0
Storage Bay Dist (ft)	100		
Storage Blk Time (%)	8	87	
Queuing Penalty (veh)	18	103	

Queuing and Blocking Report
Baseline

02/26/2025

Intersection: 4: West Temple & 1300 South

Movement	EB	EB	EB	WB	WB	WB	NB	NB	SB	SB
Directions Served	L	T	TR	L	T	TR	L	TR	L	TR
Maximum Queue (ft)	103	309	333	68	229	234	79	98	104	119
Average Queue (ft)	24	79	97	17	98	112	33	37	42	47
95th Queue (ft)	72	211	239	50	191	208	70	80	87	92
Link Distance (ft)		709	709		716	716		1083		584
Upstream Blk Time (%)										
Queuing Penalty (veh)										
Storage Bay Dist (ft)	95			105			95		95	
Storage Blk Time (%)	0	6			5		0	1	1	2
Queuing Penalty (veh)	0	2			1		0	0	1	1

Intersection: 5: Main Street & 1300 South

Movement	EB	EB	EB	EB	WB	WB	WB	NB	NB	NB	SB	SB
Directions Served	L	T	T	R	L	T	TR	L	T	R	L	T
Maximum Queue (ft)	86	233	219	60	94	214	227	191	287	150	118	200
Average Queue (ft)	23	64	51	8	22	82	95	85	119	41	51	90
95th Queue (ft)	59	155	140	34	63	169	189	158	224	111	99	161
Link Distance (ft)		716	716			677	677		714			564
Upstream Blk Time (%)												
Queuing Penalty (veh)												
Storage Bay Dist (ft)	170			170	103			105		75	100	
Storage Blk Time (%)	0	1	0		0	5		8	21	0	2	12
Queuing Penalty (veh)	0	0	0		0	2		22	47	1	4	19

Intersection: 5: Main Street & 1300 South

Movement	SB
Directions Served	R
Maximum Queue (ft)	146
Average Queue (ft)	38
95th Queue (ft)	85
Link Distance (ft)	
Upstream Blk Time (%)	
Queuing Penalty (veh)	
Storage Bay Dist (ft)	100
Storage Blk Time (%)	0
Queuing Penalty (veh)	0

Queuing and Blocking Report
Baseline

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Intersection: 6: State Street & 1300 South

Movement	EB	EB	EB	EB	WB	WB	WB	WB	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	T	R	L	T	T	T
Maximum Queue (ft)	200	598	573	190	177	227	217	82	217	319	281	194
Average Queue (ft)	129	309	240	104	63	130	118	22	100	201	156	58
95th Queue (ft)	253	543	515	224	123	197	193	58	179	290	253	164
Link Distance (ft)		677	677			302	302			680	680	680
Upstream Blk Time (%)		0	0			0						
Queuing Penalty (veh)		1	1			0						
Storage Bay Dist (ft)	105			105	125			125	890			
Storage Blk Time (%)	4	52	7	1	1	11	8					0
Queuing Penalty (veh)	9	58	16	4	2	10	6					0

Intersection: 6: State Street & 1300 South

Movement	NB	SB	SB	SB	SB	SB
Directions Served	R	L	T	T	T	R
Maximum Queue (ft)	52	309	453	405	296	106
Average Queue (ft)	17	79	298	253	168	30
95th Queue (ft)	38	197	416	370	280	73
Link Distance (ft)			641	641	641	
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	220	280				210
Storage Blk Time (%)		0	11		1	
Queuing Penalty (veh)		0	15		2	

Intersection: 7: 1300 South

Movement	EB	EB	WB	WB
Directions Served	T	T	T	T
Maximum Queue (ft)	374	391	364	375
Average Queue (ft)	83	94	82	89
95th Queue (ft)	275	296	261	277
Link Distance (ft)	731	731	709	709
Upstream Blk Time (%)				
Queuing Penalty (veh)				
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Queuing and Blocking Report

Baseline

02/26/2025

Intersection: 18: Bend

Movement	EB
Directions Served	T
Maximum Queue (ft)	64
Average Queue (ft)	3
95th Queue (ft)	44
Link Distance (ft)	302
Upstream Blk Time (%)	0
Queuing Penalty (veh)	0
Storage Bay Dist (ft)	
Storage Blk Time (%)	
Queuing Penalty (veh)	

Network Summary

Network wide Queuing Penalty: 452

PM Peak Period (with lane reduction)

2: 400 West & 1300 South Performance by movement

Movement	EBT	EBR	WBL	WBT	WBR	NBR	SBR	All
Denied Delay (hr)	2.3	0.7	0.0	0.0	0.0	0.4	0.0	3.4
Denied Del/Veh (s)	7.9	9.6	0.0	0.0	0.0	11.3	0.2	4.6
Total Delay (hr)	5.3	0.4	0.2	0.7	0.0	3.7	0.3	10.5
Total Del/Veh (s)	18.4	5.5	18.3	2.3	2.4	96.8	10.0	14.3
Vehicles Entered	1032	251	42	1056	17	134	96	2628
Vehicles Exited	1028	251	41	1055	17	127	96	2615
Hourly Exit Rate	1028	251	41	1055	17	127	96	2615
Input Volume	1044	248	42	1082	19	139	95	2668
% of Volume	98	101	98	97	91	91	101	98

3: 300 West & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.1	0.0	0.3	0.0	0.0	10.6	38.9	11.7
Denied Del/Veh (s)	0.0	0.0	0.0	0.2	0.4	0.6	2.9	0.3	0.4	322.6	310.2	303.6
Total Delay (hr)	5.9	18.4	2.2	1.8	5.8	0.8	4.0	5.2	0.9	12.6	51.8	15.7
Total Del/Veh (s)	105.2	85.3	38.4	41.9	30.3	26.4	43.7	40.3	30.8	473.0	492.3	492.1
Vehicles Entered	195	758	201	147	682	107	323	461	104	90	352	106
Vehicles Exited	190	748	199	146	683	107	322	460	104	82	328	99
Hourly Exit Rate	190	748	199	146	683	107	322	460	104	82	328	99
Input Volume	200	778	205	152	681	105	317	458	107	120	454	134
% of Volume	95	96	97	96	100	102	102	100	97	68	72	74

3: 300 West & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	61.6
Denied Del/Veh (s)	60.1
Total Delay (hr)	125.2
Total Del/Veh (s)	124.4
Vehicles Entered	3526
Vehicles Exited	3468
Hourly Exit Rate	3468
Input Volume	3711
% of Volume	93

4: West Temple & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.0
Denied Del/Veh (s)	0.2	0.2	0.0	0.0	0.1	0.0	3.7	0.2	0.2	3.9	0.3	0.3
Total Delay (hr)	0.4	6.5	0.4	0.2	4.1	0.2	0.4	0.3	0.2	0.7	0.4	0.3
Total Del/Veh (s)	36.9	30.5	28.2	29.4	18.1	15.8	42.9	37.2	21.8	43.1	38.7	22.3
Vehicles Entered	37	757	55	25	809	38	35	25	30	54	37	44
Vehicles Exited	37	752	54	25	809	38	35	25	30	53	37	45
Hourly Exit Rate	37	752	54	25	809	38	35	25	30	53	37	45
Input Volume	39	821	57	25	811	39	36	26	30	55	36	44
% of Volume	95	92	95	101	100	97	97	97	100	97	103	102

4: West Temple & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.2
Denied Del/Veh (s)	0.3
Total Delay (hr)	14.0
Total Del/Veh (s)	25.6
Vehicles Entered	1946
Vehicles Exited	1940
Hourly Exit Rate	1940
Input Volume	2018
% of Volume	96

5: Main Street & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.0	0.1	0.1	0.0	0.1
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	3.5	0.8	3.6	3.7	0.6	3.7
Total Delay (hr)	0.6	7.0	0.3	0.5	4.3	0.3	1.8	1.9	0.6	0.9	1.5	0.4
Total Del/Veh (s)	43.7	34.8	17.6	49.4	23.1	20.8	52.4	38.2	24.3	50.1	38.2	13.8
Vehicles Entered	53	714	69	37	668	43	124	173	94	61	142	102
Vehicles Exited	52	712	69	37	670	43	124	172	94	61	141	102
Hourly Exit Rate	52	712	69	37	670	43	124	172	94	61	141	102
Input Volume	55	778	73	39	672	45	126	178	93	64	142	98
% of Volume	94	92	95	95	100	96	99	97	101	95	99	104

5: Main Street & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.4
Denied Del/Veh (s)	0.7
Total Delay (hr)	20.2
Total Del/Veh (s)	31.5
Vehicles Entered	2280
Vehicles Exited	2277
Hourly Exit Rate	2277
Input Volume	2362
% of Volume	96

6: State Street & 1300 South Performance by movement

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Denied Delay (hr)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.1	0.1	0.1	0.1
Denied Del/Veh (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.2	0.1	2.6	2.3	0.2	2.2
Total Delay (hr)	2.6	9.3	2.6	1.1	9.3	0.2	2.1	6.2	0.3	1.3	12.6	0.7
Total Del/Veh (s)	88.8	65.0	41.3	43.4	75.8	8.8	42.8	29.4	11.3	35.7	36.7	16.1
Vehicles Entered	103	502	225	89	434	72	174	750	89	131	1219	148
Vehicles Exited	103	501	225	89	427	72	174	751	90	131	1221	148
Hourly Exit Rate	103	501	225	89	427	72	174	751	90	131	1221	148
Input Volume	113	543	240	89	436	77	176	755	93	133	1223	144
% of Volume	91	92	94	100	98	94	99	99	97	98	100	103

6: State Street & 1300 South Performance by movement

Movement	All
Denied Delay (hr)	0.3
Denied Del/Veh (s)	0.3
Total Delay (hr)	48.3
Total Del/Veh (s)	43.5
Vehicles Entered	3936
Vehicles Exited	3932
Hourly Exit Rate	3932
Input Volume	4023
% of Volume	98

7: 1300 South Performance by movement

Movement	EBT	WBT	All
Denied Delay (hr)	0.0	0.0	0.0
Denied Del/Veh (s)	0.1	0.0	0.0
Total Delay (hr)	3.9	4.2	8.1
Total Del/Veh (s)	16.5	17.0	16.7
Vehicles Entered	849	890	1739
Vehicles Exited	847	889	1736
Hourly Exit Rate	847	889	1736
Input Volume	915	892	1807
% of Volume	93	100	96

Total Network Performance

Denied Delay (hr)	66.2
Denied Del/Veh (s)	33.0
Total Delay (hr)	232.9
Total Del/Veh (s)	114.3
Vehicles Entered	7052
Vehicles Exited	6964
Hourly Exit Rate	6964
Input Volume	26760
% of Volume	26

Queuing and Blocking Report
Baseline

03/05/2025

Intersection: 2: 400 West & 1300 South

Movement	EB	EB	EB	WB	WB	WB	NB	SB
Directions Served	T	T	R	L	T	TR	R	R
Maximum Queue (ft)	313	288	150	67	7	3	338	88
Average Queue (ft)	124	103	38	26	0	0	115	41
95th Queue (ft)	400	365	161	60	6	4	395	71
Link Distance (ft)	380	380			700	700	693	529
Upstream Blk Time (%)	11	8					3	
Queuing Penalty (veh)	0	0					0	
Storage Bay Dist (ft)			70	145				
Storage Blk Time (%)		11	0					
Queuing Penalty (veh)		29	0					

Intersection: 3: 300 West & 1300 South

Movement	EB	EB	EB	EB	WB	WB	WB	B13	NB	NB	NB	NB
Directions Served	L	T	T	R	L	T	TR	T	L	L	T	TR
Maximum Queue (ft)	210	727	722	225	249	323	324	315	222	251	293	300
Average Queue (ft)	192	540	493	156	117	203	204	36	100	140	153	163
95th Queue (ft)	262	850	868	301	230	333	333	187	194	216	242	255
Link Distance (ft)		700	700			249	249	426			1094	1094
Upstream Blk Time (%)		17	11		0	4	5	0				
Queuing Penalty (veh)		100	64		0	19	25	1				
Storage Bay Dist (ft)	150			140	150				230	230		
Storage Blk Time (%)	7	68	20	0	4	23			0	1	1	
Queuing Penalty (veh)	23	136	42	2	15	34			0	1	5	

Intersection: 3: 300 West & 1300 South

Movement	SB	SB	SB	SB
Directions Served	L	L	T	TR
Maximum Queue (ft)	142	195	1082	1081
Average Queue (ft)	80	135	991	984
95th Queue (ft)	143	280	1227	1236
Link Distance (ft)			1038	1038
Upstream Blk Time (%)			72	64
Queuing Penalty (veh)			0	0
Storage Bay Dist (ft)	100	100		
Storage Blk Time (%)	23	0	87	
Queuing Penalty (veh)	53	0	104	

Queuing and Blocking Report
Baseline

03/05/2025

Intersection: 4: West Temple & 1300 South

Movement	EB	EB	WB	WB	NB	NB	SB	SB
Directions Served	L	TR	L	TR	L	TR	L	TR
Maximum Queue (ft)	184	690	149	572	82	101	109	141
Average Queue (ft)	42	360	26	269	31	39	43	55
95th Queue (ft)	135	712	91	508	69	84	90	113
Link Distance (ft)		710		716		1095		596
Upstream Blk Time (%)		1		0				
Queuing Penalty (veh)		13		1				
Storage Bay Dist (ft)	95		105		95		95	
Storage Blk Time (%)	0	31		21	0	1	2	3
Queuing Penalty (veh)	1	12		5	0	0	1	2

Intersection: 5: Main Street & 1300 South

Movement	EB	EB	EB	WB	WB	NB	NB	NB	SB	SB	SB
Directions Served	L	T	R	L	TR	L	T	R	L	T	R
Maximum Queue (ft)	225	705	243	192	500	187	270	146	128	188	139
Average Queue (ft)	65	315	64	45	297	87	111	49	49	91	45
95th Queue (ft)	196	713	237	141	460	157	207	118	100	161	92
Link Distance (ft)		716			675		726			576	
Upstream Blk Time (%)		2			0						
Queuing Penalty (veh)		20			0						
Storage Bay Dist (ft)	170		170	103		105		75	100		100
Storage Blk Time (%)		27		0	26	8	19	3	1	12	1
Queuing Penalty (veh)		35		0	10	23	42	9	4	19	1

Queuing and Blocking Report
Baseline

03/05/2025

Intersection: 6: State Street & 1300 South

Movement	EB	EB	EB	WB	WB	WB	WB	B18	NB	NB	NB	NB
Directions Served	L	T	R	L	T	T	R	T	L	T	T	T
Maximum Queue (ft)	200	691	190	198	295	292	186	89	217	311	273	182
Average Queue (ft)	106	545	143	91	186	174	52	28	98	209	164	59
95th Queue (ft)	215	833	254	207	318	311	182	190	171	291	256	164
Link Distance (ft)		675			302	302		425		692	692	692
Upstream Blk Time (%)		6			6	5		2				
Queuing Penalty (veh)		56			0	0		0				
Storage Bay Dist (ft)	105		105	125			125		890			
Storage Blk Time (%)	9	53	2	1	40	37						0
Queuing Penalty (veh)	68	188	12	2	35	29						0

Intersection: 6: State Street & 1300 South

Movement	NB	SB	SB	SB	SB	SB
Directions Served	R	L	T	T	T	R
Maximum Queue (ft)	87	268	429	395	284	134
Average Queue (ft)	25	74	300	260	176	38
95th Queue (ft)	62	181	397	357	264	97
Link Distance (ft)			641	641	641	
Upstream Blk Time (%)						
Queuing Penalty (veh)						
Storage Bay Dist (ft)	220	280				210
Storage Blk Time (%)		0	10		1	0
Queuing Penalty (veh)		0	14		1	0

Intersection: 7: 1300 South

Movement	EB	B13	B13	WB
Directions Served	T	T		T
Maximum Queue (ft)	508	260	47	712
Average Queue (ft)	241	56	3	266
95th Queue (ft)	604	211	43	681
Link Distance (ft)	426	249	249	710
Upstream Blk Time (%)	9	1	0	1
Queuing Penalty (veh)	80	3	1	5
Storage Bay Dist (ft)				
Storage Blk Time (%)				
Queuing Penalty (veh)				

Network Summary

Network wide Queuing Penalty: 1344

Appendix F:
Creek Daylighting Study



MEMORANDUM

To: Marc Asnis
Perkins & Will

From: Emily Palumbo, P.E.
Kimley-Horn and Associates, Inc.

Date: March 3, 2025

Subject: Ballpark Next Development: Creek Daylighting Study

Background

The Ballpark Next Urban Design project seeks to incorporate the Three Creeks waterway into the proposed development. The purpose of this study is to document the information gathered on the feasibility of partially daylighting the Three Creeks through the proposed development.

Data Collection

The Three Creeks waterway is the confluence of Parley's Creek, Emigration Creek, and Red Butte Creek. It is currently conveyed under 1300 South by a 6-foot by 10-foot culvert with a 60-inch diameter overflow pipe. This system discharges into the Jordan River approximately one mile to the west at the Three Creeks Confluence Park.

The existing waterway through the project area appears on FEMA FIRM Panel 49035C0282H, the FIRMette for this area is included in the Appendix. This panel shows that the surface area to the north of 1300 South is in a FEMA Zone AH. The waterway and its upstream tributaries are discussed in Flood Insurance Study Number 49035CV001D. This study states the 10-year discharge in the 1300 South system is 825 cubic feet per second (cfs).

On January 13, 2025, the Kimley Horn team walked the 1300 South corridor to investigate existing conditions. The team observed a bolted down stormwater vault at the intersection of 1300 South and State Street that is assumed to tie into the 1300 South box culvert, though this could not be verified because the structure could not be opened. Nearby, the team opened an existing stormwater manhole that ties into the 1300 South system and measured it to be approximately eight feet deep. This manhole appears to tie into the existing box culvert and was flowing steadily at the time of the site visit.

Google Earth elevation data was used to approximate elevations across the project corridor. At the intersection of 1300 South and State Street, the elevation was observed to be approximately 4,240 feet. The Three Creeks confluence park is where the 1300 South system discharges into the Jordan River and is approximately 7,900 feet west along 1300 South. The road elevation at the park is

approximately 4,230 feet, and the roadway slope along 1300 South is approximately 0.1%. These features and elevations are annotated in Figure 1 in the Appendix.

Stakeholder Coordination

As part of the research process, the design team met with Salt Lake City Public Utilities and Salt Lake County Flood Control to discuss the project and gather more information. Key takeaways from these conversations are summarized below.

Salt Lake City Public Utilities

The design team met with Jason Draper from Salt Lake City Public Utilities on January 28th, 2025. Jason is the chief engineer for SLC Public Utilities and the local floodplain administrator.

The team discussed the existing culvert and the surrounding areas. The existing culvert has been observed to convey urban runoff, but fish have also been observed at this location. This is likely due to the proximity to the outfall location into the Jordan River, which can backflow into the 1300 South culvert. The existing culvert is considered to be undersized during high flow events. Additionally, flooding issues are prevalent in the surrounding neighborhoods, mainly Paxton Avenue and Lucy Avenue.

The team discussed potential challenges with daylighting the 1300 South culvert through the project site, these include not violating any existing water rights, staying within existing Total Maximum Daily Load (TMDL) requirements, and avoiding triggering the FEMA regulatory process.

The team also discussed SLC Public Utilities' riparian buffer zone requirements and determined that these requirements will likely not apply to this project as the existing culvert does not provide any riparian benefits to be protected.

Overall, Jason felt positively about the potential of daylighting the Three Creeks culvert. He suggested that water rights and FEMA issues could be avoided if the daylight was added back in to the 1300 South culvert. He also was interested in the possibility of providing some water quantity relief to the existing culvert during high flow events, providing water quality features, and providing educational opportunities to the community.

Meeting minutes from this conversation are included in the Appendix.

Salt Lake County Flood Control

The design team met with Shane Ellis (County Director), Aaron Murphy (Flood Control Permit Review Specialist), and Bob Thompson (Watershed Section Manager) on February 24th, 2025.

The team discussed the existing culvert and how it may impact the proposed daylight. It was mentioned that during low flow conditions, the culvert flows as low as one cubic foot per second (cfs), and during high flow events, the culvert is under capacity. The seasonal variability of flow could present challenges to the functionality of the daylight and the surrounding areas. It was also mentioned that the Jordan River is under a TMDL requirement for E. Coli, and so any potential introduction of E. Coli to the waterway will need to be mitigated.

The team discussed the proposed daylight of the Three Creeks and the associated implications. Firstly, any alteration to the culvert would require a permit through SLC Flood Control. If more than 300 feet of the culvert is altered, a Stream Alteration Permit through the Army Corps of Engineers may be required. If the daylighting system requires any maintenance, SLC Flood Control likely will not have the capacity to provide maintenance. A maintenance agreement is recommended to establish standard procedures and who is responsible.

Concept

Based on the information gathered from coordinating with stakeholders and doing high level investigations, an initial concept layout has been developed and is shown in Figure 2 in the Appendix. Due to the depth and size of the existing 1300 South system, a partial daylight with custom flow and geometry is more feasible than a full daylight of the system. To achieve this, a 24-inch pipe or similar can tie in to the existing manhole at the 1300 South and State Street intersection and flow under the sidewalk on the north side of 1300 South to the proposed daylight location. This proposed pipe would tie in to the manhole as shallow under the road as possible, and travel at a slope up to 0.5% to minimize the depth of the discharge location relative to the ground surface.

The proposed pipe would discharge into a proposed daylight channel. This channel will convey flow across the southern boundary of the project for 300 feet or less and will re-enter the 1300 South system just east of West Temple Street. The channel can vary in geometry, material, and other parameters. Figure 3 below shows a sample daylight channel cross section, assuming a manning’s roughness coefficient of 0.040 to represent a vegetated channel, three-to-one side slopes, 1.5 feet of water depth, and a five-foot channel bottom.

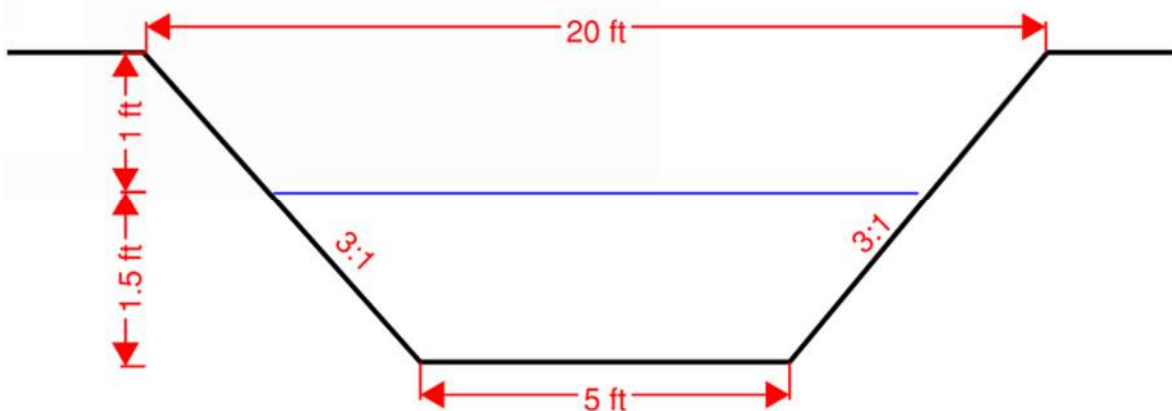


Figure 3: Sample Channel Cross Section

This proposed concept plan would necessitate the design of the diversion structure to remove flow from the existing 1300 South culvert and a control structure at the downstream end of the channel. As the existing culvert flows full during some periods of the year, this diversion system must be designed

with the ability to be sealed off from the 1300 South system so that the new infrastructure does not flood during high flow events. Along with the diversion system, the proposed pipe and channel will need to be designed further than what is presented in this initial study.

Conclusion

It is feasible to incorporate a partial daylight of the Three Creeks waterway in the Ballpark Next Development. This daylight will include the design and installation of a diversion structure to remove water from the existing 1300 South system at the intersection with State Street, a diversion pipe along 1300 South to the project site, the daylight channel, and a control structure to re-enter the 1300 South system.

There are a number of challenges to this partial daylight concept that may be encountered during design, including diverting water from the 1300 South system during high flow events, complying with local water rights, ensuring site discharge meets existing TMDL requirements, and triggering any FEMA processes.

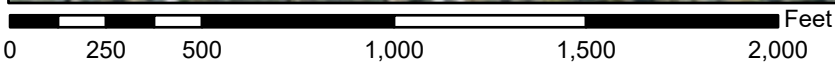
Appendix

- FEMA FIRMette
- Daylighting Requirements Meeting Minutes
- Figure 1: 1300 South System
- Figure 2: Concept Plan

National Flood Hazard Layer FIRMMette



111°53'52"W 40°44'44"N



1:6,000

111°53'14"W 40°44'16"N

Basemap Imagery Source: USGS National Map 2023

Legend

SEE FIS REPORT FOR DETAILED LEGEND AND INDEX MAP FOR FIRM PANEL LAYOUT

SPECIAL FLOOD HAZARD AREAS		Without Base Flood Elevation (BFE) Zone A, V, A99
		With BFE or Depth Zone AE, AO, AH, VE, AR
		Regulatory Floodway
OTHER AREAS OF FLOOD HAZARD		0.2% Annual Chance Flood Hazard, Areas of 1% annual chance flood with average depth less than one foot or with drainage areas of less than one square mile Zone X
		Future Conditions 1% Annual Chance Flood Hazard Zone X
		Area with Reduced Flood Risk due to Levee. See Notes. Zone X
		Area with Flood Risk due to Levee Zone D
OTHER AREAS		NO SCREEN Area of Minimal Flood Hazard Zone X
		Effective LOMRs
GENERAL STRUCTURES		Area of Undetermined Flood Hazard Zone D
		Channel, Culvert, or Storm Sewer
		Levee, Dike, or Floodwall
OTHER FEATURES		20.2 Cross Sections with 1% Annual Chance
		17.5 Water Surface Elevation
		Coastal Transect
		Base Flood Elevation Line (BFE)
		Limit of Study
		Jurisdiction Boundary
MAP PANELS		Coastal Transect Baseline
		Profile Baseline
		Hydrographic Feature
		Digital Data Available
		No Digital Data Available
		Unmapped



The pin displayed on the map is an approximate point selected by the user and does not represent an authoritative property location.

This map complies with FEMA's standards for the use of digital flood maps if it is not void as described below. The basemap shown complies with FEMA's basemap accuracy standards

The flood hazard information is derived directly from the authoritative NFHL web services provided by FEMA. This map was exported on **11/11/2024 at 8:21 PM** and does not reflect changes or amendments subsequent to this date and time. The NFHL and effective information may change or become superseded by new data over time.

This map image is void if the one or more of the following map elements do not appear: basemap imagery, flood zone labels, legend, scale bar, map creation date, community identifiers, FIRM panel number, and FIRM effective date. Map images for unmapped and unmodernized areas cannot be used for regulatory purposes.

Ballpark NEXT – Daylighting Requirements Meeting

January 28, 2024

Attendees

- Perkins & Will: Marc Asnis, Geeti Silwal, Dishaddra Poddar, Sharvari Rajee
- RDA: Lauren Parisi
- Kimley Horn: Derrick Turner, Emily Palumbo, Leslie Morton
- SLC Public Utilities: Jason Draper

Meeting Notes

- Layout/Feasibility
 - Existing Culvert/Waterways
 - Culvert conveys debris, mostly urban runoff from storm drain systems that tie into it (NOT large debris from streams)
 - Have observed fish in culvert
 - Emigration Creek may have E. Coli issues. Not currently under a TMDL, but there have been issues in the past.
 - Jordan River
 - Has TMDLs for temperature and DO.
 - Sometimes backs up into culvert during high flow.
 - Proposed Daylight
 - Jason had no issues with currently shown layout.
 - Daylight discharge options:
 - Back into the existing culvert would be the best way to avoid issues with regulatory agencies
 - Discharge water into storm drain system that flows north into Jefferson Park detention facility
 - This would help with capacity issues on the existing culvert
 - Could run into water rights issues, Jason will ask internally for opinions on this
 - Potential alternative water sources
 - Many basements in the neighborhood flood regularly, may be able to pull from the groundwater and reduce stress on nearby homeowners.
 - Use culinary water, not ideal but can provide the desired feature we are aiming for.
 - Will need to work with SLC Flood Control
 - Needs a flood control permit since the water source is under their jurisdiction.
 - Misc. Concerns
 - Paxton Ave and Lucy Ave flood regularly

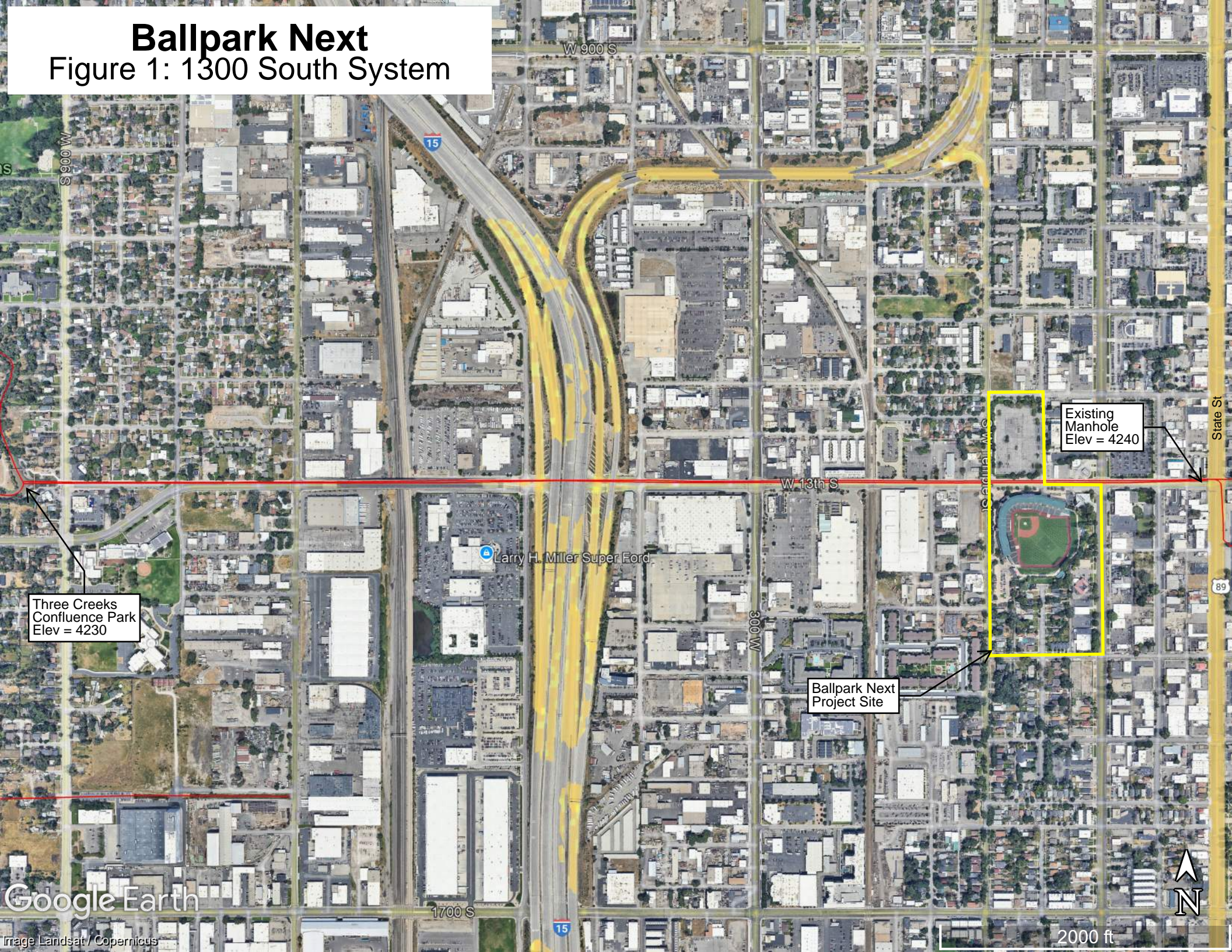
- Running into issues with water rights when taking water out of culvert
 - Jason suggested ways that we can sell the daylighting to stakeholders despite risks/concerns:
 - Providing some water quality educational opportunities
 - Providing some mitigation to existing flooding issues
 - Providing additional stormwater detention volume
- Riparian Buffer Zone
 - Requirement is a part of a Zoning Ordinance within City Code
 - City Code 21A
 - Jason does not believe that this area would apply to the riparian buffer zone requirements. The requirement is intended to maintain and protect existing riparian buffers & the existing pipe does not have a riparian zone
 - Jason to coordinate with Kristeen on this interpretation
 - City Creek daylight project was not subject to riparian buffer zone requirements
- FEMA
 - Jason is the SLC Local Floodplain Administrator
 - FEMA Zone AH which includes some intentional flooding/ponding during the 1% and 0.2% annual chance storm events
 - Don't want this project to create additional floodplains
 - The design should avoid using a pump at the upstream tie in with the culvert – this would create issues with FEMA

Action Items

1. Jason Draper to coordinate with Kristeen Beitel on riparian buffer zone requirements & whether or not they would apply to this project.
2. Jason Draper to look into water rights & how they would potentially apply to a proposed daylighting.
3. Kimley Horn/Perkins & Will to set up a meeting with SLC Flood Control and to include SLC Public Utilities if possible.

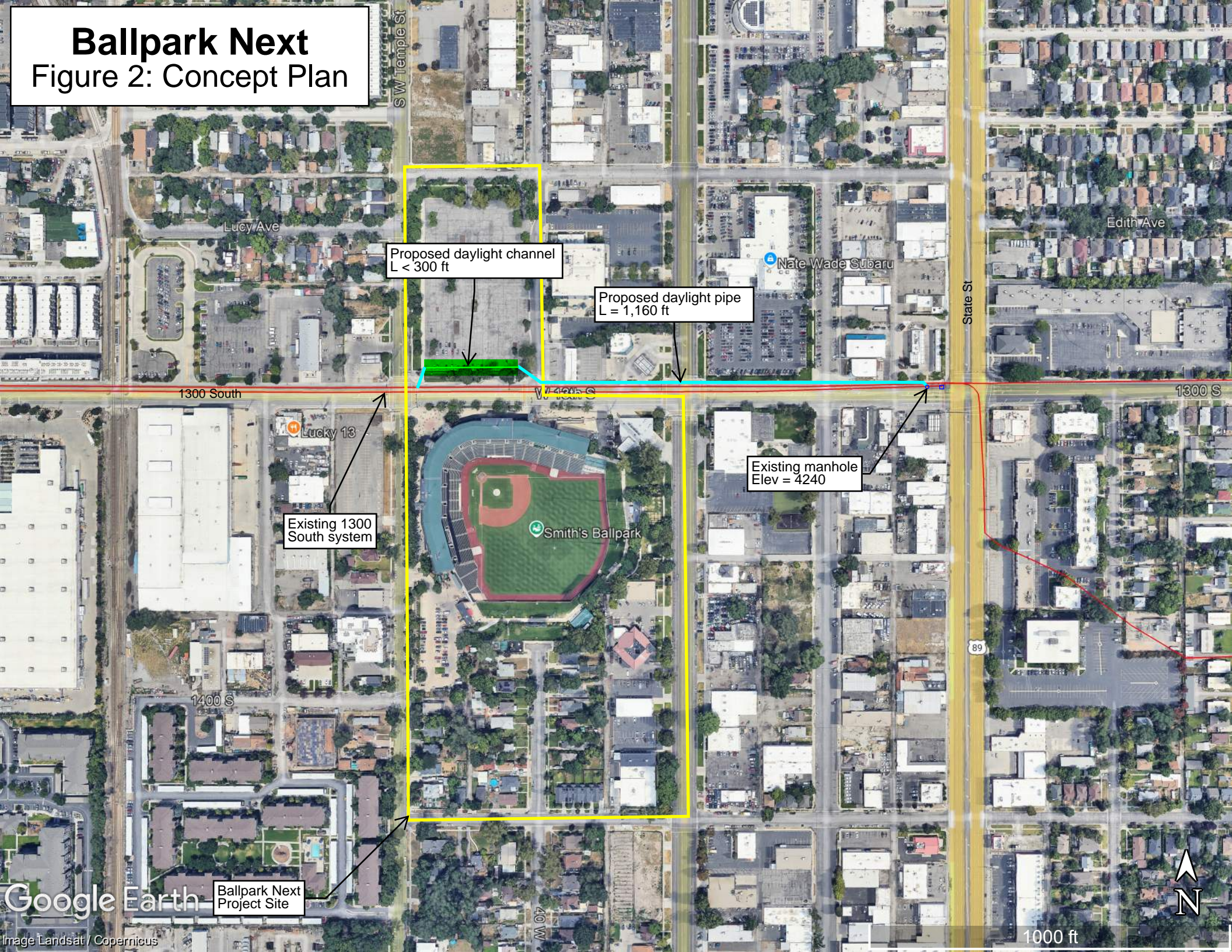
Ballpark Next

Figure 1: 1300 South System



Ballpark Next

Figure 2: Concept Plan



Proposed daylight channel
L < 300 ft

Proposed daylight pipe
L = 1,160 ft

Existing manhole
Elev = 4240

Existing 1300 South system

Ballpark Next Project Site

Google Earth

Image Landsat / Copernicus

1000 ft



Appendix G:

Proposed Design Transportation and Civil Evaluation



MEMORANDUM

To: Marc Asnis
From: Brent Crowther, PE; Leslie Morton, PE
Kimley-Horn and Associates, Inc.
Date: March 13, 2025
Subject: Transportation and Civil Evaluation

Introduction

Salt Lake City is moving forward with the redevelopment of the 13.5-acre Ballpark Site at the corner of West Temple and 1300 South. The plan will balance historic preservation with new development, incorporating partial reuse of the stadium while creating a vibrant, mixed-use destination centered on arts, culture, and community gathering spaces.

The purpose of this memorandum is to provide information and analysis in fulfillment of Kimley-Horn scope of work Task 4.3, Transportation and Civil Evaluation, which reads:

Kimley Horn will analyze and test the test fit on the potential traffic implications. Kimley Horn will recommend improvements/strategies to enhance multimodal mobility and connectivity. Recommendations will consider:

- *Trip generation*
- *Parking requirements*
- *Mode shift goals*
- *Transit connectivity*
- *Multi-modal connectivity, infrastructure and recommendations*
- *Vehicle access*
- *Required on-site utility concepts*

Scenario 2 – Partial Adaptive Reuse

Scenario 2, Partial Adaptive Reuse, was identified as the preferred scenario. The land use concept for Scenario 2 is described as:

Scenario 2's focal point is the long central flexible green space, being a draw for night markets, concerts, little league, and picnics, all the way to afternoon reading with a view of the mountains. The western portion of the stadium has been preserved and revitalized to bring people together through dynamic activations and social engagements.

Land Use Assessment

TRIP GENERATION

The development area encompasses the existing ballpark and the existing parking lot north of the ballpark. Scenario 2 concept layout is illustrated in **Figure 1**. Proposed land uses in Scenario 2 include residential, hospitality, retail, commercial, and public facilities (e.g., library). Scenario 2 land uses were used to estimate the number of vehicle trips that would be generated by the development.

Scenario 2 is divided into two blocks: a north block, and a south block. The north block extends from 1300 South to Paxton Avenue. The south block extends from 1300 South to 1400 South. Land uses for each block are listed in **Table 1**.

Commented [MA1]: How do we define public facilities?
Library and Event Venue?

Commented [BC2R1]: Library.



Figure 1 - 1300 South Ballpark Concept Layout

Table 1 – Ballpark Next Scenario 2 Proposed Land Uses

Block	Location	Building	Land Use	Size
1	Paxton Avenue to 1300 South	A	RESIDENTIAL	98 (DU)
			PARKING	40,000
		B	PARKING	162,000
			RESIDENTIAL	118 (DU)
		C	GROCERY STORE	22,000
			RESTAURANT	10,000
2	1300 South to 1400 South	D	RESIDENTIAL	91 (DU)
			HOTEL	140 (ROOMS)
			ACTIVE USES	31,000
		E	SENIOR HOUSING	63 (DU)
			LIBRARY	34,000
		F	SPECIALTY RETAIL	14,000
			SPORTS & EVENT UTILITY	19,000
		G	PARKING	148,000
		H	FIRE STATION	25,000
		I	RESIDENTIAL	23 (DU)
			PARKING	6,000
		J	RESIDENTIAL	36 (DU)
			PARKING	5,000
		K	RESIDENTIAL	35 (DU)
			PARKING	19,000
		L	RESIDENTIAL	13 (DU)
PARKING	5,000			

The Institute of Transportation Engineers' (ITE) Trip Generation Manual, 11th Edition provides daily and peak hour trip generation rates and inbound-outbound percentages, which were then used to estimate the number of daily and peak hour trips that can be attributed to the proposed development.

As documented in the ITE Trip Generation Manual, 11th Edition, the restaurant and retail land uses do not typically generate all new traffic on the roadway system. The total traffic generation is a combination of pass-by trips, or traffic drawn directly from the already passing traffic flow on adjacent streets, and primary trips, which represent new traffic drawn to the development. Additionally, some internal trips between the land uses are expected.

Trip generation estimates remove pass-by trips and internal trips from the total new trips generated by the development to provide a more accurate trip assignment to the roadway network. A transit reduction was also applied to the trip generation as the development is located less than a quarter mile from the TRAX Ballpark Station. Data for the transit reduction was estimated using the Replica mode split application.

The trip generation characteristics of the site are summarized in **Table 2**. Scenario 2 will generate an estimated 6,006 daily external new trips, with 310 total external new trips occurring in the AM peak hour and 486 total external new trips occurring in the PM peak hour.

Table 2 - 1300 South Ballpark Proposed Trip Generation

Land Use	Size	Units	Daily Trips ¹	AM Peak			PM Peak		
				Total	In	Out	Total	In	Out
RESIDENTIAL	414	Dwelling Unit(s)	1,993	133	32	101	122	74	48
SENIOR HOUSING	63	Dwelling Unit(s)	204	13	4	9	16	9	7
HOTEL	140	Room(s)	1,119	64	36	28	83	42	41
GROCERY STORE	22	1,000 Sq Ft	2,064	63	37	26	197	98	99
RESTAURANT	10	1,000 Sq Ft	1,072	96	53	43	91	55	36
RETAIL	14	1,000 Sq Ft	762	33	20	13	92	46	46
SPORTS AND EVENT UTILITY	19	1,000 Sq Ft	273	32	26	6	41	14	27
LIBRARY	34	1,000 Sq Ft	2,450	34	24	10	277	133	144
FIRE STATION	25	1,000 Sq Ft	120	0	0	0	12	3	9
GRAND TOTAL			10,057	468	232	236	931	474	457
Internal Capture (Office)			-98	-12	-7	-5	-14	-7	-7
Internal Capture (Retail)			-783	-17	-10	-7	-109	-43	-66
Internal Capture (Restaurant)			-531	-34	-22	-12	-58	-30	-28
Internal Capture (Residential)			-670	-16	-3	-13	-69	-44	-25
Internal Capture (Entertainment)			-93	0	0	0	-21	-9	-12
Internal Capture (Hotel)			-176	-7	-1	-6	-17	-11	-6
Total Internal Capture			-2,351	-86	-43	-43	-288	-144	-144
Retail Pass-by Reduction			-179	0	0	0	-30	-17	-13
Restaurant Pass-by Reduction			-116	0	0	0	-14	-11	-3
Total External Trips			7,411	382	189	193	599	302	297
Transit Reduction (19%)			-1,405	-72	-35	-37	-113	-57	-56
Total External Vehicle Trips			6,006	310	154	156	486	245	241

Source: ITE Trip Generation, 11th Edition

PARKING ANALYSIS

A parking evaluation was conducted to determine if the supply of proposed shared parking will be sufficient to address the parking demand for the proposed development.

The parking evaluation found that there is sufficient parking in both zones to satisfy the developments parking needs for typical day-to-day conditions.

Parking Supply

Table 3 presents the number of parking spaces by zone. Scenario 2 proposes 964 spaces between the north and south project areas. Parking needs for the fire station were disregarded in this analysis as their parking would not be shared with the rest of the development.

Table 3 - 1300 South Ballpark Project Parking Inventory Summary

Block	Description	Sub Area	Parking Stalls
1	Paxton Avenue to 1300 South	Residential Spaces	100
		Parking Garage North	405
2	1300 South to 1400 South	Parking Garage South	370
		Residential Spaces	89
Total			964

Parking Requirements

Section 21A.44.040 of the Salt Lake City Code of Ordinances establishes minimum requirements for off-street parking. **As this development falls under the transit context, there are no minimum parking requirements.**

Shared Parking Analysis Methodology

The shared parking analysis utilized the Urban Land Institute (ULI) *Shared Parking Calculation Model*, which calculates shared parking demand based on information and methodology in ULI's reference publication *Shared Parking, 3rd Edition*. This reference estimates shared parking based on land use, as well as collected data on parking fluctuation based on month and time of day for each land use. The shared parking analysis considered two scenarios: 1) Typical conditions with no event and 2) Event conditions.

Typical Conditions (No Event) Shared Parking Analysis Results

The ULI Shared Parking model provides monthly and hourly peak parking demand for both weekday and weekend. **Figure 2** presents the monthly weekday parking demand for the AM, PM, and evening peak periods for typical conditions. **Figure 3** presents the monthly weekend parking demand during the AM, PM, and evening peak periods for typical conditions.

The average weekday and weekend parking demand for the typical conditions is 711 and 661, respectively. The maximum parking demand for typical conditions occurs during the May peak, with a weekday and weekend demand of 729 and 677, respectively. Both average and peak demands can be accommodated by the proposed 964 spaces.

Commented [MA3]: Does the shared parking analysis take into consideration an event/concert? If its a +/- 6,000 person capacity - how would the shared parking accommodate?

Commented [BC4R3]: Added a new section for event traffic. Note that event traffic will exceed parking supply. Emphasis on transit, as well as nearby on-street parking.

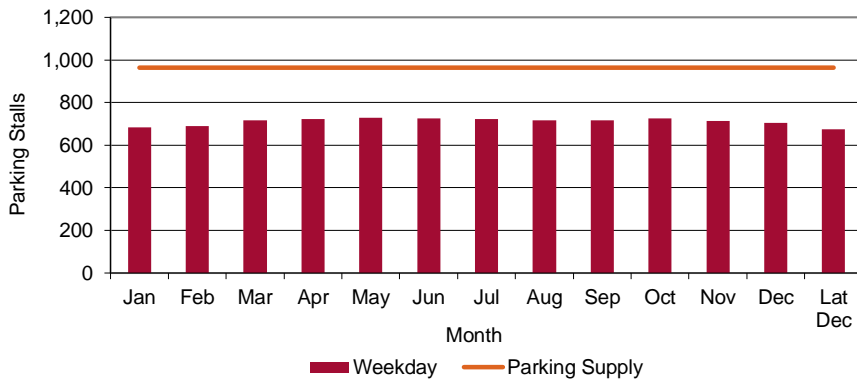


Figure 2 – Typical Conditions Monthly Weekday Peak Parking Demand

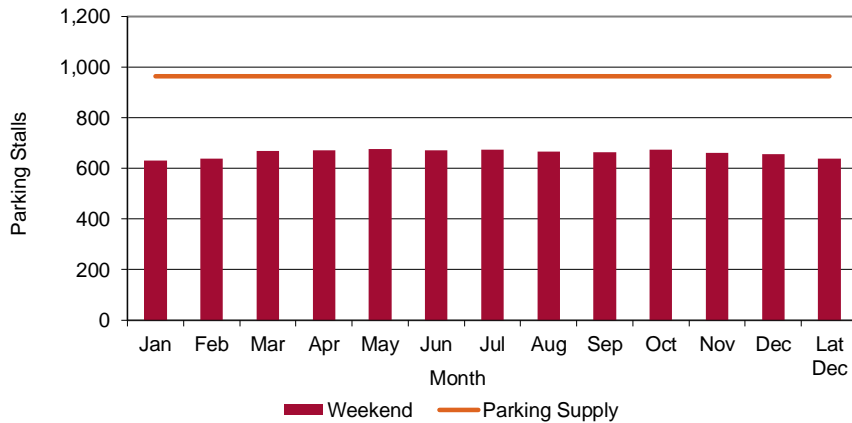


Figure 3 – Typical Conditions Monthly Weekend Peak Parking Demand

Figure 4 Error! Reference source not found. and Figure 5 illustrate the May peak demand at an hourly interval. Other months will also have a similar hourly distribution. During the weekday, higher parking demand occurs between 12:00 PM and 7:00 PM where demand related to office and commercial uses are in operation. Similarly, during the weekend, higher parking demand occurs between 12:00 PM and 8:00 PM. Outside these hours, the majority of parking demand is associated with the residential uses.

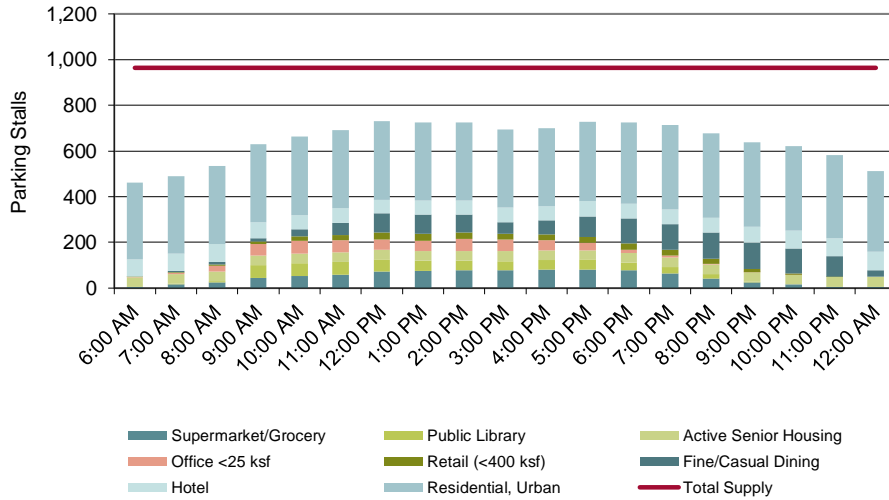


Figure 4 - Typical Conditions Peak Month Weekday Daily Parking Demand by Hour

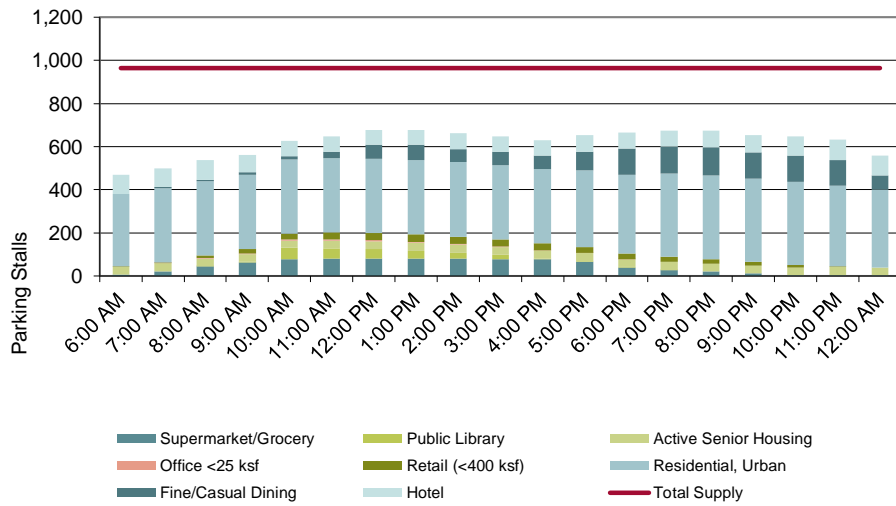


Figure 5 - Typical Conditions Peak Month Weekend Daily Parking Demand by Hour

Typical Conditions (No Event) Zonal Parking Analysis

While the overall site will accommodate the total parking demand, additional evaluation was conducted to determine if there was adequate parking for each zone during the peak month of May. **Table 4** presents the weekday and weekend results. The analysis shows that Block 1 and Block 2 each have sufficient parking supply to meet demand, with a larger surplus in Block 1 than in Block 2.

Table 4 – Typical Conditions Parking Demand And Supply by Block

		Weekday			Weekend		
		Block 1	Block 2	Total	Block 1	Block 2	Total
Parking Demand	Customer / Visitor	130	118	248	124	118	242
	Employee / Resident	31	88	119	29	49	78
	Reserved	175	160	335	175	160	335
	Total	336	366	702	328	327	655
Parking Supply	Customer / Visitor	405	370	775	405	370	775
	Employee / Resident	100	89	189	100	89	189
	Reserved	-	-	-	-	-	-
	Total	505	459	964	505	459	964
Surplus(+)/ Deficit(-)	Customer / Visitor	275	252	527	281	252	533
	Employee / Resident	59	1	70	71	40	111
	Reserved	-175	-160	-335	-175	-160	-335
	Total	169	93	262	177	132	309

Event Conditions Shared Parking Analysis Results

Event conditions added the parking demand for a 5,000-seat arena, which increased the average weekday and weekend parking demand to 1,562 and 1,641, respectively. The maximum parking demand for Event conditions is 1,857 during the weekday and 1,974 on the weekend. Both the average and maximum demand exceed the proposed supply of 964 spaces.

Figure 6 and Error! Reference source not found. illustrate the May peak demand on at hourly interval. Other months will also have a similar hourly distribution. Under Event condition, the demand exceeds supply starting approximately a hour before an event begins and resends an hour after an event.

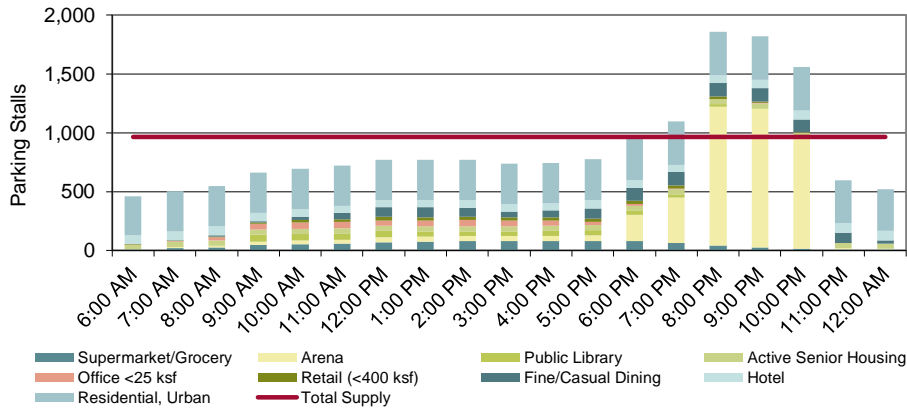


Figure 6 – Event Conditions Peak Month Weekday Daily Parking Demand by Hour

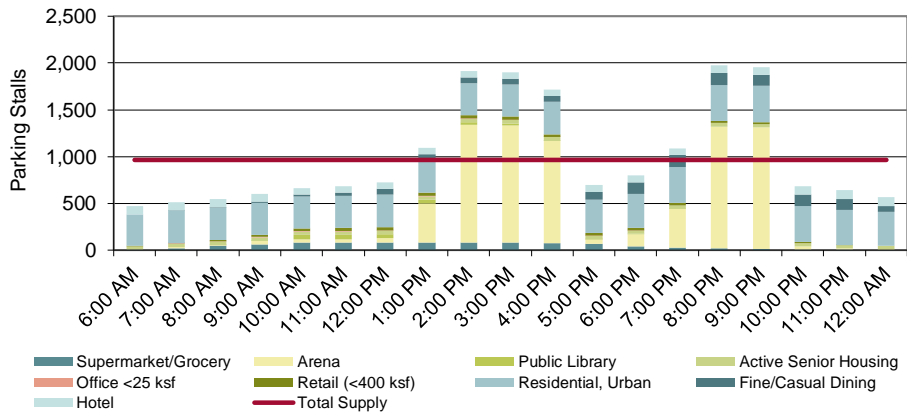


Figure 7 – Event Conditions Peak Month Weekend Daily Parking Demand by Hour

Event Conditions Zonal Parking Analysis

Additional evaluation was conducted to determine if there was adequate parking for each zone during the peak month of May. **Table 5** presents the weekday and weekend results for the event scenario. The analysis shows that Block 1 and Block 2 each have sufficient parking supply to meet demand, with Block 3 having insufficient parking supply to meet demand.

Block 3 represents the parking demand generated by an event. There is a paid parking lot to the west of the site with approximately 140 parking spaces, but no street parking within a ¼ mile radius of the project site.

The current ballpark management encourages attendees to use the TRAX system to get to the venue, with the nearest station being less than ¼ mile away.

Attendees will rely on nearby on-street parking to match the parking demand in both the weekday and weekend time periods, as well as an additional emphasis on utilization of transit (TRAX).

Table 5 – Event Weekday and Weekend Peak Parking Demand And Supply by Block

		Weekday				Weekend			
		Block 1	Block 2	Block 3	Total	Block 1	Block 2	Block 3	Total
Parking Demand	Customer / Visitor	149	113	1060	1322	153	114	1180	1447
	Employee / Resident	24	28	121	173	24	24	121	169
	Reserved	175	160	-	335	175	160	-	335
	Total	348	301	1181	1830	352	298	1301	1951
Parking Supply	Customer / Visitor	405	370	-	775	405	370	-	775
	Employee / Resident	100	89	-	189	100	89	-	189
	Reserved	-	-	140	-	-	-	140	-
	Total	505	459	140	1104	505	469	140	1104
Surplus(+)/ Deficit(-)	Customer / Visitor	256	257	-1060	-547	252	256	-1180	-672
	Employee / Resident	76	61	-121	16	76	65	-121	20
	Reserved	-175	-160	140	-195	-175	-160	140	-195
	Total	157	158	-1041	-726	153	161	-1161	-847

TRAFFIC OPERATIONS ANALYSIS

Turning movement count data were collected on Tuesday, February 11, 2025, for the following study area intersections:

- I-15 Northbound Ramps and 1300 South
- 400 West and 1300 South
- 300 West and 1300 South
- West Temple and 1300 South
- Main Street and 1300 South
- State Street and 1300 South

Study area intersections were analyzed based on average total delay for signalized and unsignalized intersections as presented in the Transportation Research Board’s Highway Capacity Manual, 7th Edition (HCM 7). Under the unsignalized analysis, the level of service (LOS) for a two-way stop controlled (TWSC) intersection is determined by the computed or measured control delay and is defined for each minor movement. LOS for a two-way stop-controlled intersection is not defined for the intersection. LOS for signalized and unsignalized intersections is defined in **Table 6**.

Table 6 - Level of Service Definition

Level of Service	Signalized Intersection Average Total Delay (sec/veh)	Unsignalized Intersection Average Total Delay (sec/veh)
A	≤10	≤10
B	>10 and ≤20	>10 and ≤15
C	>20 and ≤35	>15 and ≤25
D	>35 and ≤55	>25 and ≤35
E	>55 and ≤80	>35 and ≤50
F	>80	>50

Definitions sourced from the Highway Capacity Manual, 7th Edition, Transportation Research Board

Synchro 12 Analysis and Optimization Software was used to analyze study area intersections for level of service (LOS) and total delay. A LOS of D or better is typically considered acceptable.

Scenario: 2025 Existing

Operational analysis results for the 2025 existing weekday AM and PM peak hour traffic volumes are shown in **Table 7**.

Table 7 - 2025 Existing Scenario Operational Analysis

Intersection	2025 Existing (AM)		2025 Existing (PM)	
	Delay (LOS)	Queue (ft)	Delay (LOS)	Queue (ft)
1300 South / 400 West Two-Way Stop Control	Worst Movement- Westbound left 10.2 (LOS B)	50 (Southbound Right)	Worst Movement Westbound left 13.7 (LOS B)	72 (Southbound Right)
1300 South / 300 West Signalized	22.3 (LOS C)	223 (Eastbound Through)	103.1 (LOS F)	1233 (Southbound Through/Right)
1300 South / West Temple Signalized	12.4 (LOS B)	270 (Eastbound Through/Right)	12.1 (LOS B)	239 (Eastbound Through/Right)
1300 South / Main Street Signalized	12.7 (LOS B)	171 (Eastbound Through/Right)	16.8 (LOS B)	224 (Northbound Through Right)
1300 South / State Street Signalized	23.1 (LOS C)	307 (Eastbound Through/Right)	33.5 (LOS C)	543 (Eastbound Through/Right)

Study area intersections all operate at acceptable LOS in each peak hour evaluated in this analysis, apart from the intersection of 1300 South and 300 West, which operates at a LOS F in the PM peak hour.

Scenario: 2025 With Scenario 2 Development

Operational analysis results for the 2025 with the Scenario 2 development weekday AM and PM peak hour traffic volumes are shown in **Table 8**. The study area intersections all operate at acceptable LOS in each peak hour evaluated in this analysis, apart from the intersection of 1300 South and 300 West, which operates at a LOS F in the PM peak hour.

Table 8 - 2025 Plus Project Scenario Operational Analysis

Intersection	2025 Plus Project AM		2025 Plus Project PM	
	Delay (LOS)	Queue (ft)	Delay (LOS)	Queue (ft)
1300 South / 400 West Two-Way Stop Control	Worst Movement – Westbound Left 13.2 (LOS B)	50 (Southbound Right)	Worst Movement – Westbound Left 18.6 (LOS C)	88 (Northbound Right)
1300 South / 300 West Signalized	22.7 (LOS C)	276 (Westbound Through/Right)	100.3 (LOS F)	1216 (Southbound Through/Right)
1300 South / West Temple Signalized	14.3 (LOS B)	359 (Eastbound Through/Right)	15.2 (LOS B)	319 (Eastbound Through)
1300 South / Main Street Signalized	7.9 (LOS A)	198 (Eastbound Through)	25.0 (LOS C)	517 (Northbound Through)
1300 South / State Street Signalized	24.1 (LOS C)	327 (Eastbound Through)	40.7 (LOS D)	725 (Eastbound Through)

TRANSPORTATION CHARACTERISTICS

Mode Shift Goals

Modal shift goals aim to encourage a change in how people travel by reducing reliance on single-occupancy vehicles (SOVs) and increasing trips via public transit, active transportation, and micromobility options. The project is within the Ballpark Station Transit-Oriented Development (TOD) zone, benefiting from direct access to TRAX light rail service (Red, Blue, and Green lines) and multiple bus routes, including Route 200 on State Street and Route 9 on 300 West/1300 South.

A key transportation objective of this redevelopment is to reduce single-occupancy vehicle trips and promote multimodal transportation for residents, visitors, and employees. To achieve this, improvements should focus on transit service enhancements, pedestrian and bicycle infrastructure, micromobility options, and efficient curbside management for rideshare and carshare users.

Enhance Public Transit Access

Reducing single-occupancy vehicle trips requires good public transit options, including increased service frequency, reliability, and connectivity. The Ballpark area already benefits from TRAX service running every 7-10 minutes during peak hours. Coordination with Utah Transit Authority (UTA) should continue especially in relation to future special events at the site. Potential strategies to improve transit access include:

- Improved transit stops: ensuring easy and accessible station access, adding shelters and other small-scale station amenities (e.g., real-time arrival information) to existing and future station or integrating future transit stations along 1300 South with the proposed development, and good street lighting.
- On-demand service area expansion: expand UTA's on-demand service area to include the development site to better connect Westside residents to the neighborhood.
- Fare integration: encourage fare program partnerships between UTA and development partners to offer free or reduced transit passes to residents and employees.

Expand Active Transportation Infrastructure

To further encourage non-motorized travel, the Scenario 2 will prioritize pedestrian and bicycle connectivity. Enhancing infrastructure along key corridors in the neighborhood will improve safety, accessibility, and overall usability. Key recommendations include:

- Wide sidewalks and shared-use paths: Implement pedestrian-friendly design features to support pedestrian traffic and accommodate strollers, wheelchairs, and other users.
- Safe bicycle routes: While 1300 South is currently not identified to have dedicated bike lanes, Salt Lake City is planning to include new neighborhood byways on Kelsey and Paxton Avenues which will also include improved crossings at major intersections. Additionally, Salt Lake City has long-term plans to study bicycle infrastructure on 1300 South.
- Secure bike parking: Installing bike racks and lockers at key locations, including near transit stations, commercial areas, and residential buildings, will support cycling as a viable option.

Micromobility & Car/Ridesharing

Another strategy to encourage the switch from single occupancy vehicle trips to active uses can be achieved by increasing the availability of bikeshare programs and electric scooters as well as expanding car sharing programs in the area. Micromobility and shared transportation services provide convenient alternatives to private vehicle use, particularly for short trips.

GREENbike, a local non-profit agency, operates a bikeshare program in Salt Lake City and Ogden. Most docking stations are located in downtown Salt Lake City and there is no docking station close to the site. The city also currently contracts with *Spin* and *Lime* to offer e-scooters and both companies have vehicles available in the Ballpark neighborhood. Key recommendations for improved micro mobility:

- Bikeshare expansion: Adding docking stations near the redevelopment site would increase the viability of bike share programs as a mode of transportation.
- E-scooter parking and management: Establishing dedicated e-scooter parking areas can streamline operations and mitigate issues related to vehicles obstructing the sidewalk.

Ridesharing has become a popular choice of transportation for individuals without a personal vehicle and those not wanting to drive and navigate traffic congestion and parking at event sites. Meanwhile, carsharing programs offer short-term vehicle access as an alternative to owning a personal vehicle. These programs can be commercial or peer-to-peer. Key recommendations related to ride/carsharing:

- Rideshare zones: The concept design for 1300 South includes a central pick-up/drop-off area. Additional rideshare zones on West Temple and Main Street will be considered to improve traffic flow and reduce congestion during peak hours.

Transit Connectivity

The Ballpark area benefits from existing high-frequency transit service, including TRAX and bus routes. Ensuring safe and convenient connections to these services is critical for meeting the project’s modal shift goals and encouraging more transit use.

Existing transit services:

- TRAX light rail: The Ballpark Station serves the Red, Blue, and Green lines, providing direct access to the airport, downtown Salt Lake City, and the University of Utah, as well as regional destinations.
- Bus routes:
 - Route 200 (State Street) runs on 15-minute frequency and connects transit users between the North Temple FrontRunner Station to the Murray Central FrontRunner Station.
 - Route 9 (900 South/300 West) operates on 15-minute frequency and provides service between the University of Utah and the Orange Street bus terminus just west of Redwood Road.

Based on UTA’s 5-year strategic plan and the region’s long-range transportation plan future service expansion will further enhance mobility in the Ballpark neighborhood. Future service expansions include:

- UTA 5-year Strategic Service Plan (by 2029)
 - Route 17: Expanded service between Highberry Center in West Valley City and the University of Utah, passing the Ballpark on 1300 South and Main Street before turning onto 1700 South. This route is planned to operate on regular 15-30-minute service and will be adjusted in 2028.
- Long-Range Transportation Plan:

- Route 3 Core Service (300 West): High-frequency (10-minute) bus service between FrontRunner North Temple Station and Central Pointe TRAX Station (Phase 1: 2023-2032)
- State Street BRT: Upgraded bus rapid transit (BRT) service replacing Route 200 (Phase 2: 2033-2042)
- TRAX Red Line Extension: Extending TRAX service via the American Spur on 400 West, connecting to 400 South at 400 West (Phase 2: 2033-2042).

Multi-modal Connectivity

The site benefits from its proximity to multiple transit services and an existing urban fabric that supports multimodal access. Ensuring seamless connections between transportation modes—such as safe and convenient first/last-mile access to transit, improved pedestrian and bicycle infrastructure, and expanded micromobility options—will be critical to the project's success.

Currently, the pedestrian experience from the Ballpark TRAX Station to the development site is poor. Pedestrians must walk on a 10-foot-wide attached sidewalk along 1300 South without a buffer, making it an unpleasant and potentially unsafe experience due to the road's traffic volumes and vehicle speeds. Enhancing pedestrian-focused urban design elements, such as wider sidewalks, landscaping buffers, and improved crossings, can significantly improve the walking experience for residents and visitors. A concept exhibit is included as **Figure 8**. Key improvements for the area's multimodal connectivity should include:

- New pedestrian crossing: The Ballpark Station Area Plan recommends adding pedestrian crossings on both sides of the TRAX tracks to improve station access, connectivity, and safety.
- Active transportation corridors: streetscape enhancements along Jefferson Street and Albermarle Avenue, including a new midblock crossing at Albermarle Avenue and West Temple will provide safer and more direct access to the site. These recommendations are also outlined in the Ballpark Station Area Plan.
- Traffic calming: Implementing pedestrian-friendly design elements such as raised intersections at Richard Street, curb extensions at Main Street, and improved lighting along the whole corridor can enhance walkability and overall site accessibility.

While driving may be preferred by some residents and visitors, the surrounding context and proposed site design encourages active transportation as the primary way to access the site. Parking will be accommodated in centralized parking garages (buildings B and G in **Figure 1**) and limited on-street parking will remain available along West Temple and Main Street.

Vehicle Access

The development's vehicle access strategy balances the need for efficient traffic flow with a pedestrian-friendly urban environment. The site's design minimizes on-site vehicle-pedestrian conflicts while maintaining convenient access for residents, employees, visitors, and emergency services.

Northern Site Access (north of 1300 South)

The north site's primary vehicle access will be from West Temple and Paxton Avenue. Richard Street will serve as an access point for the shared parking in building B. However, vehicle use on Richard Street is limited for west-bound traffic only and should be limited access the shared parking structure to maintain a pedestrian-oriented environment. The intersection of 1300 South and Richards Street will incorporate a raised intersection design, creating a seamless pedestrian experience while still allowing vehicle access. The current pedestrian hybrid beacon (PHB) should be upgraded to a full pedestrian signal. Drivers should anticipate significant pedestrian activity in this area.

Southern Site Access (south of 1300 South)

- Main Street will serve as the primary vehicle access point for the southern portion of the development, including the shared parking garage (building G).
- Aligning with the Ballpark Station Area Plan, West Temple is planned as a "Festival Street," meaning vehicle access will be reduced and highlight managed during events to prioritize pedestrians and public gatherings.
- 1300 South will have no direct vehicle access points into the development, helping maintain a pedestrian-priority environment.
- Internal Promenade Streets will be designed to accommodate emergency vehicles but will otherwise function as pedestrian-oriented spaces.
- Richards Street (south of the existing ballpark) will not extend into the development site, reinforcing a pedestrian-priority environment.

By managing vehicle access and prioritizing active and multimodal transportation options, the development will support a welcoming, walkable, and transit-oriented environment.

Transportation Conclusion

The Ballpark NEXT redevelopment presents a unique opportunity to transform the area into a vibrant, multimodal urban district that prioritizes sustainable transportation options while maintaining necessary vehicle access. By implementing strategies to reduce single-occupant vehicle trips, enhance public transit access, expand active transportation infrastructure, and improve multimodal connectivity, the project can create a welcoming, accessible, and pedestrian-friendly environment.



Figure 8 – 1300 South Design Concept



CIVIL EVALUATION

Culinary Water

The existing stadium building is currently connected to the SLC water system via a service line. The portion of the stadium to remain will keep this connection. Three new 8" connections will be established to the existing water main along West Temple and a second connection could be established in Main Street to provide additional looping. The existing main lines in West Temple and Main Street are 6-inch cast iron pipes, that will need to be upsized to a minimum 12-inch water line along the project frontage per Salt Lake City requirements.

Fire Water

The existing stadium and new buildings will require the installation of a new fire service line connected to a Salt Lake City distribution mainline. Three new 12" connections will be established along West Temple and a second connection could be established in Main Street to provide additional looping.

Sanitary Sewer

There are existing sewer service laterals extending into the property from the 36-inch sewer main running under West Temple. The exact size, location and condition of the laterals is unknown at this time. The portion of the stadium to remain could maintain its existing connection upon review of the existing condition. Three new 8" sewer connections will be established along West Temple to service the new buildings.

If any of the new buildings are to have commercial kitchens, a grease interceptor will be needed. Grease interceptors should be located in service areas or areas with little public interaction due to the potential odor. Additionally, to mitigate odor concerns, the grease interceptor should be vented back through the building.

Salt Lake City has not indicated any issues with sewer capacity in this area. The on-site sanitary sewer lines must adhere to the Utah State Administrative Code for minimum slopes based on pipe diameter.

Storm Water

According to the FEMA Firm Panel 49035C0282H effective August 2, 2012, the north parking lot portion of the site is located in a Zone X or "Area of Minimal Flood Hazard". The portion of the site south of 1300 South is not located in any FEMA flood hazards.

The existing site storm drainage is divided into two subbasins: the north parking lot parcel and the south stadium parcel.

North Parcel:

The existing storm drain network under the north parking lot consists of 15-inch reinforced concrete pipe and captures water throughout a series of catch basins, draining underground to the north and

connecting to a line in West Temple Street that flows to the north. It does not appear that there is any underground or surface stormwater detention to collect and treat the stormwater runoff.

The proposed drainage system will be a significant enhancement to the existing condition. The proposed system is anticipated to consist of capturing of stormwater runoff from impervious surfaces and building roof areas and routing to a series of bioswales to detain and treat the water before discharging into the West Temple line. These bioswales can be implemented into the open space and creek partial daylighting design as habitat enhancements.

Based on Salt Lake City requirements of detaining the 100-year, 3-hour storm event, the north parcel will need to detain approximately 10,000 cubic feet of storage.

South Parcel:

The existing storm drain network drains into the line in West Temple, then heads west in 1300 South. There are two existing concrete vaults that are used for stormwater detention for the ballpark. One of these vaults can be preserved and used as part of the proposed system. The other vault will be demolished.

The proposed drainage system will capture stormwater runoff from impervious surfaces and building roof areas and route through a series of bioswales to detain and treat the water before discharging into the roadway system. The existing concrete vault can also provide some of the required stormwater detention. Based on Salt Lake City requirements, the south parcel will need to detain approximately 25,000 cubic feet of storage.

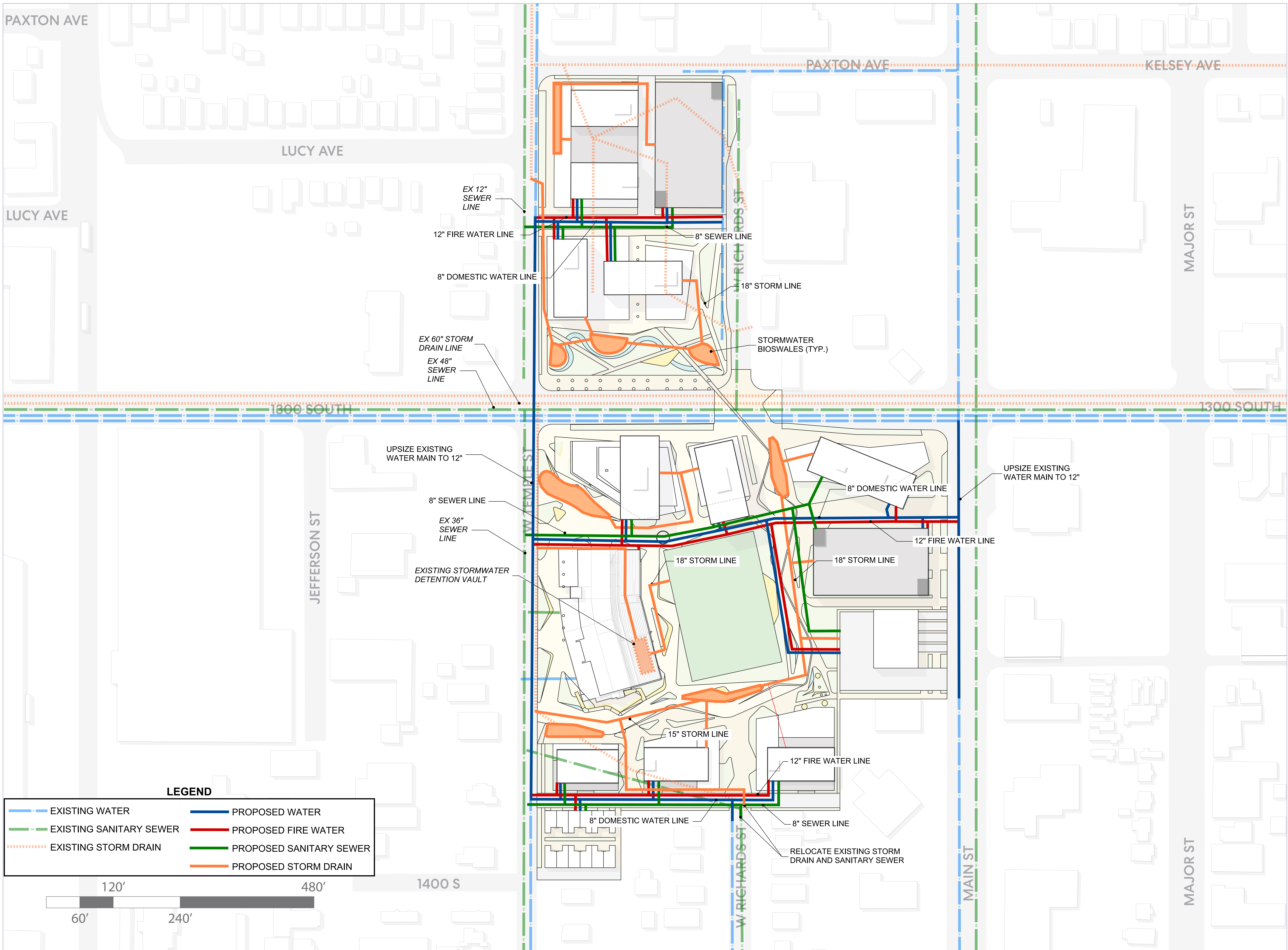
From the outset, storm water management solutions and low impact development (LID) techniques should be seamlessly integrated into the overall site design. Implementing LID strategies, such as flow-through islands in parking lots, bioswales, and underground galleries, will be beneficial. These measures can significantly influence the layout, grading, and landscaping of the site, ensuring effective storm water management while maintaining the functionality and aesthetics of the site.

Natural Gas

Natural gas service is supplied by Enbridge Energy, with existing gas mains running in 1300 South and West Temple. The proposed gas demand is unknown at this time and could vary significantly depending on the type of heating system used for the proposed buildings.

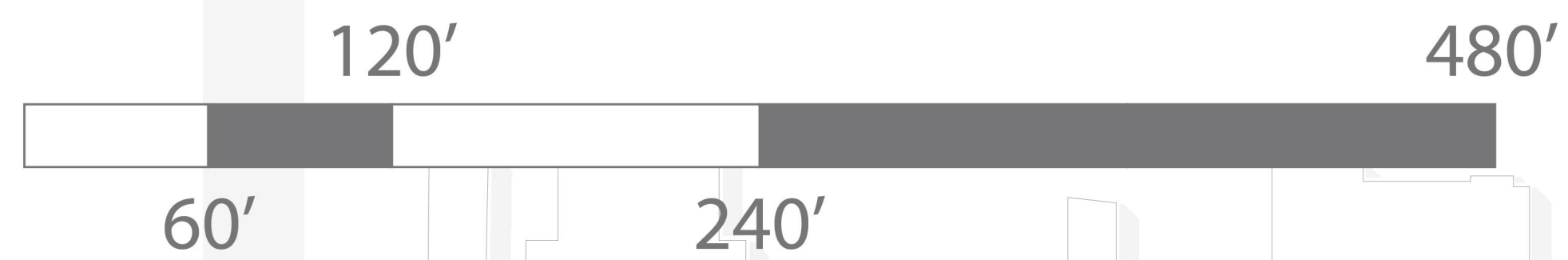
Utility Concept Exhibit

A utility concept exhibit is included under separate cover.



LEGEND

- EXISTING WATER
- EXISTING SANITARY SEWER
- EXISTING STORM DRAIN
- PROPOSED WATER
- PROPOSED FIRE WATER
- PROPOSED SANITARY SEWER
- PROPOSED STORM DRAIN



Appendix H:

Market Feasibility Study for Ballpark Next Redevelopment



SLCRDA

DRAFT
Subject to Change

Ballpark
NEXT



MARKET FEASIBILITY STUDY FOR THE BALLPARK NEXT
REDEVELOPMENT PROJECT IN SALT LAKE CITY

OCTOBER 9, 2024 // DRAFT V1 - SUBJECT TO CHANGE

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4. Comparable Outdoor Concert Venues	23
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A. Local Event Organizer Interviews	57
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8. Research Findings & Venue Options	85

Please Note: This report relies on a variety of information and assumptions to develop market, financial, and economic projections. Sources of information and assumptions include, but may not be limited to, information provided by the client, input and opinions provided by relevant third parties, Victus Advisors' industry experience and previous studies, and publicly available data from various industry sources. Any such information collected by Victus Advisors has not been audited or verified and has been assumed to be correct. There will be differences between actual events and the projections contained herein, and we express no assurances of any kind related to any projected information. Differences between projections and actual events may be material.



1. PROJECT INTRODUCTION

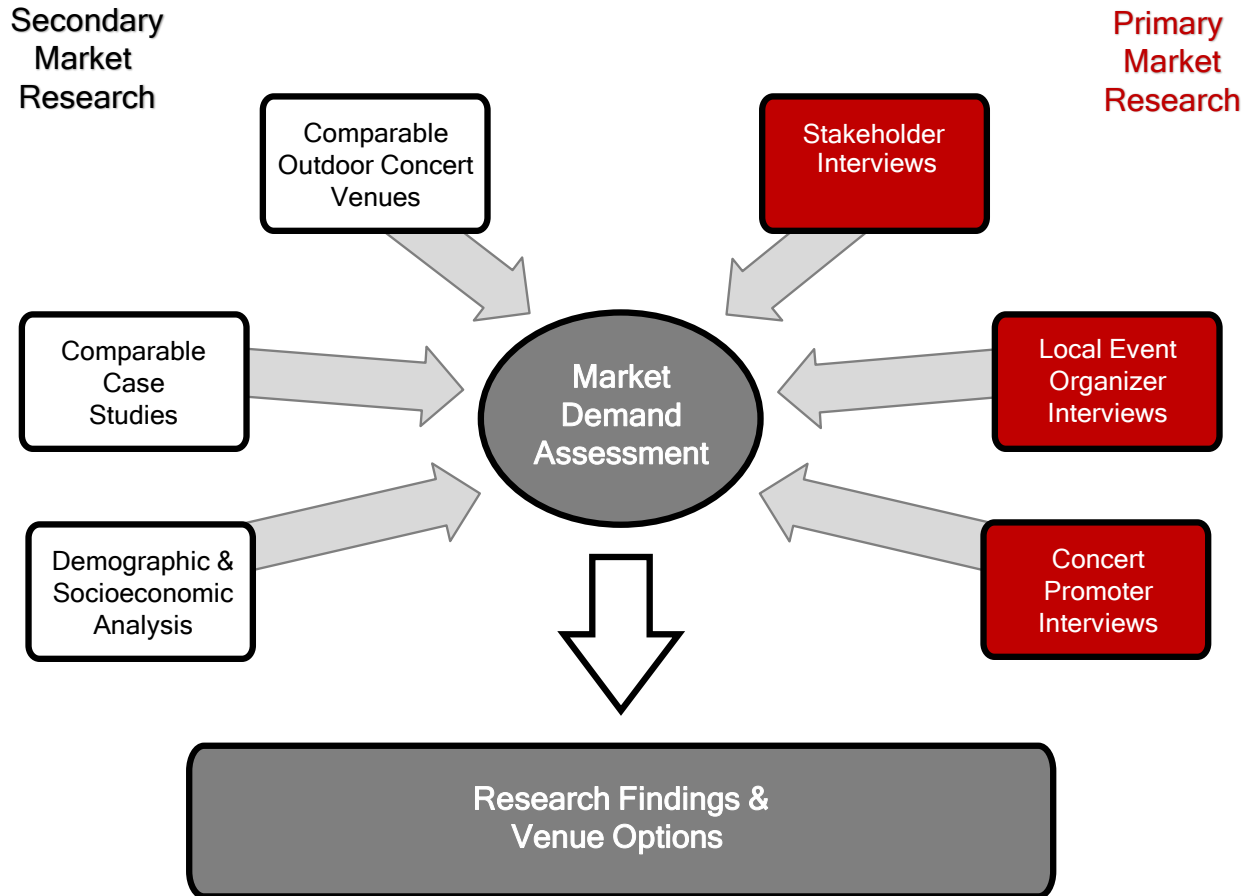
Victus Advisors (or “Victus”) was engaged in July 2024 by the Redevelopment Agency of Salt Lake City (“the Client” or “SLCRDA”) to a market demand study for potential future sports, recreation, arts, entertainment and/or event venues to serve as a community hub and destination anchor for the Ballpark Next development.

Market Feasibility Analysis

- a) **Local Demographic, Socioeconomic & Existing Facility Market Analysis:** Analyze baseline data related to the relevant events facility market area. This includes strengths and weaknesses of current local events facilities, analysis of drivetime zone populations, and comparable projects.
- b) **Market Demand Analysis:** Develop a comprehensive profile of the strengths and weaknesses of the area around Smith’s Ballpark.
- c) **Stakeholder and Event Organizer/Promotor Interviews:** Gain an understanding of where the market is regarding concerts and events in addition to community needs.



SPORTS & EVENT VENUE MARKET DEMAND ANALYSIS



Victus Advisors recognizes that the Client seeks a new development at the Smith's Ballpark site that is to serve as a community hub and destination anchor. The Salt Lake City Bee's played at Smith's for 30 years, ending an era in which residents in the area and specifically Council District 5 could gather for a ballgame. Our understanding is that this venue could either be an adaptive reuse of the existing Ballpark facility or a new community gathering and event space.



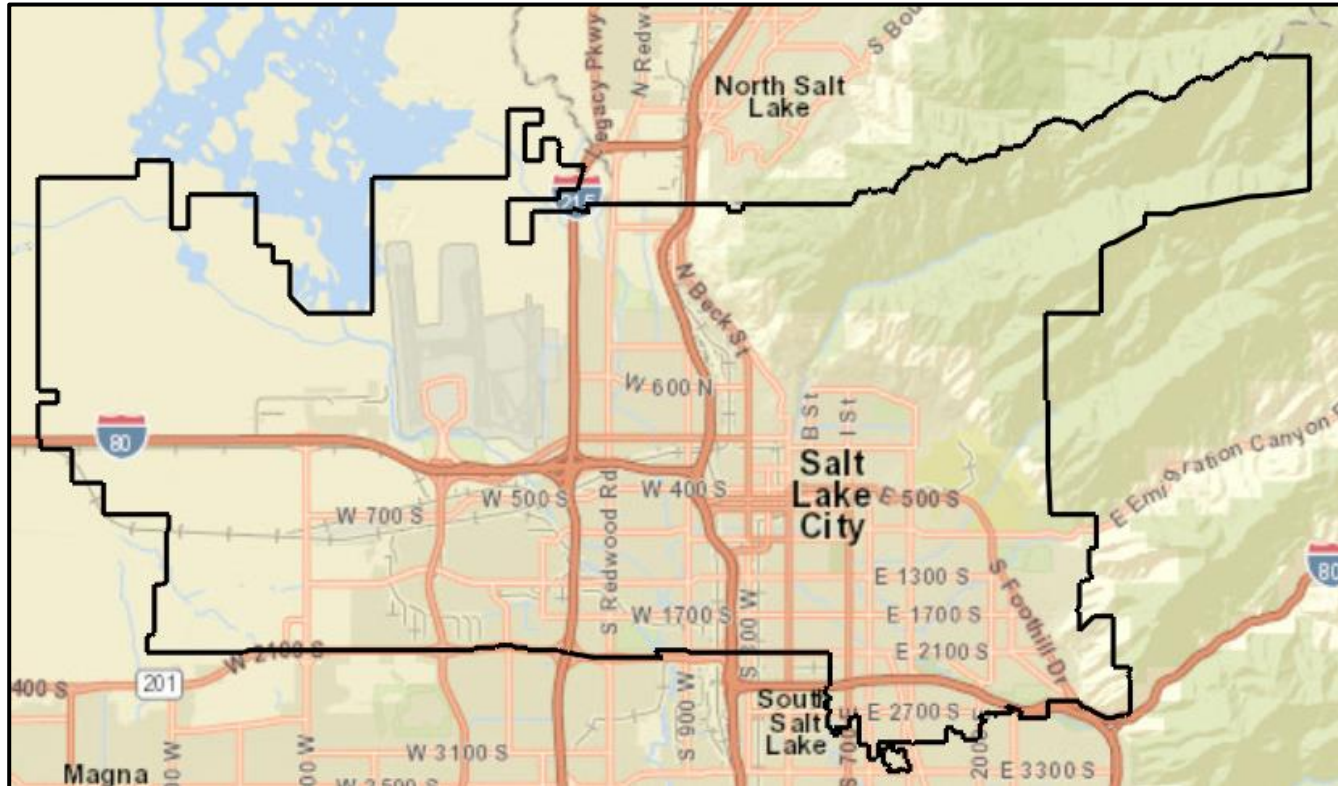


Source: Esri

The Smith's Ballpark site shown above.



2. DEMOGRAPHIC & SOCIOECONOMIC ANALYSIS



Source: Esri

The City of Salt Lake (or the “City”) has a population of 205,841.

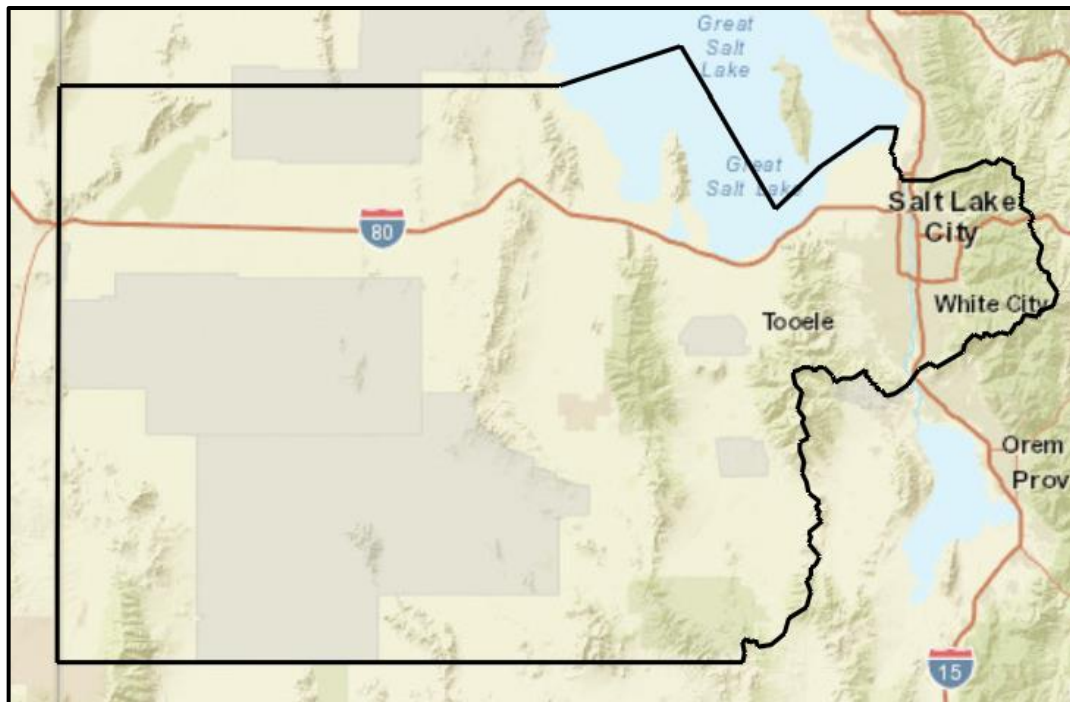


Metro.
Statistical
Areas

- MSA's are defined by the U.S. Office of Management & Budget.
- They are defined by adjacent counties with a high degree of social/economic integration and an urban core of 50,000 people or more.

Atlanta
MSA

- The Salt Lake City MSA ("Salt Lake MSA" for the remainder of the report) is comprised of 2 counties in Utah, Salt Lake and Tooele.
- The Salt Lake MSA has a population of more than 1,300,000 people, approximately 16% of which live in Salt Lake City.



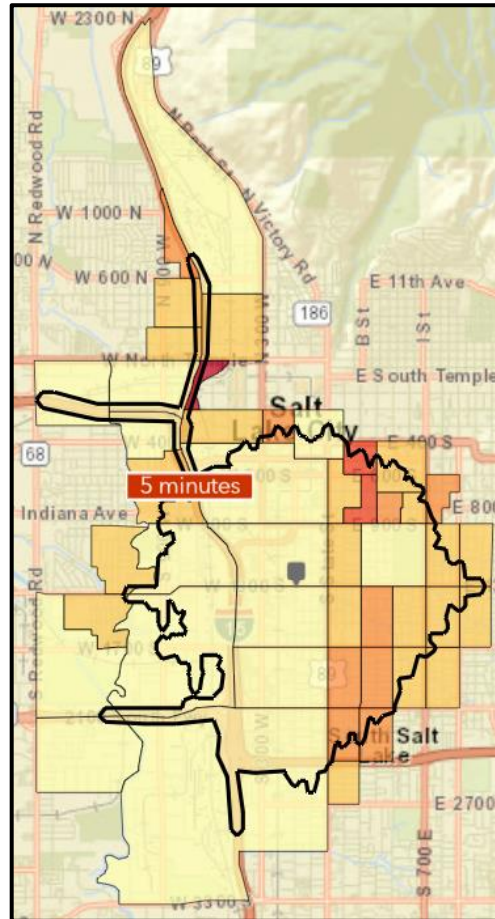
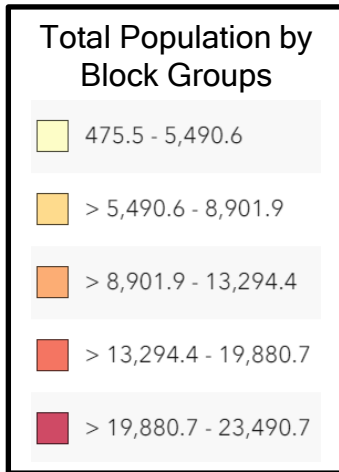
Source: Esri

	5-Minute Drive Smith's Ballpark
Population	32,328
Population Under 18	4,739
Percentage of Population Under 18	14.7%
Annual Pop. Growth (5-year Projection)	1.8%
Median Age	34.1
Median Household Income	\$64,729
Adjusted Median Household Income (1)	\$53,363
Total Households	15,523
Households With Children Under 18	5,996
Percentage of Households With Children	38.6%
Avg. Annual Spend (Entertainment & Recreation)	\$3,214

Source: Esri

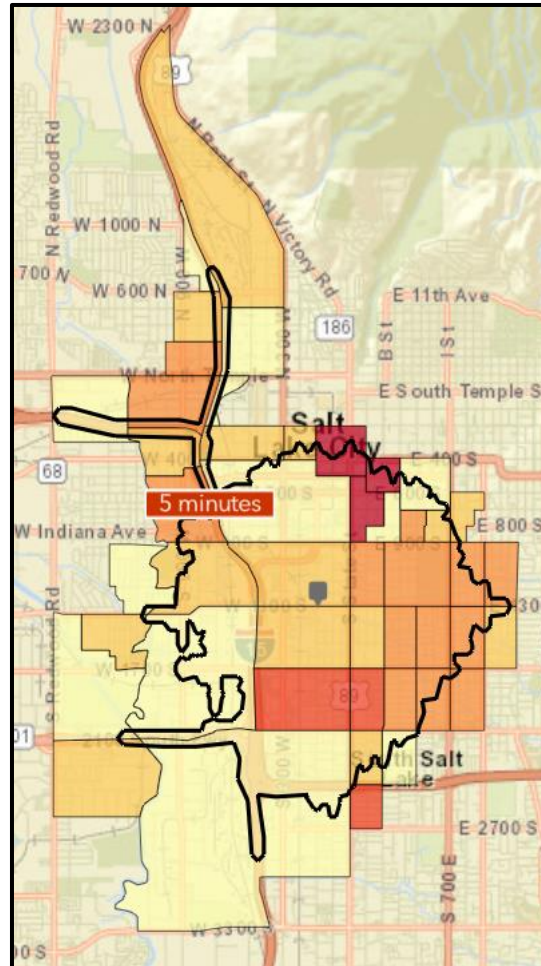
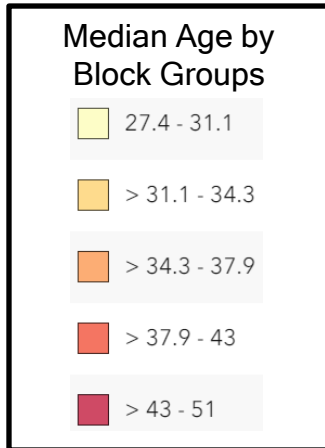
(1) Adjusted for cost of living according to Sperling

Within a 5-minute drive-time of Smith's Ballpark, the adjusted median household income is significantly lower than the city of Salt Lake and the Salt Lake MSA. Average annual spend on entertainment and recreation around \$3,200 is also on the lower end. Average median age below 35 signifies a younger population, in addition to almost 40% of households having children.



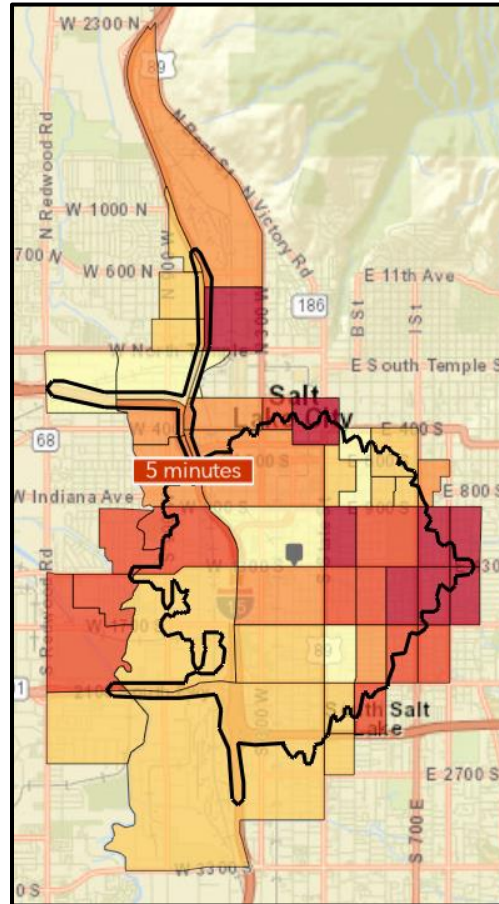
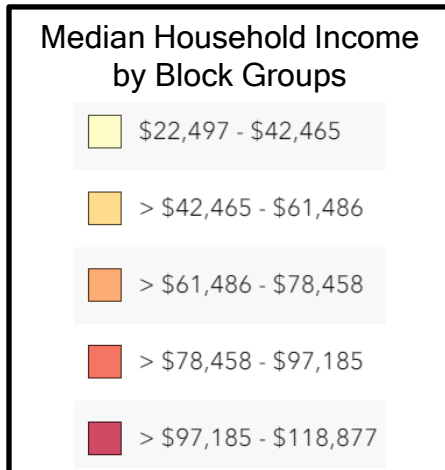
Source: Esri

Population density within 5 minutes from Smith's Ballpark is higher east of the stadium in addition to one block north, which is closer to downtown.



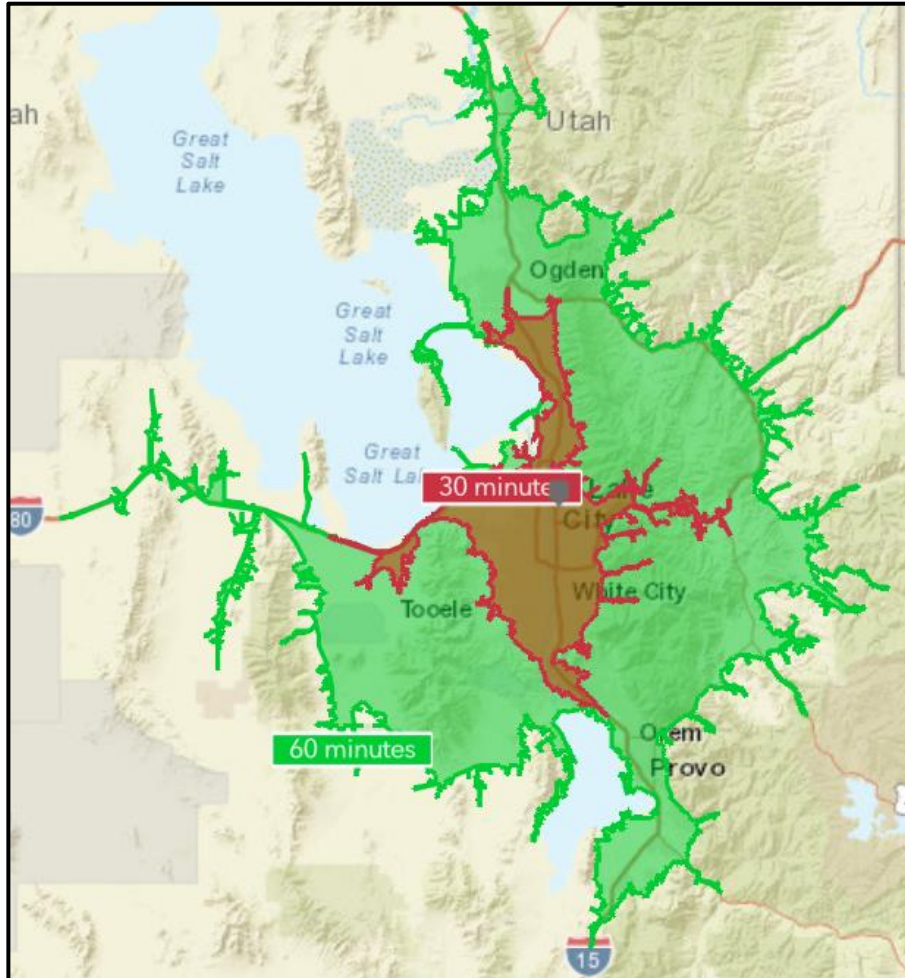
Source: Esri

Median age within 5 minutes from Smith's Ballpark is lowest in the southwest direction.



Source: Esri

Median Household Income within 5 minutes from Smith's Ballpark is significantly higher to the east of the site, in addition to some blocks north and west.



Source: Esri

Based upon Victus Advisors' experience, primary attendance for a potential concert venue (especially for weekday events) is typically drawn from within a 30-minute drive radius.

The population within a 30-minute drive-time radius of Smith's Ballpark is 1,536,850.

The population within a 60-minute drive-time radius of Smith's Ballpark is 2,804,700.

Smith's Ballpark is accessible from the highly trafficked I-15.



- **Population Growth:** Projected in the next 5 years, the population around 5 minutes from Smith's Ballpark is expected to grow at 1.8%.
- **Age:** The 5-minute area around Smith's Ballpark median age is lower than the national average. A lower median age tends to represent a larger presence of working-age populations, which can be a positive indicator for a more active community.
- **Household Income:** After adjustments for cost of living, the median household income within 5 minutes of Smith's Ballpark is lower than both Salt Lake City and the Salt Lake MSA.

CONCLUSION

The 5-minute area around Smith's Ballpark is a young and growing population that could benefit from a redeveloped ballpark with features and amenities for young families.



3. SALT LAKE OUTDOOR CONCERT VENUE ANALYSIS



This section presents a summary of key trends in the concert industry in order to provide a perspective from which to assess operational impacts of the possible amphitheater at Smith's Ballpark. The review of industry trends is presented as follows:

- Overview of Live Concert Economics
- Talent Costs
- Consolidation of Promoters



The economics of the concert industry have changed substantially over the past several years, as acts have demanded higher compensation or guarantees, resulting in increased ticket prices. The upward trend in ticket prices has increased gross ticket sales revenue to record levels, but it has also negatively impacted attendance at concert events over the past several years. The result of these trends has produced lower profit margins for concert promoters and live music venues.

Concert ticket revenue is generally allocated between the touring act, show expenses, and the promoter. While each deal is unique and can vary considerably from market to market, concert ticket revenue is generally allocated as follows: approximately 60 to 70 percent of the ticket price is paid to the performer as their guarantee; approximately 20 percent pays for show expenses including staffing, utilities, facility rent and other such costs; and approximately 10 to 20 percent is retained by the promoter.

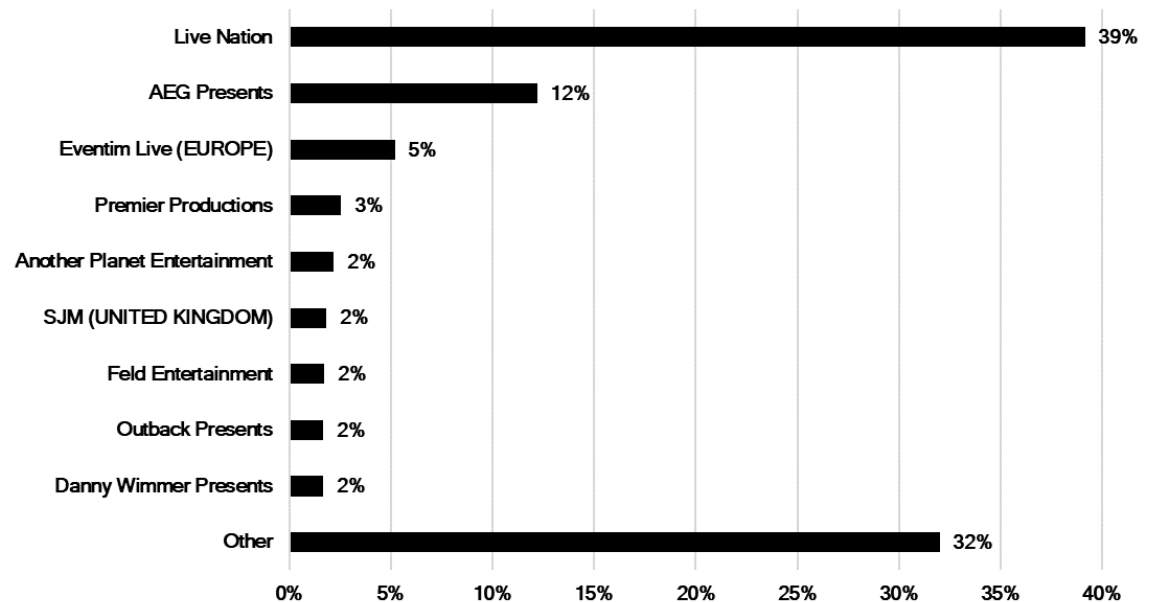
The facility owner/operator typically receives fixed rent from the promoter for use of the venue, and potentially a small share of ticket sales revenues if they help co-promote the event. It should be noted that the venue operator also generates other revenue streams from premium seating, box office fees and ticket sales rebates, food and beverage sales, venue advertising/sponsorships, and parking.



The concert industry has experienced a significant consolidation of promoters in recent years, which has had a significant impact on the ability of live entertainment venues to attract marquee events without a direct affiliation with a major promoter. The following table summarizes the top global concert promoters in terms of percentage of ticket sales, and it is also worthy to note that:

- Live Nation and AEG own, operate, and/or have booking rights for such a significant number of live entertainment venues in the United States that they can create block-booking arrangements, sometimes exclusive, with the top talent.

TOP GLOBAL PROMOTERS:
% OF TICKET SALES



Source: Pollstar



As shown on the previous page, Live Nation (and its subsidiaries) was responsible for 39% tickets sold worldwide in 2021, while AEG (and its subsidiaries) was responsible for 12% of tickets sold. Combined, Live Nation and AEG sold 51% of all concert tickets across the globe in 2021.

Today, Live Nation owns, operates, has booking rights and/or has an equity investment in over 200 concert venues, and over 30 are amphitheaters. They produce over 40,000 concerts for 4,000 artists annually around the world. Their relationships/ownership positions with a significant number of live entertainment venues have given them the opportunity to create block-booking arrangements with facilities across North America and beyond.

Traditionally, live entertainment venues would have multiple booking agents and promoters bringing acts into their market. However, over the last several years, organizations such as Live Nation have taken control of more acts and have sought exclusive rights to book a particular venue, altering the former “free market” relationships among artists, promoters, and venues. That said, the trend toward exclusivity seems to be waning, as these entities have become more willing to work with each other to maximize revenues. For example, a concert act controlled by Live Nation may be more likely than in years past to play a venue operated by AEG, if both entities feel there is an opportunity for a mutually beneficial agreement.



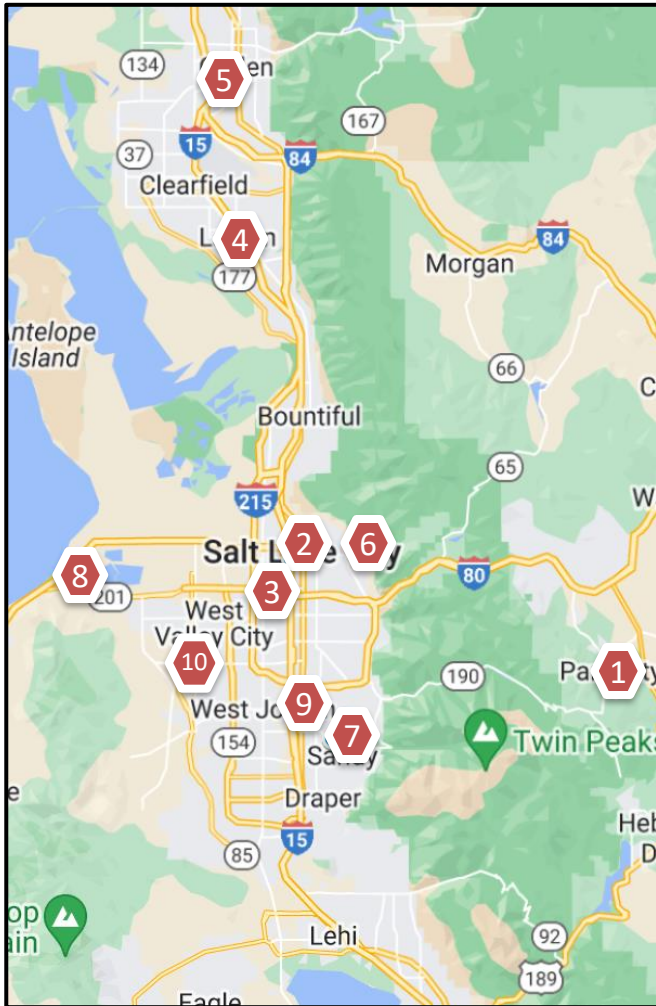
- **Live Concert Economics:** The economics of the concert industry have changed substantially over the past several years, as acts have demanded higher compensation or guarantees, resulting in increased ticket prices. The upward trend in ticket prices has increased gross ticket sales revenue to record levels, but it has also negatively impacted attendance at concert events over the past several years. The result of these trends has produced lower profit margins for concert promoters and live music venues.
- **Consolidation of Promoters:** Live Nation and AEG own, operate, and/or have booking rights for such a significant number of live entertainment venues in the United States that they can create block-booking arrangements, sometimes exclusive, with the top talent.

CONCLUSION

An outdoor concert venue could be a potential option for a redeveloped Smith's Ballpark. That said, concert venues are seeing lower profit margins. Furthermore, it is imperative that that a concert venue be operated and/or booked by a firm like Live Nation or AEG to ensure maximize usage.



4. COMPARABLE OUTDOOR CONCERT VENUES



Venues

- 1 Deer Valley Snow Park Outdoor Amphitheater
- 2 Gallivan Center
- 3 Granary Live
- 4 Kenley Amphitheater
- 5 Ogden Amphitheater
- 6 Red Butte Garden Amphitheater
- 7 Sandy Amphitheater
- 8 The Great Saltair Amphitheater
- 9 The Plaza at America First Field
- 10 Utah First Credit Union Amphitheatre

Victus Advisors identified and profiled a set of comparable outdoor concert venues in the Salt Lake market that are potential competitors for a new outdoor concert venue at a redeveloped Smith's Ballpark. These facilities have been utilized for various concert and entertainment events.

Source: Google Maps

Note: Venues are listed in alphabetical order

- Location: Park City
- Opened: 1999
- Seating Capacity: 4,500
- Owner/Operator: Deer Valley
- Annual Event Mix:
 - Concerts and shows
 - There has been 5 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 3,204 average tickets sold
 - \$216,398 average gross revenue per show
- Sample Artists:
 - Leslie Odom Jr.
 - The Beach Boys
 - Michael Franti & Spearhead
 - Ziggy Marley
 - Utah Symphony



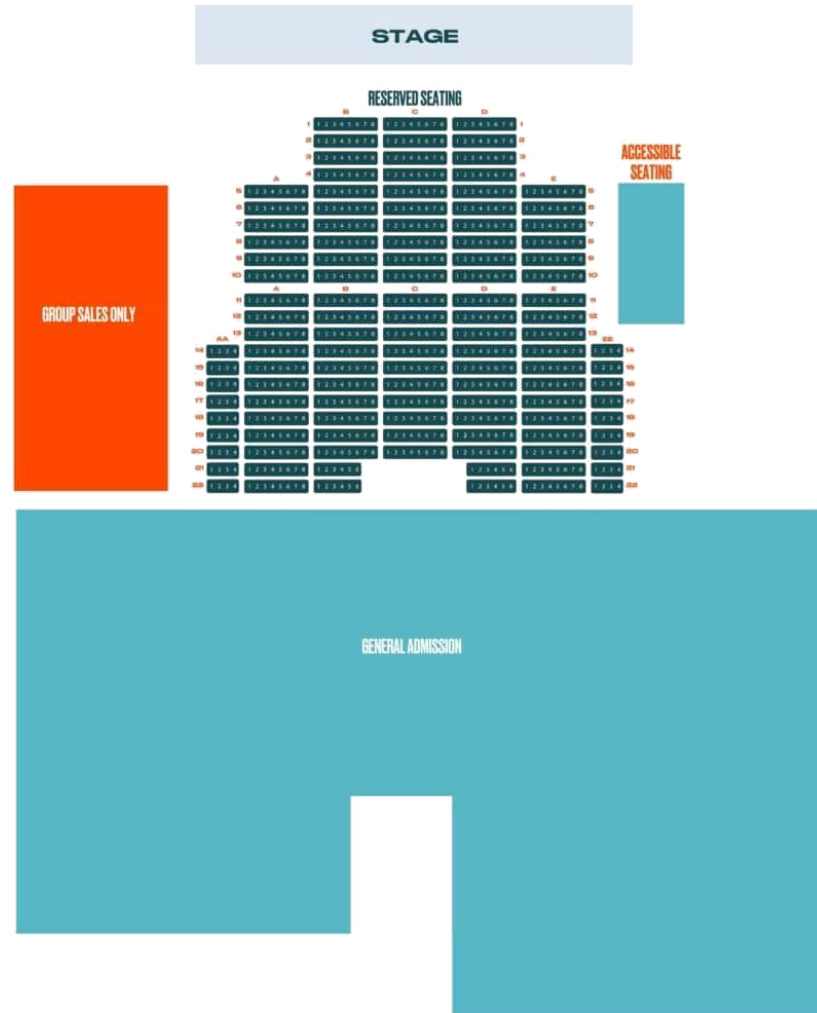
DEER VALLEY®



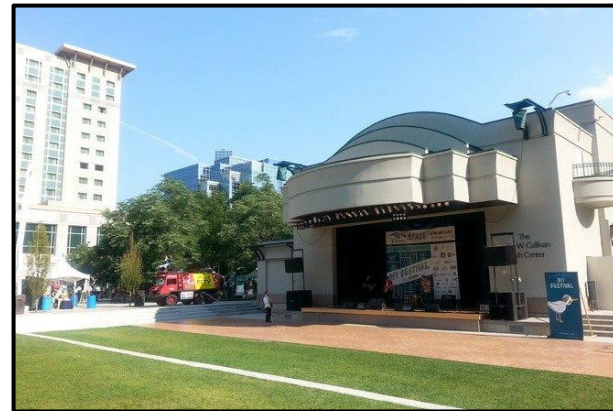
* Source: Pollstar (based on last 36 months of data available)



Seating Chart



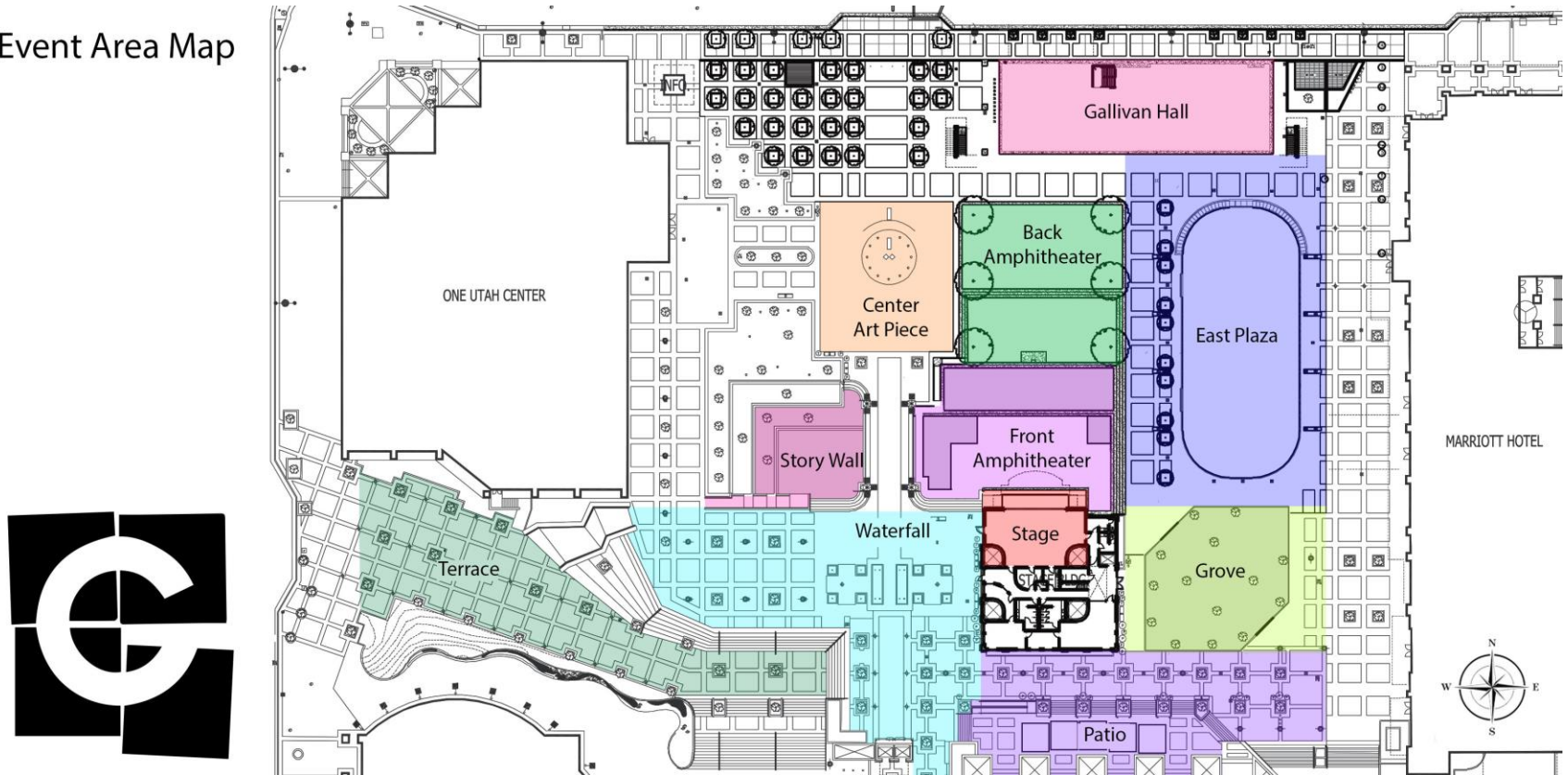
- Location: Salt Lake City
- Opened: 1998
- Seating Capacity: 3,000
- Owner: Salt Lake City
- Operator: Gallivan Center Team
- Annual Event Mix:
 - Concerts and shows
- Box Office Averages (past 36 months)*:
 - Annual average of 5 events
- Sample Artists:
 - Thundercat
 - Remi Wolf
 - Sean Paul
 - Diplo
 - Young The Giant





Seating Chart

Event Area Map



- Location: Salt Lake City
- Opened: 2023
- Seating Capacity: 7,500
- Owner: Private
- Operator: Live Nite Events
- Annual Event Mix:
 - Concerts and shows
 - There have been 13 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 7,164 average tickets sold
 - \$ 316,577 average gross revenue per show
- Sample Artists:
 - Still Woozy
 - The National
 - The War on Drugs
 - Tyga
 - Ludacris

GRANARY

— LIVE —



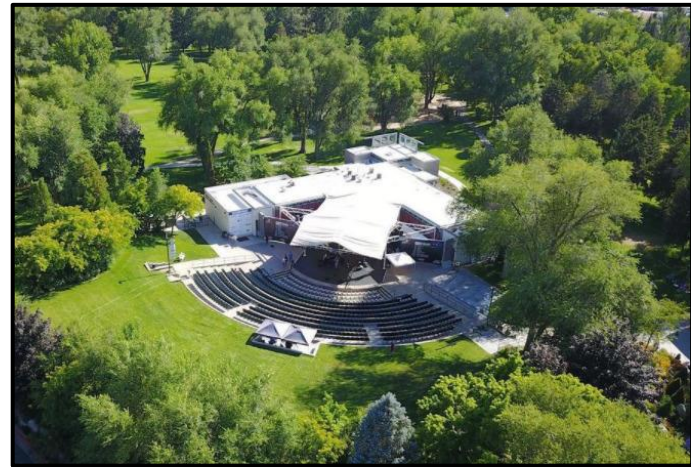
* Source: Pollstar (based on last 36 months of data available)

Layout



- Location: Layton
- Seating Capacity: 2,438
- Owner: Layton City
- Operator: Davis Arts Council
- Annual Event Mix:
 - Concerts and shows
 - There have been 8 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 1,672 average tickets sold
 - \$49,040 average gross revenue per show
- Sample Artists:
 - Ben Folds
 - Straight No Chaser
 - Tito Puente Jr.
 - Boz Scaggs
 - Kansas

DAVIS **arts** COUNCIL



Seating Chart



- Location: Ogden City
- Seating Capacity: approximately 7,500 (about 7,300 standing, about 273 fixed seats)
- Owner/Operator: Ogden City
- Annual Event Mix:
 - Concerts and shows
 - There have been 11 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 6,049 average tickets sold
 - \$108,096 average gross revenue per show
- Sample Artists:
 - Bon Iver
 - Flume
 - Beach House
 - Modest Mouse
 - The National

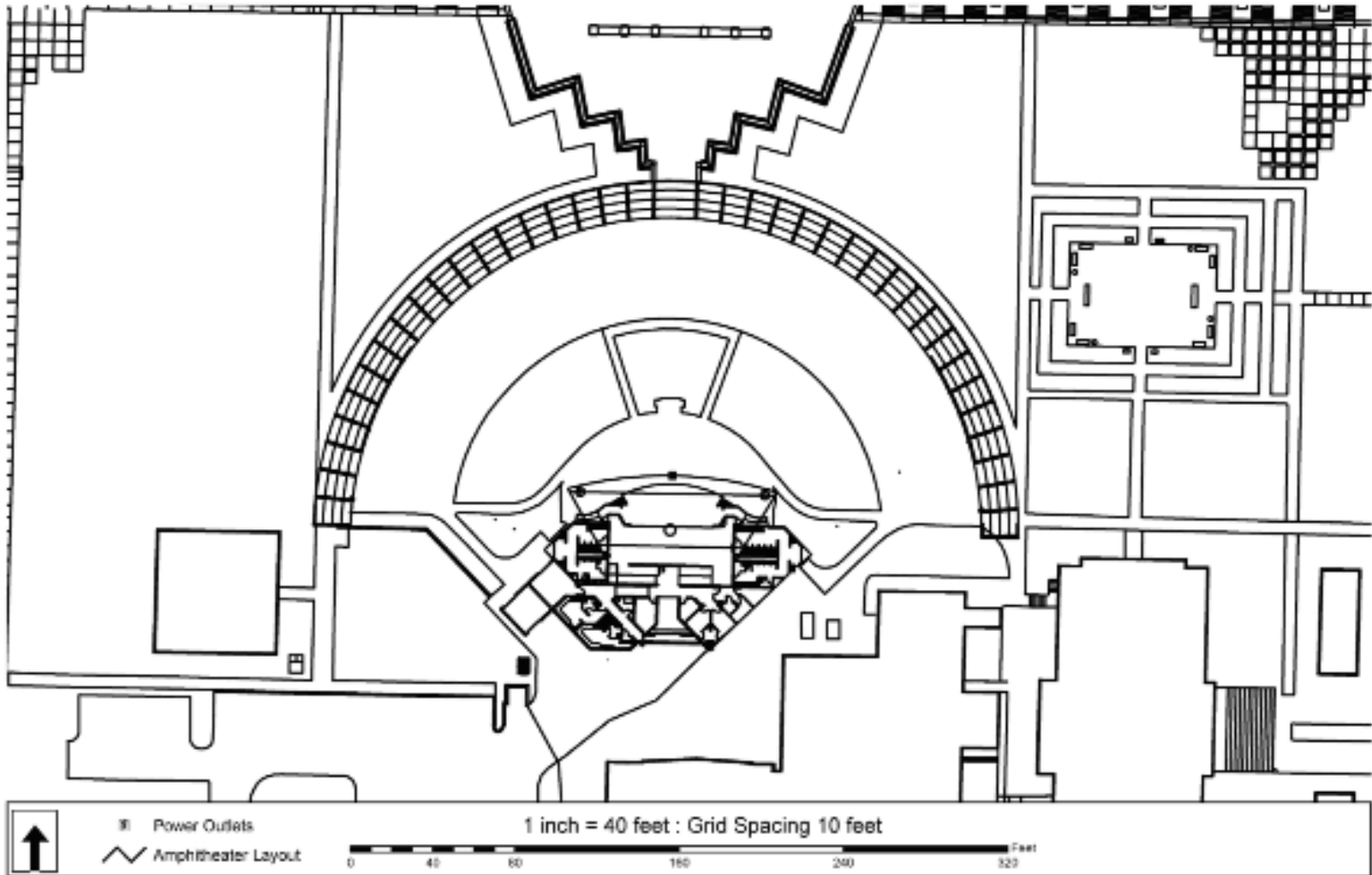
Ogden
UTAH™
Arts, Culture,
& Events



* Source: Pollstar (based on last 36 months of data available)



Venue Layout



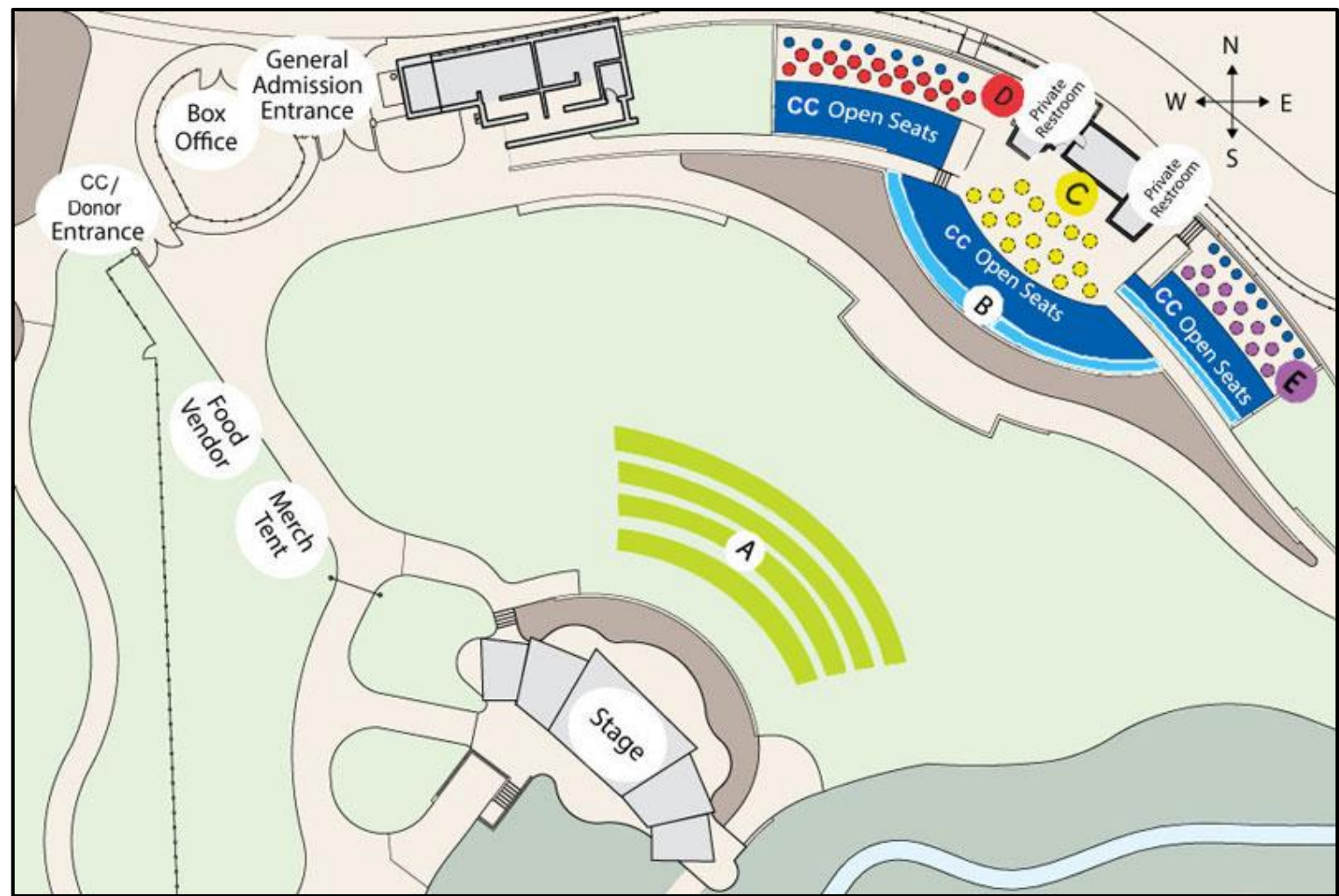


- Location: Salt Lake City
- Opened: 2008
- Seating Capacity: 3,000
- Owner: University of Utah
- Operator: Red Butte Garden and Arboretum
- Annual Event Mix:
 - Concerts and shows
 - There have been 30 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 2,975 average tickets sold
 - \$160,263 average gross revenue per show
- Sample Artists:
 - Train
 - My Morning Jacket
 - The Revivalists
 - Michael Franti & Spearhead
 - O.A.R
 - ZZ Top





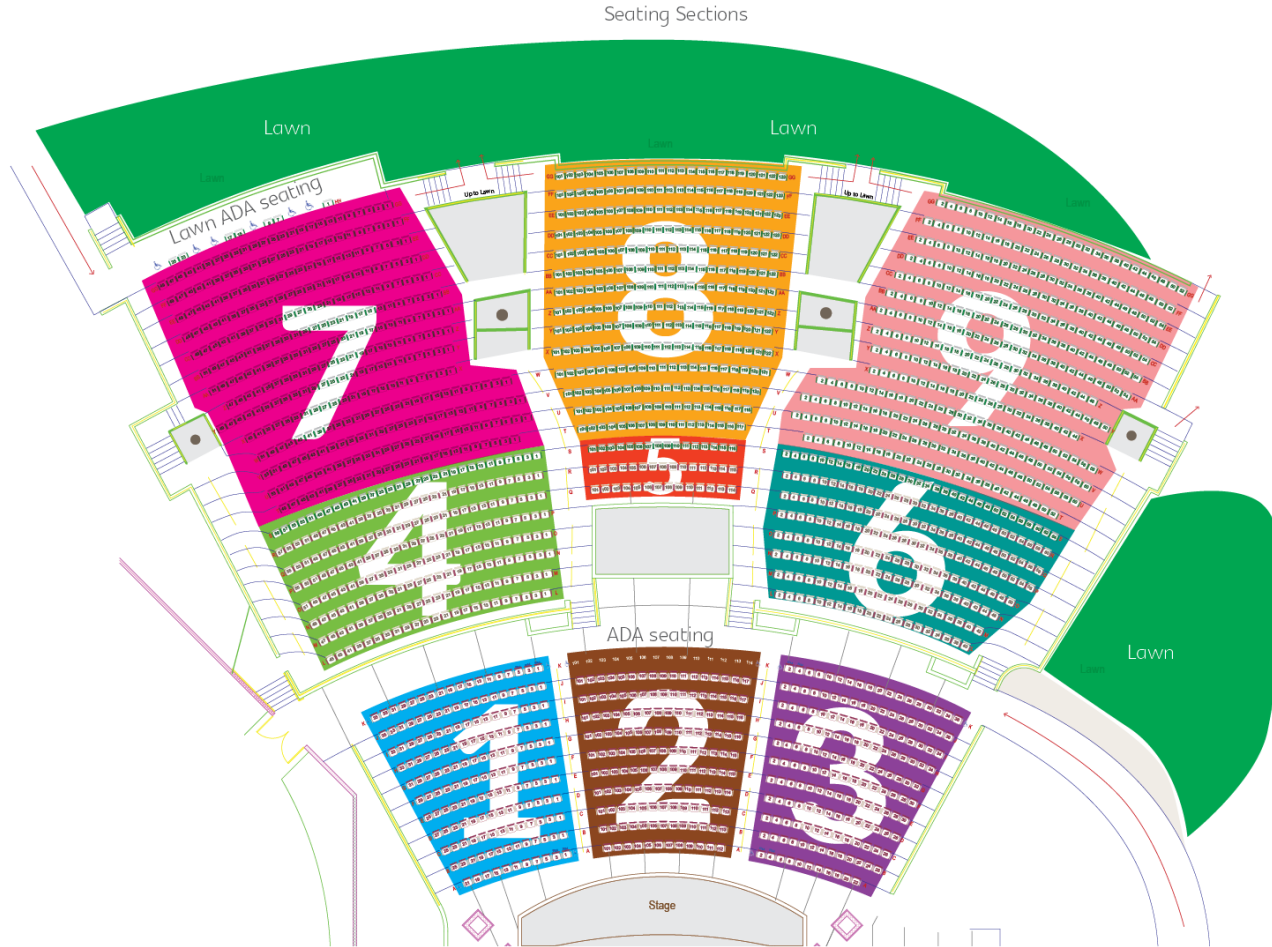
Seating Chart



- Location: Sandy City
- Opened: 2003
- Seating Capacity: 2,700 (2,000 seated, 700 lawn)
- Owner/Operator: Sandy City
- Annual Event Mix:
 - Concerts and shows
 - There have been 25 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 2,530 average tickets sold
 - \$142,894 average gross revenue per show
- Sample Artists:
 - Norah Jones
 - Tash Sultana
 - Peter Frampton
 - John Legend
 - Mt. Joy



Seating Chart



- Location: Magna
- Seating Capacity: 7,000
- Owner/Operator: Private
- Annual Event Mix:
 - Concerts and shows
 - There have been 8 ticketed events in 2024
- Box Office Averages (past 36 months)*:
 - 5,180 average tickets sold
 - \$262,522 average gross revenue per show
- Sample Artists:
 - Weezer
 - Dominic Fike
 - Quinn XCII
 - The All-American Reject
 - Paramore



- Location: Sandy
- Seating Capacity: 7,000
- Owner/Operator: Real Salt Lake
- Annual Event Mix:
 - Concerts and shows
- Additional Information:
 - The Plaza at America First Field has recently begun hosting concerts, and the venue is currently scheduled to host two Live Nation events later this year in September.
- Sample Artists:
 - Wallows
 - Goose



- Location: West Valley City
- Opened: 2003
- Seating Capacity: 25,000 (7,000 fixed, 18,000 lawn)
- Construction Cost: \$20.51M in Q2 2024 dollars*
- Owner/Operator: Live Nation
- Annual Event Mix:
 - Concerts and shows
 - There have been 45 ticketed events in 2024
- Box Office Averages (past 36 months)**:
 - 15,626 average tickets sold
 - \$775,362 average gross revenue per show
- Sample Artists:
 - Jack Johnson
 - Chris Stapleton
 - Dierks Bentley
 - Post Malone
 - Luke Bryan



UTAH FIRST
CREDIT UNION
AMPHITHEATRE

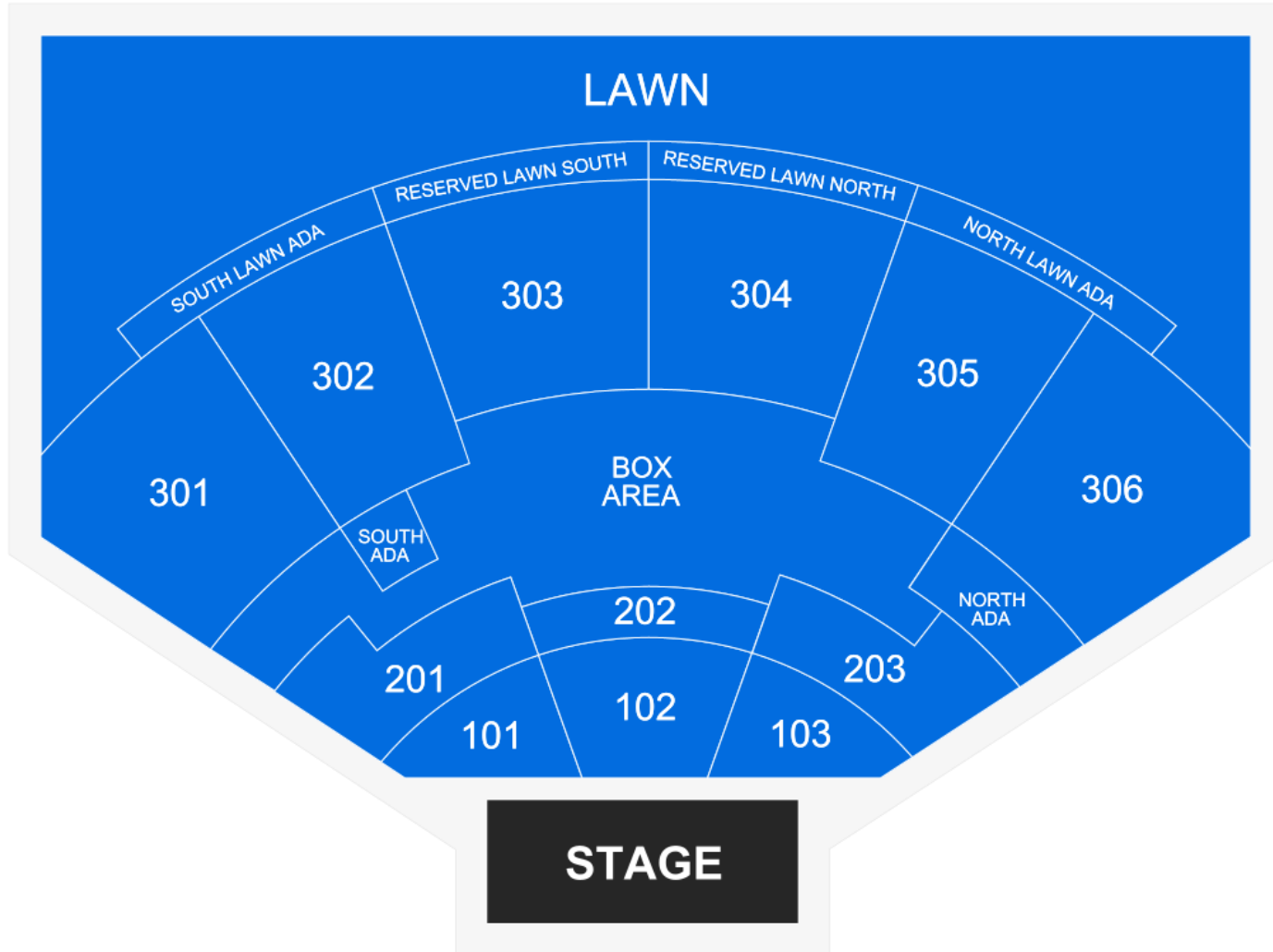


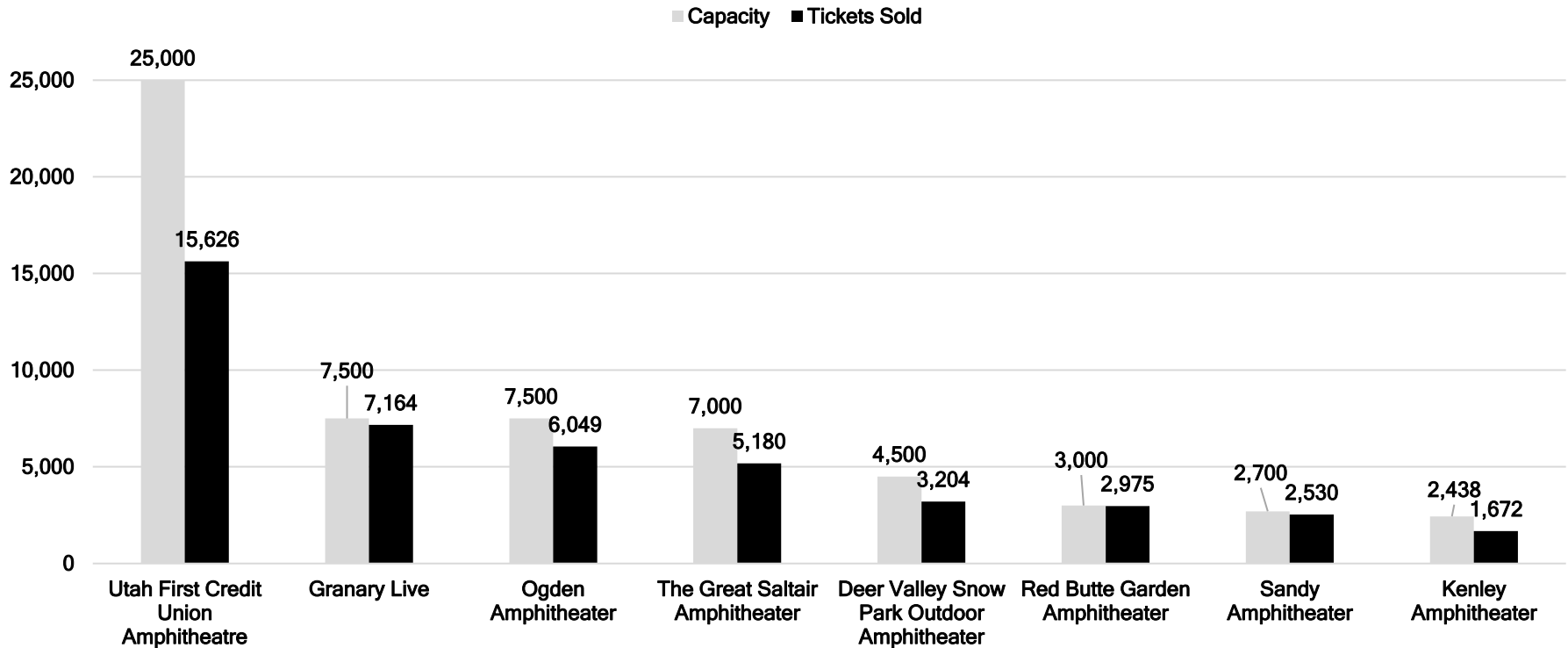
* Estimated according to the Turner Building Cost Index Q2 2024

** Source: Pollstar (based on last 36 months of data available)



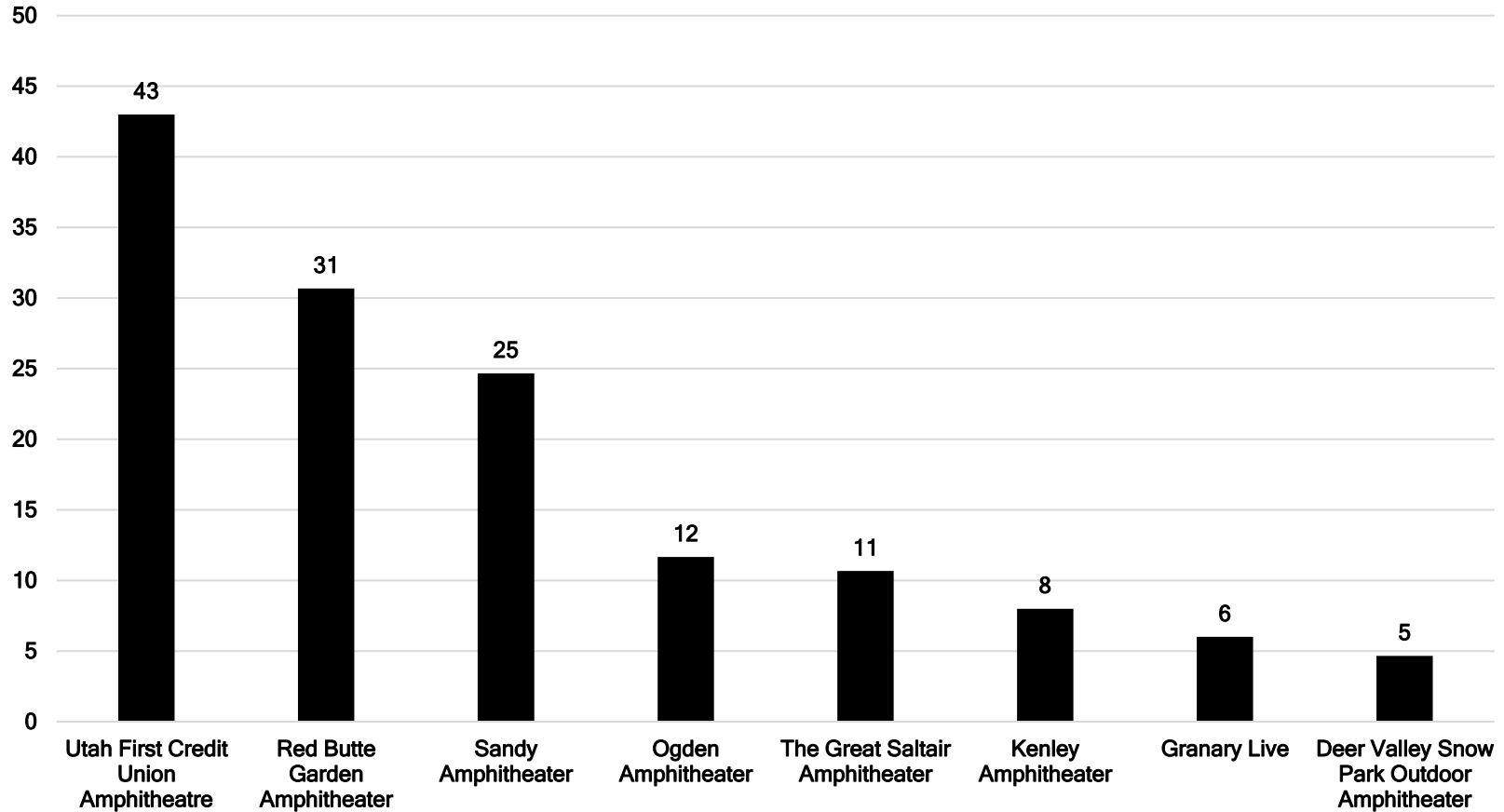
Seating Chart





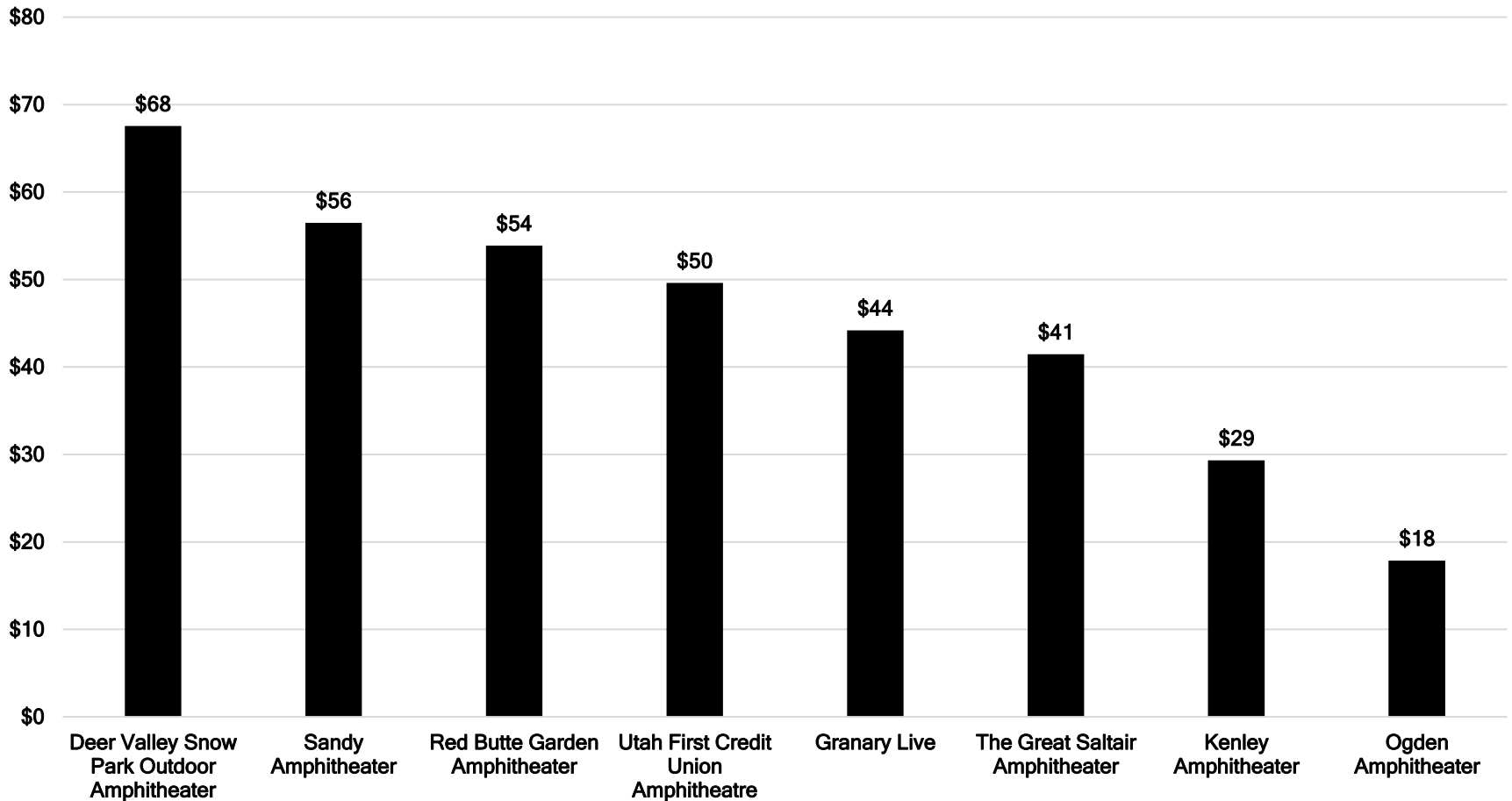
Source: Pollstar (based on 36-month averages)

The profiled outdoor concert venues averaged 5,500 tickets sold per event over the last three (3) years. Only three (3) venues sold more than 6,000 tickets per concert on average.



Source: Pollstar (based on 36-month averages)

Utah First Credit Union Amphitheater has averaged the most annual events over the last three (3) years. Overall, the profiled venues have averaged a total of 17 annual events.



Source: Pollstar (based on 36-month averages)

Deer Valley Snow Park Outdoor Amphitheater has averaged the highest ticket price for its events over the last 36 months. The profiled venues have averaged a ticket price of \$45, with the median ticket price being \$47 dollars.



Venue	Location	Owner	Operator	Primary Promoter	Seating Capacity
Utah First Credit Union Amphitheatre	West Valley City	Live Nation	Live Nation	Live Nation	25,000
Granary Live	Salt Lake City	Live Nite Events	Live Nite Events	Live Nite Events	7,500
Ogden Amphitheater	Ogden	Ogden City	Ogden Twilight	Ogden Twilight	7,500
Gallivan Center	Salt Lake City	Salt Lake City	Gallivan Center Team	S&S Presents	7,500
The Plaza at America First Field	Sandy	Real Salt Lake	Real Salt Lake	Live Nation	7,000
The Great Saltair Amphitheater	Magna	Saltair Rentals	Live Nite Events	Live Night Events	7,000
Deer Valley Snow Park Outdoor Amphitheater	Park City	Deer Valley	Deer Valley	The State Room Presents	4,500
Red Butte Garden Amphitheater	Salt Lake City	University of Utah	Red Butte Garden and Arboretum	The State Room Presents	3,000
Sandy Amphitheater	Sandy	Sandy City	Sandy City	Postfontaine Presents	2,700
Kenley Amphitheater	Layton	Layton City	David Arts Council	David Arts Council	2,438
AVERAGE					7,414
MEDIAN					7,000

Source: Pollstar (based on last 36 months of data available)

Note: Sorted by Seating Capacity in descending order

Utah First Credit Union Amphitheater is the largest outdoor concert venue in the market with a capacity of approximately 25,000 people. The profiled venues have an average of 7,414 people, with a median of 7,000 people.



<u>Venue</u>	<u>Tickets Sold</u>	<u>Seating Capacity</u>	<u>Percentage</u>
Red Butte Garden Amphitheater	2,975	3,000	99%
Granary Live	7,164	7,500	96%
Sandy Amphitheater	2,530	2,700	94%
Ogden Amphitheater	6,049	7,500	81%
The Great Saltair	5,180	7,000	74%
Deer Valley Snow Park Outdoor Amphitheater	3,204	4,500	71%
Kenley Amphitheater	1,672	2,438	69%
Utah First Credit Union Amphitheatre	15,626	25,000	63%
AVERAGE	5,550	7,455	81%
MEDIAN	4,192	5,750	77%

*Source: Pollstar (based on last 36 months of data available)
Note: Sorted by Percentage of Capacity in descending order*

The profiled concert venues filled their capacity at an average of 81% and a median of 77%. Additionally, these venues sold 5,550 tickets per event on average with a median of 4,192 tickets per event.



- **Average Tickets Sold:** The profiled outdoor concert venues averaged 5,500 tickets sold per event over the last three (3) years. Only three (3) venues sold more than 6,000 tickets per concert on average.
- **Concert Seating Capacity:** Utah First Credit Union Amphitheater is the largest outdoor concert venue in the market with a capacity of approximately 25,000 people. The profiled venues have an average of 7,414 people, with a median of 7,000 people.

CONCLUSION

Based on average tickets sold and average seating capacity for outdoor concert venues in the Salt Lake City Market, there could be demand for a venue in the 5,000 to 6,000-seat capacity range.



5. STAKEHOLDER INTERVIEWS

The identified local stakeholders with valuable insight on the Smith's Ballpark site and scheduled a series of interviews. The goal of these interviews was to gather feedback from key local groups and community leaders regarding the market potential for a potential new development to replace the Salt Lake Bees vacancy. The set of topics and questions included perceptions of Salt Lake County nodes near the stadium, different types of possible usage, and other feedback concerning the potential new developments.

INTERVIEWS - Victus Advisors conducted interviews with representatives from the following seven (7) local groups and organizations:

- Maven District
- Salt Lake City Arts Council
- Salt Lake City Department of Economic Development
- Salt Lake City Downtown Alliance
- Salt Lake City Public Lands
- Salt Lake City Redevelopment Agency
- Visit Salt Lake

Key highlights of these interviews are summarized by topic on the following pages. The highlights presented tend to focus on the topics and sentiments that were raised by multiples interviewees.



PERCEPTIONS OF SMITH'S BALLPARK & COMMUNITY NEED

- In general, interviewees felt that there is a need for the development to be a major part of the community's identity. One interviewee stated that local business development is crucial if the space includes a business sector. Multiple stakeholders believe in the primary activation being public space, one emphasizing how it should be easily programmable. Consensus is that the Bee's are leaving a void in the community that needs to be filled with something community focused and not what's best financially.
- Regarding the idea of an outdoor concert venue, some stakeholders did say that depending on capacity, it could fill a void in the performance industry. While practical, a venue like this might not be the best idea because the surrounding area of the ballpark is mainly single-family homes. Concerns about loud noise continuing into late hours was mentioned by multiple interviewees.
- Another interviewee expressed that to keep any sort of public/open space safe, bright lighting in addition to clear sight lines are very important. Other parks have been susceptible to homeless staying there because of a lack of effort to account for these problems. There is a concern that a new open/green space could amplify those issues



POTENTIAL PROGRAMMING & ACTIVITY

- Interviewees had mixed opinions regarding what should take the place of the Salt Lake Bee's. While these views varied when referencing secondary uses, most of the ideas for the venue included a community focus. Some stated they wanted a possibility for larger, commercial operations while others opposed that idea, believing that would take away from the space being something the local residents could claim as their own.
- One interviewee stressed the importance of attempting to keep the venue revolved around sports because of the history, whether that be a community park or something more structured with an indoor gym where organized leagues can play.
- One stakeholder wants the venue to be a possible place to host smaller, community events like flea markets/farmers markets. Many of these smaller groups have had trouble finding places to operate downtown, so the ballpark site could provide a place to go.
- Another stakeholder wants to turn the venue into a women's sports and concert venue. The field would act as the epicenter for development around it, including possible festival streets, local art, a parking lot with programmable green space on top, and more. The stakeholder stated, "the community wants to feel something", and local activation is the best way to do that.



CONCLUSIONS

- All interviewees were supportive of ideas that appeal to people within Salt Lake City in addition to others outside of the area to drive growth in a location that many people may not know much about, including those who live within closer proximity.
- The TRAX station within walking distance of the ballpark is very important in ensuring people have free/affordable transportation. Easy access at zero to low cost will push attendance numbers higher and create more foot traffic for the entire area around the site.
- The main goal of the ballpark project isn't to just create something special at the site. While that is part of the process, positive overall economic impact in the areas within close proximity to the stadium is another major part of this development and a key indicator of how successful it will be.



- **Perceptions of Smith's Ballpark & Community Need:** Consensus is that the Bee's are leaving a void in the community that needs to be filled with something community focused and not what's best financially.
- **Concert Venue Possibility:** Regarding the idea of an outdoor concert venue, some stakeholders did say that depending on capacity, it could fill a void in the performance industry. While practical, a venue like this might not be the best idea because the surrounding area of the ballpark is mainly single-family homes. Concerns about loud noise continuing into late hours was mentioned by multiple interviewees.
- **Potential Programming & Activity:** Interviewees had mixed opinions regarding what should take the place of the Salt Lake Bee's. While these views varied when referencing secondary uses, most of the ideas for the venue included a community focus. Some stated they wanted a possibility for larger, commercial operations while others opposed that idea, believing that would take away from the space being something the local residents could claim as their own.

CONCLUSION

The community surrounding Smith's Ballpark and other Salt Lake City stakeholders have various opinions and feedback regarding the future of the site. It is unlikely that the ultimate decision will be universally ideal for all of the stakeholders we interviewed.



6. LOCAL EVENT ORGANIZER & CONCERT PROMOTER INTERVIEWS



This section presents a summary of the interviews held by Victus with potential groups about their interest and recommendations regarding the redevelopment of Smith's Ballpark. The review of the interviews is as followed:

- Local Event Organizers
- Concert Promoters



6. LOCAL EVENT ORGANIZER & CONCERT PROMOTER INTERVIEWS

A. LOCAL EVENT ORGANIZERS

Victus spoke with the following five (5) local event organizers regarding the feasibility of a potential new event space at a redeveloped Smith's Ballpark.

- Craft Lake City
- Salt Lake City Weekly
- Sports Marketing Collective
- Urban Flea Market SLC
- Utah Arts Alliance

Key highlights from their feedback are presented in the remainder of this section.

PERCEPTIONS OF CURRENT EVENT VENUES IN THE SALT LAKE MARKET

- Local event organizers had similar feedback regarding existing venues for their events. Multiple interviewees stated that they've gotten displaced from downtown venues like the Gallivan Center due to financial reasons. Another group noted that the Fair Park and Pioneer Park have not been friendly toward their events.



CRAFT LAKE CITY

- Craft Lake City is a charitable organization that educates and inspires Utah's arts community through science, technology, and art.
- The organizer mentioned that they used to use the Gallivan Center for the better part of a decade but moved to the Fairgrounds due to pricing. It was stated that the Ballpark Site could attract high demand because of its past and the public transportation that runs right by it. Their main event is the DIY festival which hosts 18,000 people over 3 days; a permanent stage along with easy access to power would allow them to run their events at a lower cost. Additionally, the organizer noted that they could maybe do two-three (2-3) events per year granted amenity requests are met.
 - **INTEREST LEVEL: HIGH**

SALT LAKE CITY WEEKLY

- Salt Lake City's only free alternative newsweekly; they also host a multitude of smaller events.
- This organization has used venues like the Utah State Fair Park, the Gallivan Center, and the Gateway. Versatility of a venue and its ability to handle extreme weather is important to them. Their largest event is the Utah beer festival in which they host 3,000-5,000 people for 2 days. The interviewee stated that the location of the venue is fine because of public transportation already in place. Many of their events are around 500-1,000 people, so while a site like the Ballpark could host them, they would not need the whole space. It would depend on how the site is built out, but they are interested in hosting a handful of new events there.
 - **INTEREST LEVEL: MEDIUM-HIGH**



SPORTS MARKETING COLLECTIVE

- Provides Sponsorship Sale and Media Rights consulting services. Has relationships with USA Cycling, USA Skateboarding, USA Climbing, X Games, and more.
- The interviewee stressed to keep this venue multi-use and active year-round. To do that, an anchor tenant is necessary to ensure foot traffic every day. Also, keeping popular attractions in the Ballpark is key, like the wiffle ball field and the train. The consultant stated that the location is great because all new development is happening south of downtown. The interviewee supported an entertainment venue of around 5,000-6,000 capacity to regularly host 2,000-3,000. Weekly programming could be achievable between entertainment events and events with the above relationships.
 - **INTEREST LEVEL: MEDIUM-HIGH**

URBAN FLEA MARKET

- The Urban Flea Market is a year-round vintage, yard sale style, and handcrafted market.
- The interviewee stated that they've been running their market for 14 years now at the Gateway. The organizer mentioned more market friendly event space is necessary; the Fair Park was a bad experience as they attempted to charge vendors to be there, and Pioneer Park is now unavailable due to upcoming redevelopment. With brick-and-mortar amenities like bathrooms and structures to cope with weather, the Ballpark Site would be a great place for them. The market is hosted monthly on every second Sunday, with over 100 vendors and 3,000 people in attendance. If the venue can act as a larger marketplace, the Flea Market would definitely host here.
 - **INTEREST LEVEL: HIGH**



UTAH ARTS ALLIANCE

- Their mission is to foster the arts to create an aware, empowered, and connected community.
- Past venues include the Gateway, the Gallivan Center, Library Square, and the Utah State Fair Park. They were priced out of the Gateway and the Gallivan Center, and they've had trouble getting access to Library Square. The interviewee said they feel like they're being abandoned, and that this venue has a big opportunity to make them feel welcome again. The organizer stated the Ballpark is an iconic landmark and believes the structure could be repurposed. They host two main festivals, the Urban Arts Festival, which does 35K+ in attendance over two days, and Illuminate Salt Lake. They could host anywhere from their two large events to weekly events depending on cost/amenities.
 - **INTEREST LEVEL: HIGH**



- **Perceptions of Current Event Venues in the Salt Lake Market:** Local event organizers had similar feedback regarding existing venues for their events. Multiple interviewees stated that they've gotten displaced from downtown venues like the Gallivan Center due to financial reasons. Another group noted that the Fair Park and Pioneer Park have not been friendly toward their events.
- **Ideal Amenities for a Redeveloped Smith's Ballpark:** Permanent infrastructure including a stage, bathrooms, access to power, and mechanism for weather protection are common asks.

CONCLUSION

Local event organizers expressed high interest and the ability to program the redeveloped ballpark consistently. If the site had the features and amenities to make it turnkey for the event organizers, it's very likely outdoor events would be held almost year-round.



6. LOCAL EVENT ORGANIZER & CONCERT PROMOTER INTERVIEWS B. CONCERT PROMOTERS



Victus spoke with the following four (4) concert promoters regarding the feasibility of a potential new outdoor concert venue at a redeveloped Smith's Ballpark.

- MagicSpace Entertainment
- Richter Entertainment Group
- S&S Presents
- The State Room Presents

Key highlights from their feedback are presented in the remainder of this section.

PERCEPTIONS OF OUTDOOR CONCERT VENUES IN THE SALT LAKE MARKET

- Event organizers had varying perceptions regarding the current Salt Lake outdoor concert venue market. Two promoters mentioned that there is a gap to be filled within the market with a mid-sized venue somewhere between 4,000 to 8,000 capacity. Another event organizer said 11,000-12,000 capacity would be ideal.



MAGICSPACE ENTERTAINMENT

- MagicSpace produces a wide variety of stage shows, ranging from comedians and illusionists, to dance groups, holiday shows, and other concert performers.
- The promoter has used multiple facilities in the area and believes in some demand for a new venue at Smith's Ballpark. The interviewee stated that there is big competition in neighboring markets, so with no commercial pipeline, the site is better suited for festivals. Concern about possible noise ordinances was voiced as other venues up near North Temple in addition further south in Sandy have 10pm shutdowns. The promoter noted that they could do 12-20 events yearly with an exclusive deal and three (3) to five (5) events if they were not exclusive.
 - **INTEREST LEVEL: MEDIUM-HIGH**

RICHTER GROUP

- Richter primarily focuses on promoting classic rock concerts.
- The promoter had positive things to say about the Salt Lake Market, noting that it's not just a family market and people are not opposed to going downtown at night. The interviewee stated that a 5,000 to 7,500-seat capacity is good, possibly even 3,500 as it's more cost-efficient and there's not enough "big fish" in the area to cater to a larger venue. Two-thirds general admission and a one-third fixed-seating is ideal with power, shade, and permanent bathrooms on site. Depending on the deal, Richter would attempt to host as many events as they could in the 5-month weather window. Realistically, half a dozen events is feasible.
 - **INTEREST LEVEL: MEDIUM-HIGH**



S&S PRESENTS

- Currently produce over 1,000 concerts a year throughout multiple venues in Salt Lake City.
- The interviewee stated that in Salt Lake County, there are many venues around 3,000-seat capacity and larger ones like Utah First Credit Union Amphitheater which can hold 20,000; a venue with a 11,000 to 12,000-seat capacity could fill a void. Amenities on site to accommodate artists such as convenient load-in and nice dressing rooms is very important; a turnkey venue will attract more performers. If the venue meets the requirements above, S&S would expect 40 concert nights, with their organization programming about five (5) to six (6).
 - **INTEREST LEVEL: MEDIUM-HIGH**

THE STATE ROOM PRESENTS

- Hosts variety of performances in intimate venues featuring multiple music genres.
- The interviewee expressed that there are a good number of outdoor concert venues in the Salt Lake Market and by adding another one, the market could become oversaturated. The promoter noted that the location of the Smith's Ballpark is appealing as there's already a history of people gathering there in addition to growth in housing in that area. They indicated that the proposed venue should be multi-purpose since a dedicated outdoor concert venue would lack the programming demand necessary to keep it successful. They also stressed focusing on an experienced operator for whatever the site becomes. In terms of usage, it would depend on level of activation, community interest, and cost.
 - **INTEREST LEVEL: MEDIUM**



**PRELIMINARY INTEREST IN OPERATING AND/OR BOOKING EVENTS AT A
NEW AMPHITHEATER AT THE SMITH'S BALLPARK**

<u>Company</u>	<u>Operator/User</u>	<u>Likelihood of Usage</u>	<u>Desired Capacity</u>	<u>Number of Events</u>
S&S Presents	User	Likely/Possibly Use	11,000-12,000	5-6
MagicSpace Entertainment	Operator/User	Definitely Use	4,000-8,000	12-20/3-5
Richter Entertainment Group	User	Definitely Use	3,500-7,500	6
The State Room Presents	User	Possibly Use	-	-

Note: Sorted by Company in alphabetical order

First range of events for MagicSpace Entertainment is if they were the sole operator, second number is as a user.

As shown above, all the concert promoters interviewed expressed some level of interest booking and/or operating events at a redeveloped Smith's Ballpark. One (1) of the interviewees noted that they would also like to operate the venue and have exclusive booking rights. Another promoter expressed interest in potentially holding events at a potential new outdoor concert venue but did not disclose the number of events or ideal capacity.



Victus frequently converses with national concert promoters such as Live Nation and AEG, and they recently shared the following insights regarding the Salt Lake City concert market:

- The Salt Lake City Market is home to a young and growing population that can support a busy event season. There are a multitude of venues with capacities under 5,000 to host smaller artists, and Utah First Credit Union Amphitheater (which Live Nation owns and operates) hosts most events over 10,000-15,000 in attendance. Live Nation and AEG believe a venue with 5,000-8,000 capacity could fill a gap within the Salt Lake Market.
- Live Nation is willing to take on venues in the Salt Lake Market granted they are getting a good deal financially. This usually includes Live Nation being the exclusive operator of the venue so they can program autonomously without any scheduling conflicts.
- Because Live Nation owns and operates Utah First Credit Union Amphitheater, AEG believes there may not enough new business to be captured in the Salt Lake Market.



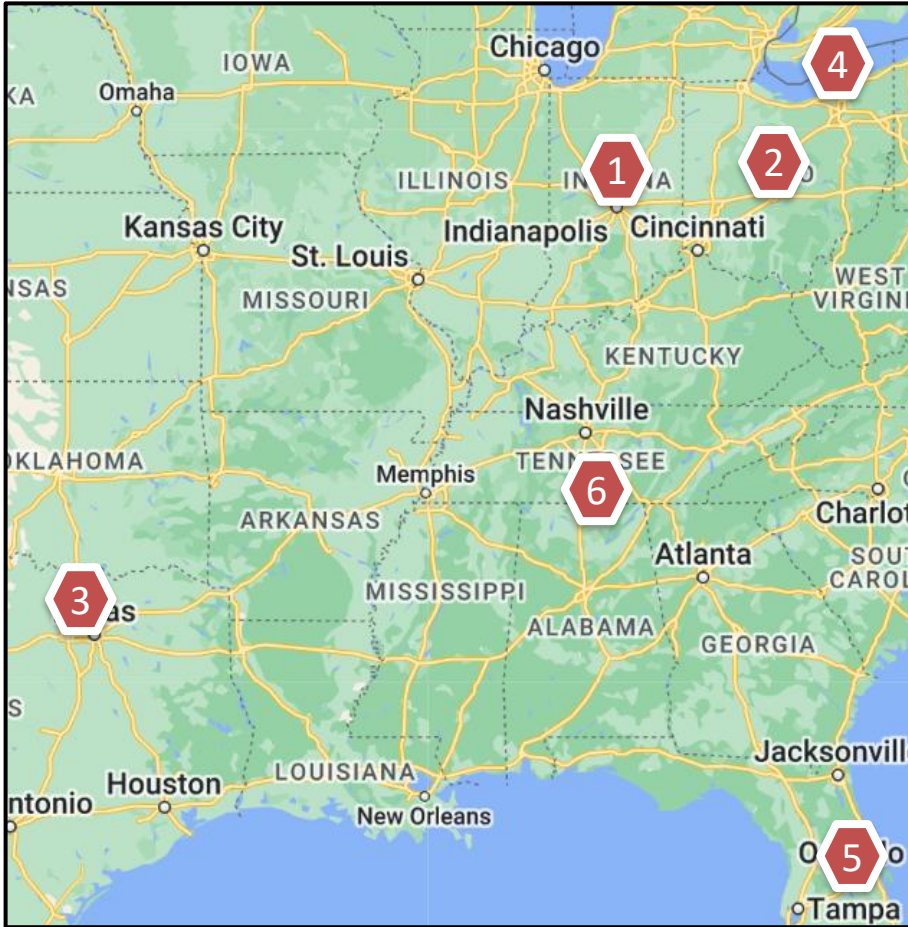
- **General Perception of Salt Lake Outdoor Concert Venue Market:** Many of the concert promoters interviewed expressed interest in a new outdoor concert venue but also some concern about over-saturation in the Salt Lake Market. While all of the promoters stated they could host events there, some voiced concern about possible lack of demand and sufficient programming opportunities.
- **National Concert Promoter Insights:** Live Nation and AEG believe a venue with 5,000-8,000 capacity could fill a gap within the Salt Lake Market. That said, because Live Nation owns and operates Utah First Credit Union Amphitheater, AEG believes there may not enough new business to be captured in the Salt Lake Market.

CONCLUSION

Concert promoters expressed interest in a potential new outdoor concert venue at a redeveloped Smith's Ballpark but were not very confident about being able to sustain enough programming. It is likely that a redeveloped Smith's Ballpark would not be sustainable as a dedicated outdoor concert venue with no other programming.



7. COMPARABLE CASE STUDIES



Comparable Case Studies

- 1 Bush Stadium
- 2 Cooper Stadium
- 3 Choctaw Stadium
- 4 League Park
- 5 Osceola County Stadium
- 6 Wicks Family Field at Joe Davis Stadium

Victus Advisors identified and profiled a set of former ballparks across the United States that have been redeveloped for other uses.

Source: Google Maps

Note: Venues are listed in alphabetical order

- Location: Indianapolis, IN
- Redevelopment Opening: 2014
- Cost of Renovation: \$21.74 million in Q2 2024 dollars*
- Features:
 - 138 apartments in existing structure
 - 282 units total, 3 floors
 - 3 acres of greenspace, community grills
 - Access to downtown shopping and restaurants
 - Baseball diamond still intact, along with a sand volleyball court/frisbee golf course
- History:
 - Was the home of the Indianapolis Indians from 1931 to 1996
 - Used as a dirt track for several years and then as a car storage site before it was abandoned
 - Went through multiple name changes from Perry Stadium to Victory Field to Bush Stadium



Site Map



- Location: Columbus, OH
- Opened: To be determined
- Repurposed Use: Proposed retail/apartments/event space
- Features (planned):
 - Two mixed-use buildings
 - Apartments and two retail buildings
 - Grandstand along third base line will be preserved
 - Field area would turn into paved event space, while the area where the stadium once stood would be turned into green space
- Programming:
 - Public use in addition to events/concerts
- History
 - Proposed Sports Pavilion and Automotive Research Complex was supposed to be built on this site in the mid 2010's, however the \$40 million project stalled
 - Originally built as the Red Bird Stadium for the then Columbus Red Birds of the American Association



Site Map



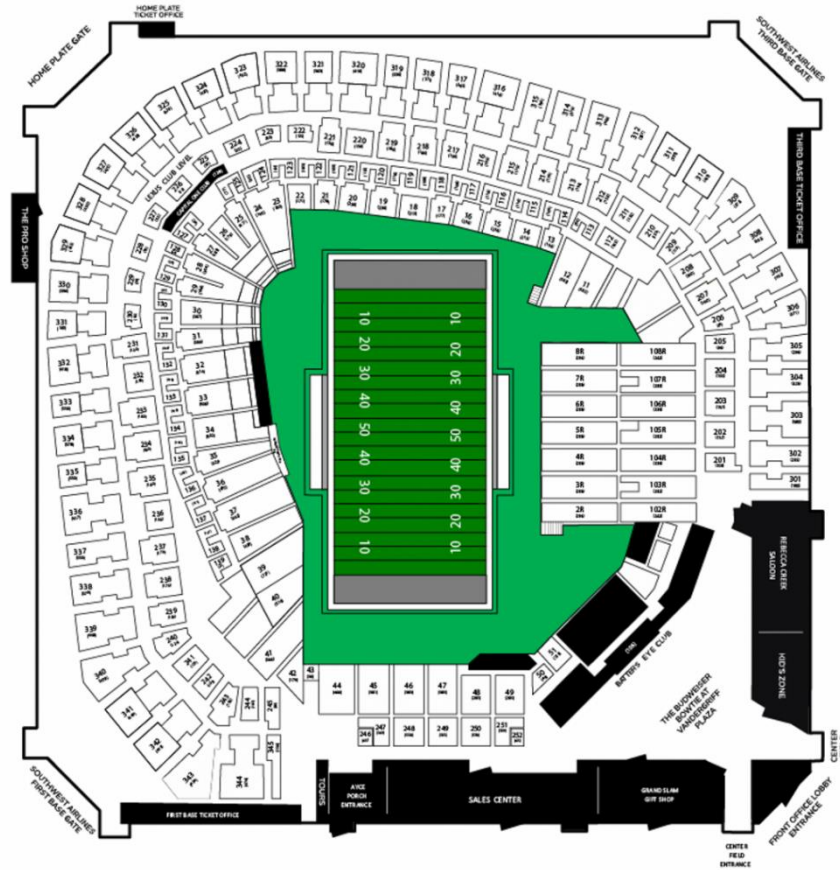
- Location: Arlington, TX
- Redevelopment Opening: 2019
- Repurposed Use: Football/soccer stadium
- Features:
 - Capacity over 48,000
 - 270-acre multi-purpose complex
 - Over 100 fixed and mobile concession stands
 - Multiple new developments outside the stadium including apartments and restaurants
- Programming:
 - Home to the Arlington Renegades (UFL), North Texas SC, and the Dallas Jackals
- History
 - Sponsored by Ameriquest in the 2000's and Globe life in the 2010's
 - Largest crowd was over 52,000 for Game 3 of the World Series
 - Construction cost of the original stadium is \$603.14 million in Q2 2024 dollars*



* Estimated according to the Turner Building Cost Index Q2 2024



Site Map



- Location: Cleveland, OH
- Redevelopment Opening: 2014
- Cost of Renovation: \$10.08 million Q2 2024 dollars*
- Repurposed Use: Baseball Heritage Museum/Public Park
- Features:
 - Around an acre of space
 - Pavilions, walking trails, public restrooms
 - Museum located in the restored ticket house
 - Artificial turf baseball field
- Programming:
 - Local youth baseball/softball
- History
 - Was the home of the Cleveland Indians when it was first built
 - Hosted American football, was the home field for multiple teams in the Ohio league and early NFL
 - The Cleveland Buckeyes of the Negro American League played here, which is part of the reason the Baseball Heritage Museum has a focus on the history of diversity in baseball



Site Map

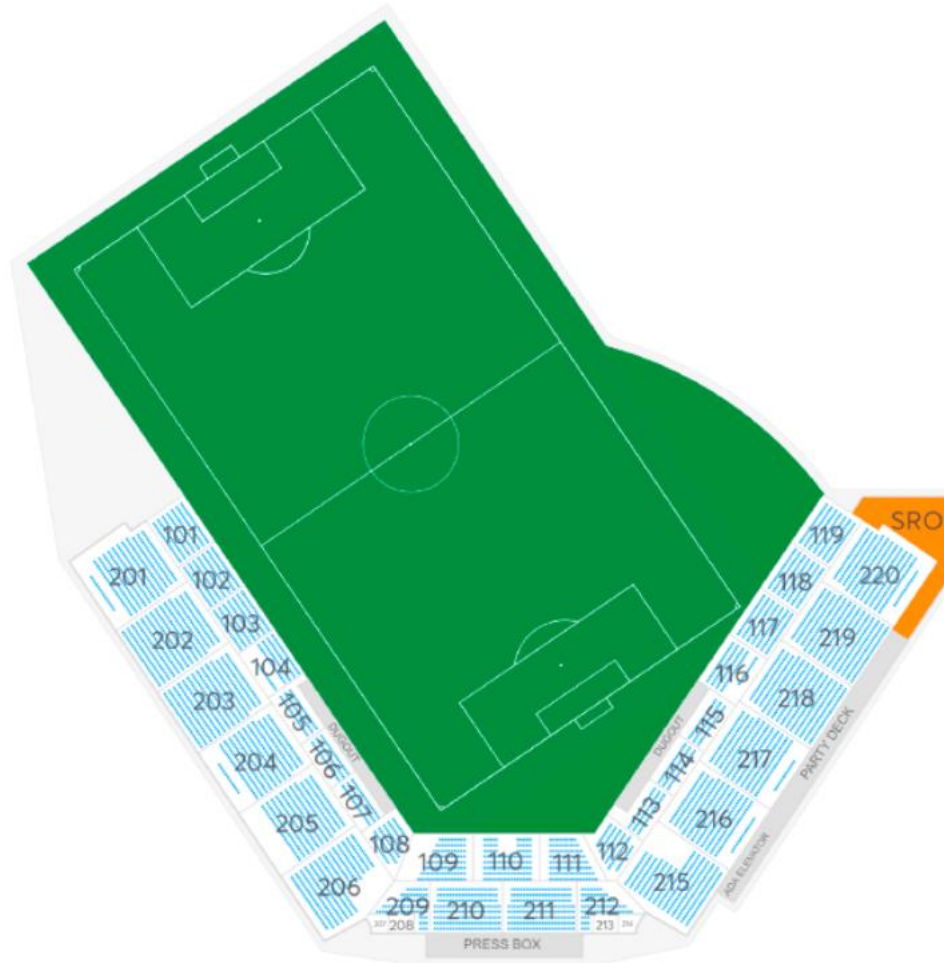


- Location: Kissimmee, FL
- Redevelopment Opening: 2019
- Repurposed Use: Soccer/multi-purpose stadium
- Features:
 - 20-acre training complex, 115,000 sq ft of grass
 - Maximum seating of 5,300
 - 4 luxury suites and an 18' by 32' video board
 - 30,000 sq ft of office space for working staff
 - Training ground includes four practice fields, 3 natural grass and one artificial turf
- Programming:
 - Home to Orlando City SC to house its senior team, USL reserve team and Development Academy
- History
 - Was the home of the Houston Astros for Spring Training for 31 years
 - Hosted American Cornhole League Championships and Atlantic Sun Conference baseball tournaments
 - Went through a renovation in 2003 at a construction cost of \$42.10 million Q2 2024 dollars*





Site Map



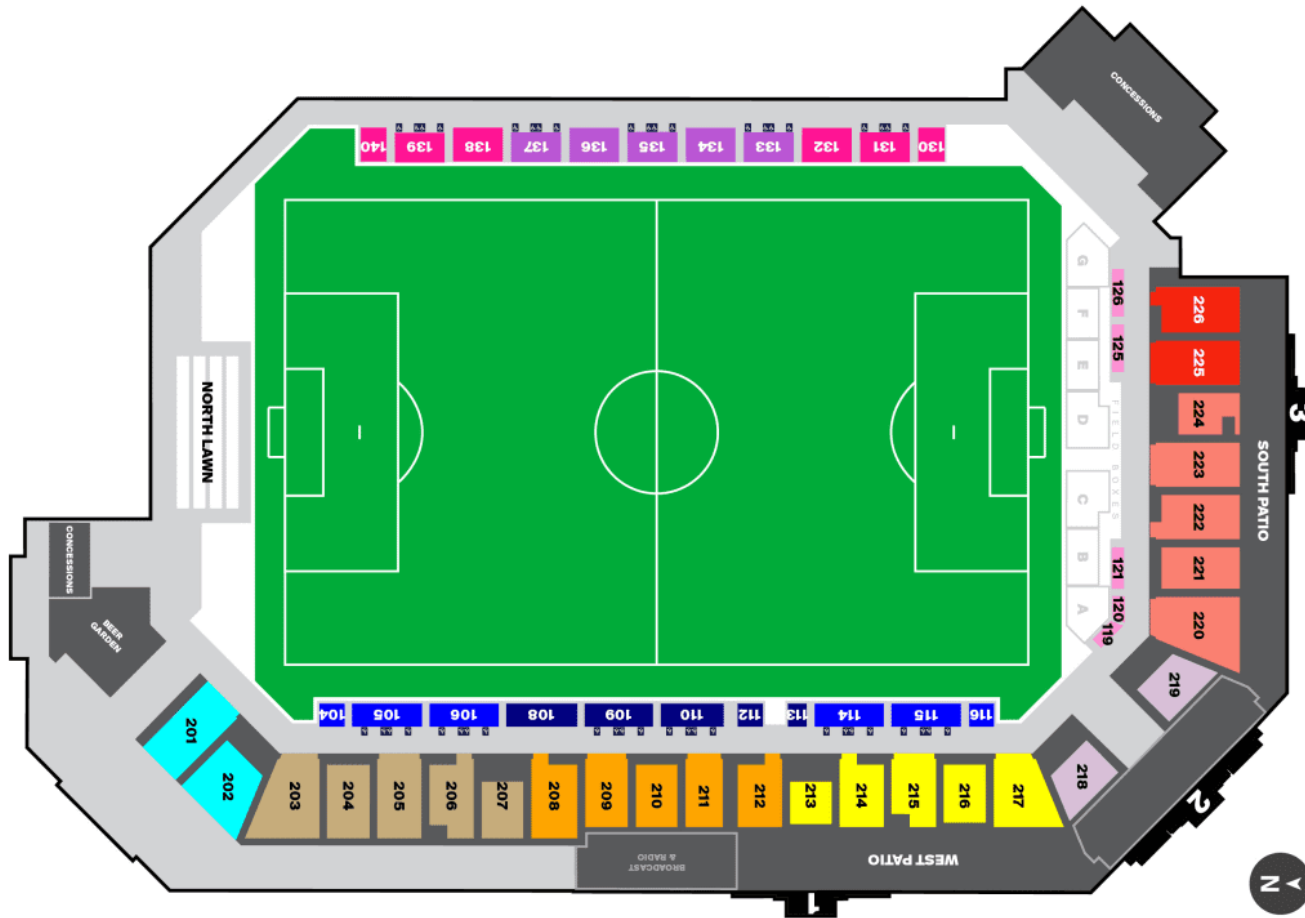
- Location: Huntsville, AL
- Redevelopment Opening: 2023
- Repurposed Use: Soccer & football stadium/events center
- Renovation Cost: \$30.56 million in Q2 2024 dollars*
- Features:
 - 6,000 seating capacity for soccer games
 - NASA rocket engine on site
 - 1,600 sq ft video scoreboard, 7 private boxes
 - Yellowhammer Brewing Craft Beer Garden in addition to in progress developments such as apartments, retail space, and a hotel around the stadium.
- Programming:
 - Huntsville City FC (MLS NEXT Pro), concerts/events
- History
 - Hosted the Huntsville Stars of the Southern League (MiLB) until 2014
 - Has been used for high school football and monster truck rallies



* Estimated according to the Turner Building Cost Index Q2 2024



Site Map



- **New Sports Uses:** There are multiple baseball stadiums that have been redeveloped into successful stadiums for other sports, mainly soccer. Those stadiums also program events and concerts to maximize revenue.
- **Other Non-Sports Uses:** One venue was converted into apartments while keeping the original structure and another has approved city funding to begin commercial and retail development while preserving parts of the concourse. Another venue has catered directly to the community by reframing the field for the youth and expanding the other areas into a park and museum.

CONCLUSION

There are multiple different approaches than can be taken with the Smith's Ballpark site as evidenced by the comparable case studies presented in this section. Ultimately, the future development should strive to meet the needs of the local community while at the same time generate year-round usage and activation.



8. RESEARCH FINDINGS & VENUE OPTIONS

Our research findings and venue options are based primarily upon the following primary and secondary research conducted by Victus Advisors over the course of this study:

- Analysis of Smith's Ballpark's unique market characteristics.
- Review of comparative and competitive outdoor concert venues in the Salt Lake City Market
- Feedback via interviews stakeholders, event organizers, and concert promoters.

Based on customized research and our extensive industry experience, we have identified three (3) options for the Ballpark Next development:

1. New Space (Demolish Smith's Ballpark & Create New Open Space)
2. Hybrid New Space (Create New Multi-Use Space & Keep Certain Elements of Smith's Ballpark)
3. Keep Ballpark (Create New Multi-Use Space & Keep Most of Smith's Ballpark)

The SLCRDA could consider a complete demolition of Smith's Ballpark and create a new open space. The space could have all or some of the following features and amenities:

- Green space
- Playgrounds
- Walking and running trails
- Splash pad
- Other outdoor park amenities such as outdoor basketball, tennis, and pickleball courts
- Skateboard park

PROS:

- New open/green spaces such as parks tend to be community assets and help to increase quality of life.

CONS:

- A complete demolition of Smith's Ballpark could create a negative reaction from community members who would feel as though a part of the community identity would be lost.
- A demolition of the site and redevelopment could be very costly.
- Other parks and open spaces in Salt Lake City have had issues with safety issues such as drug use and homelessness. There is a concern that a new open/green space could amplify those issues.

The SLCRDA could consider a repurposed Smith's Ballpark that keeps certain elements of the venue. The repurposed venue could have all or some of the following features and amenities:

- Green space
- Walking and running trails
- Indoor and outdoor retail and restaurants
- Office and residential units
- Event space for festivals, outdoor concerts, and sports events

PROS:

- Multiple stakeholders believe in the primary activation being public space, one emphasizing how it should be easily programmable. Consensus is that the Bee's are leaving a void in the community that needs to be filled with something community focused and not what's best financially.
- One stakeholder wants the venue to be a possible place to host smaller, community events like flea markets/farmers markets. Many of these smaller groups have had trouble finding places to operate downtown, so the ballpark site could provide a place to go.

CONS:

- A multi-use venue catering to various event organizers could create scheduling and programming challenges if not properly managed by a reputable venue operator.

The SLCRDA could consider a keeping Smith's Ballpark as it currently stands, but make adaptations to host other activities and events other than baseball. The venue could have all or some of the following features and amenities:

- Multi-use sports field
- Spectator seating
- Dedicated outdoor concert venue

PROS:

- One stakeholder wants to turn the venue into a women's sports and concert venue. The field would act as the epicenter for development around it, including possible festival streets, local art, a parking lot with programmable green space on top, and more. The stakeholder stated, "the community wants to feel something", and local activation is the best way to do that.
- Live Nation and AEG believe a venue with 5,000-8,000 capacity could fill a gap within the Salt Lake Market.

CONS:

- Concert promoters expressed interest in a potential new outdoor concert venue at a redeveloped Smith's Ballpark but were not very confident about being able to sustain enough programming. It is likely that a redeveloped Smith's Ballpark would not be sustainable as a dedicated outdoor concert venue with no other programming.



PIONEER BASEBALL LEAGUE

Victus understands that Salt Lake City has been approached about Smith’s Ballpark potentially being the home for a new Pioneer Baseball League (Independent League Baseball) franchise. The Pioneer Baseball League is a minor league baseball league that is recognized as an “Major League Baseball (MLB)” partner league. That said, none of its 12 current teams are affiliated with an MLB team.

The below chart details the Pioneer Baseball League:

Team	Stadium	Location	MSA/μSA	2024 MSA/μSA Population	Seating Capacity	2024 Avg. Reported Attendance
Billings Mustangs	Dehler Park	Billings, MT	Billings	192,305	3,071	2,265
Boise Hawks	Memorial Stadium	Boise, ID	Boise	852,423	3,452	3,144
Glacier Range Riders	Glacier Bank Park	Kalispell, MT	Kalispell	114,143	2,500	2,309
Grand Junction Jackalopes	Soplizio Field	Grand Junction, CO	Grand Junction	161,169	7,014	1,655
Great Falls Voyagers	Centene Stadium	Great Falls, MT	Great Falls	85,362	3,001	1,466
Idaho Falls Chukars	Melaleuca Field	Idaho Falls, ID	Idaho Falls	171,285	3,400	1,928
Missoula PaddleHeads	Ogren Park at Allegiance Field	Missoula, MT	Missoula	128,190	3,500	1,785
Northern Colorado Owlz	4Rivers Equipment Stadium	Windsor, CO	Fort Collins	378,187	2,500	n/a
Oakland Ballers	Raimondi Park	Oakland, CA	San Francisco-Oakland	4,746,767	4,000	1,918
Ogden Raptors	Lindquist Field	Ogden, UT	Ogden	672,858	8,262	3,107
Rocky Mountain Vibes	UCHealth Park	Colorado Springs, CO	Colorado Springs	792,301	8,500	2,530
Yolo High Wheelers	Dobbins Stadium	Davis, CA	Sacramento	2,466,648	3,500	n/a
HIGH				4,746,767	8,500	3,144
LOW				85,362	2,500	1,466
AVERAGE				896,803	4,392	2,211

Sources: Victus research, Pioneer Baseball League

Notes: (1) Sorted by Team in alphabetical order (2) Metropolitan Statistical Areas (MSA) are defined by U.S. Office of Management & Budget as adjacent counties with high degree of social/economic integration with urban core of 50,000 people or more. (3) Micropolitan Statistical Areas (μSA) μSA’s are defined by U.S. Office of Management & Budget as one or more adjacent counties or county equivalents that have at least one urban core area of at least 10,000 population but less than 50,000, plus adjacent territory that has a high degree of social and economic integration.



PIONEER BASEBALL LEAGUE (CONT.)

As shown on the previous page, Pioneer League Baseball stadiums have an average seating capacity of 2,500. Furthermore, average reported attendance per game for the 2024 season was under 1,500. It should also be noted that there are only two (2) franchises that play in markets with populations over one million people. The Salt Lake MSA has a population of over 1.3 million. Lastly, the Bees will still be in the same county and MSA as the potential franchise when the team begins play in South Jordan. It is very rare for an independent baseball league team to play in the same market as a minor league baseball team (MLB-affiliated).

While Salt Lake City could consider Smith's Ballpark as a home venue for a Pioneer Baseball League franchise, it is unreasonable to expect similar attendance figures and even appeal that the Bees experienced.

OTHER SPORTS USES

Victus identified potential event demand for other, non-mainstream sports such as X Games. One of the interviewees indicated that they have relationships with other sports organizations that could find the venue appealing for special sports events. They recommended a seating capacity no larger than 6,000 which could potentially work under Option 2.



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Appendix I:

**Ballpark Adaptive Re-Use Preliminary
Engineer's Opinion of Probable Cost**

Ballpark NEXT - Preliminary Engineer's Opinion of Probable Cost

ITEM #	LABEL	DESCRIPTION	VALUE	NOTES AND ASSUMPTIONS
1	DEMO - 1	Building Demolition, Large Urban Project, Steel, Includes 20 mile haul	\$ 1,056,397.71	-Separation, demolition, removal and disposal of section of the former Smith's Ballpark stadium structure down to the existing foundations. It is assumed that the separation from the structure to remain will be made at a cold expansion joint. -Includes recycling concrete, ferrous, and non-ferrous metals. If the section to be mass-wrecked contains lead on metals, it can remain and will be sent to a licensed metal recycler. -All trash and debris will be sorted and removed to a licensed disposal facility.
2	DEMO - 2	Building Demolition, Small buildings or simple buildings, steel, includes 20 mile haul	\$ 136,764.00	Includes dumpster fees
3	DEMO - 3	Building footings and foundation demolition, excludes disposal costs and dump fees	\$ 697,220.00	Demolition, removal and disposal of concrete slabs and foundations (Footings or piers) down to 4' below existing grade of the former Smith's Ballpark structure. Piers will be broken off below grade.
4	DEMO - 4	Selective Demolition, Gutting, Building Interior, Commercial Building	\$ 988,524.00	-Interior demolition and removal of architectural finishes within the existing portion of the stadium to remain (Restaurant spaces, suites, offices, mechanical rooms, etc.). -Exterior facade (CMU walls, Brick veneer, gates, canopies, metal cladding, etc.) back to the core structure.
5	DEMO - 5	Selective Demolition, Metal Roof	\$ 590,368.50	-Demolition, removal, and disposal of the existing metal roof. GMC will remove the roof while preserving the structural roof trusses for modification and installation of a new roof. -Work will be performed using hand demolition methods to protect the existing roof trusses. -Included cranes, aerial lifts, and torching or cutting methods for removal. Some patching and repairs may be required. -Includes recycling the metal roof with a licensed metal recycler if it contains lead paint. Spot lead abatement may be required for torching, cutting, or grinding operations, which could reduce the overall assumption that all lead will need to be removed.
6	DEMO - 6	Excavation and Backfill	\$ 161,558.81	Assume 4'-0" of Fill in the Demo Area
7	REPAIR - 1	Structural Maintenance and Repair	\$ 120,000.00	Preventive Maintenance/Repair Costs that will Reduce Year over Year
8	REPAIR - 2	Building Envelope Maintenance and Repair	\$ 130,000.00	Misc Building Envelope Repair/Replacement, TPTO, Weather Barrier
9	REPAIR - 3	MEP Systems Maintenance and Repair / Replacement	\$ 1,500,000.00	Assuming Complete Replacement of MEP Systems
10	REPAIR - 4	Fireproofing	\$ 205,942.50	Assuming 1/2 of Re-Use Footprint May Need Updated Fireproofing
11	STR - 1	Structural Retrofit, Seismic Strengthening	\$ 750,000.00	Assuming some level of seismic retrofit will be necessary
12	STR - 2	Adaptive Reuse Construction (Mass Timber, Structural Steel, Tower Beacon and Entry Portal)	\$ 32,950,800.00	Quantities taken from Google Earth aerials and as-built documents
13	STR - 3	Renovation/Tenant Improvement (Ground Level, Retail)	\$ 29,676,000.00	Excludes costs for re-use of steel for exterior furnishings such as benches and public art.
14	STR - 4	Facade (Curtainwall, Metal Panels, Roll Up Doors)	\$ 2,848,320.00	Assuming a Unitized (pre-fabricated) curtain wall system
			Subtotal	\$ 71,811,895.52
CONTINGENCY OR OPTIONAL LINE ITEMS				
15	DEMO - 6	Hazardous Abatement	\$ 9,718,100.00	Asbestos and Lead Paint Abatement for Whole Structure Prior to Demo. There is potential that this value could be reduced.
			Subtotal	\$ 9,718,100.00
			Total OPC	\$ 81,529,995.52

Disclaimer:

The Consultant has no control over the cost of labor, materials, equipment, or over the Contractor's methods of determining prices or over competitive bidding or market conditions. Opinions of probable costs provided herein are based on the information known to Consultant at this time and represent only the Consultant's judgment as a design professional familiar with the construction industry. The Consultant cannot and does not guarantee that proposals, bids, or actual construction costs will not vary from its opinions of probable costs.

Note that there is potential for variations in the hazardous abatement costs. The OPC line items assumes a cost for hazardous abatement of the entire structure area. The Ballpark was constructed in the early-to-mid 1990s, which is beyond the standard timeframe of which asbestos and lead paint was typically used in construction. Testing should be conducted to determine the quantity and extent of hazardous abatement that would be necessary

